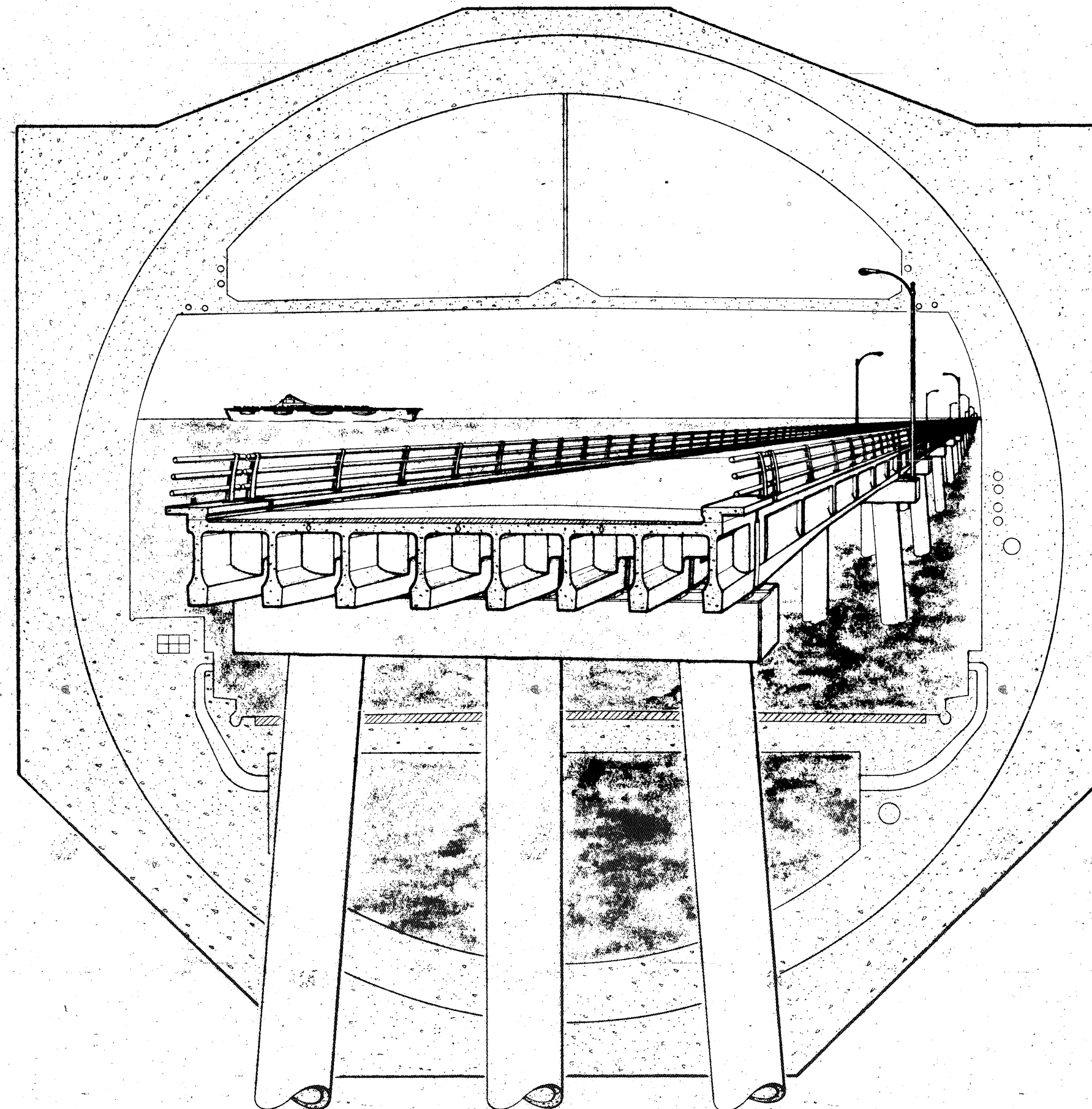


CHESAPEAKE BAY BRIDGE-TUNNEL CROSSING

SECTION NO. TF-3

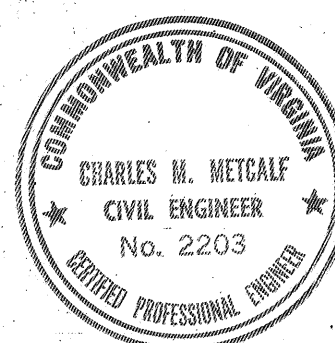
SOUTH TOLL PLAZA



FOR
COMMONWEALTH OF VIRGINIA
CHESAPEAKE BAY BRIDGE AND TUNNEL DISTRICT
NORFOLK I, VIRGINIA

PREPARED BY
SVERDRUP & PARCEL, CONSULTING ENGINEERS
NEW YORK, N.Y. - ST. LOUIS, MO. - NORFOLK, VA.

JANUARY, 1963

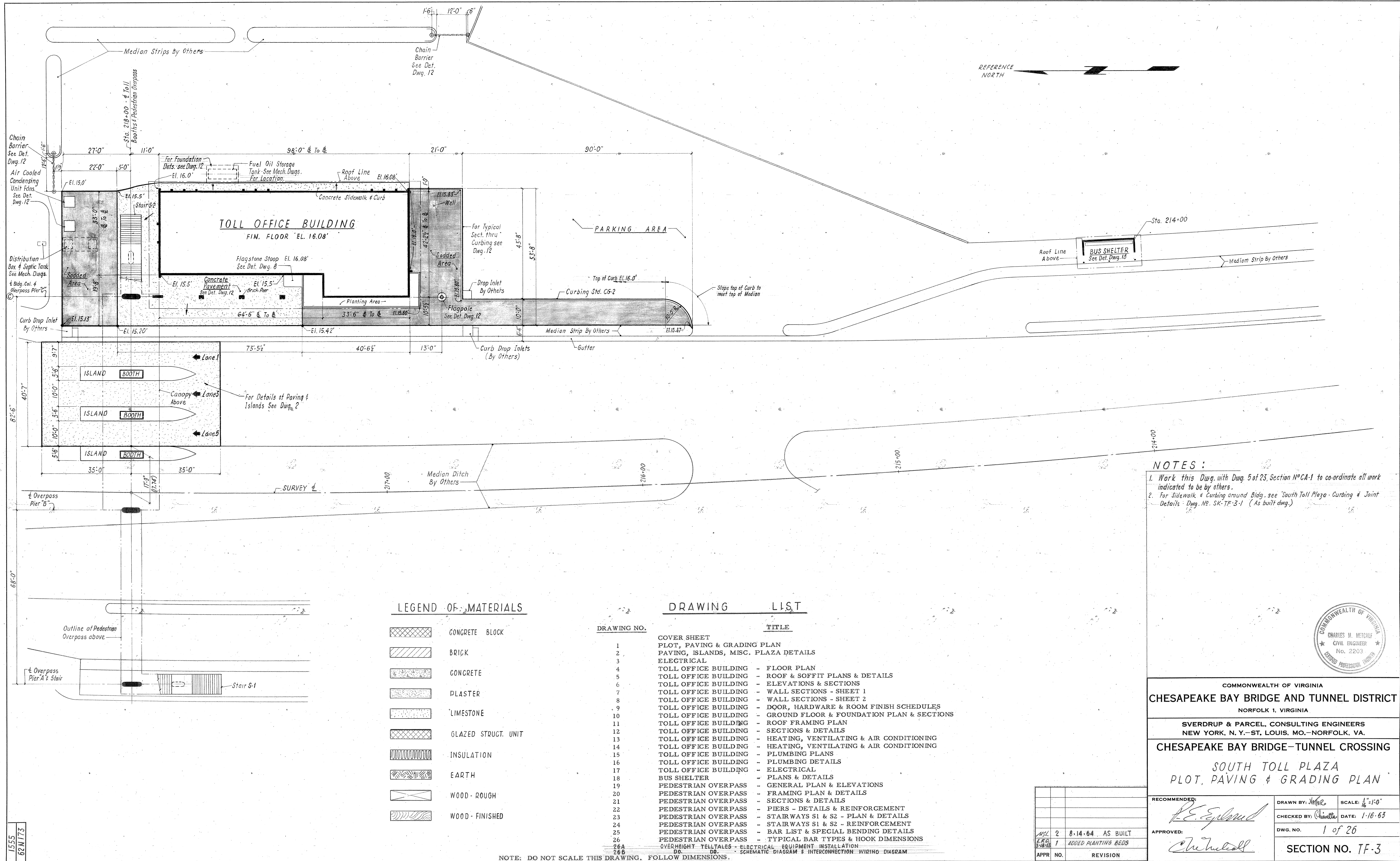


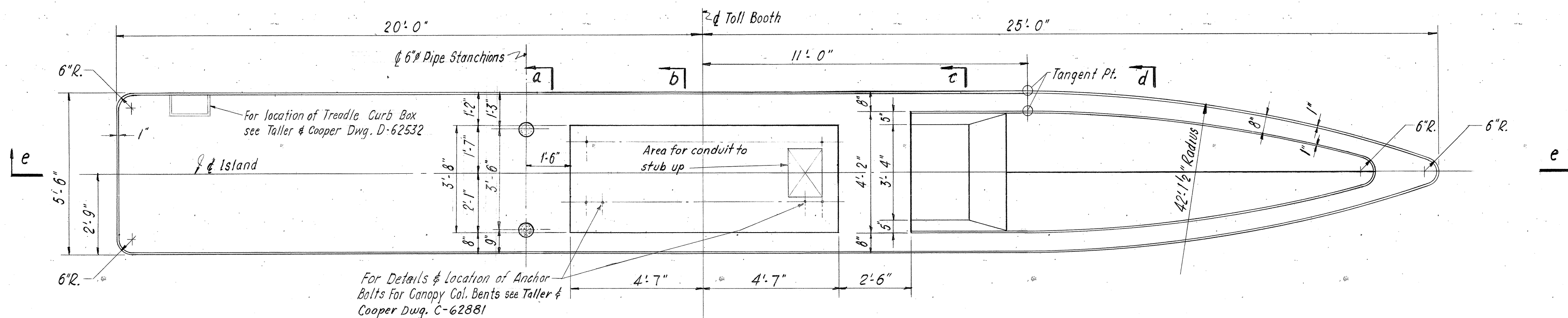
RECOMMENDED

Philip E. Eggen

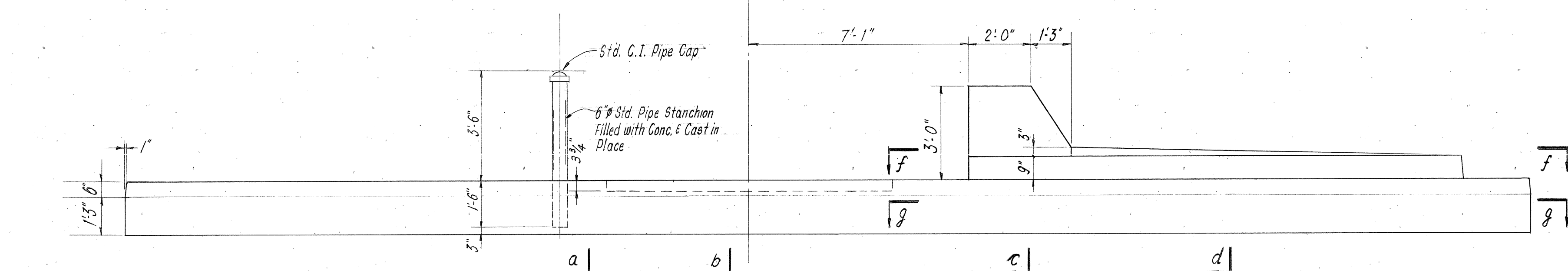
APPROVED

Charles H. Metcalf

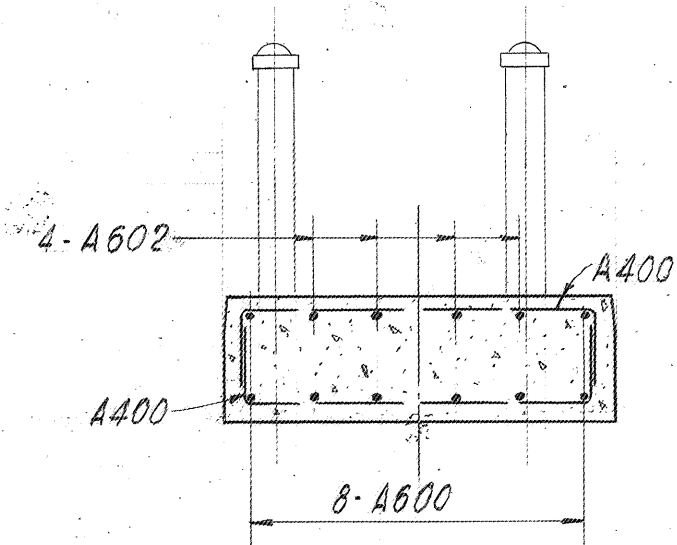




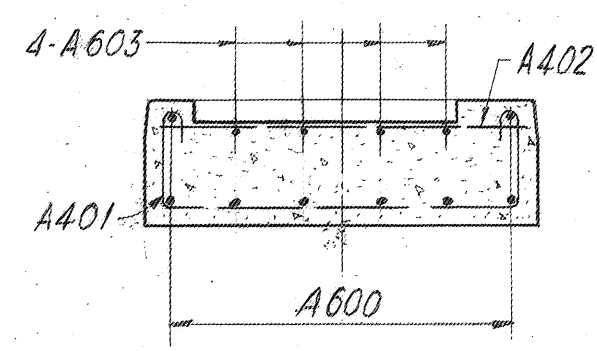
PLAN



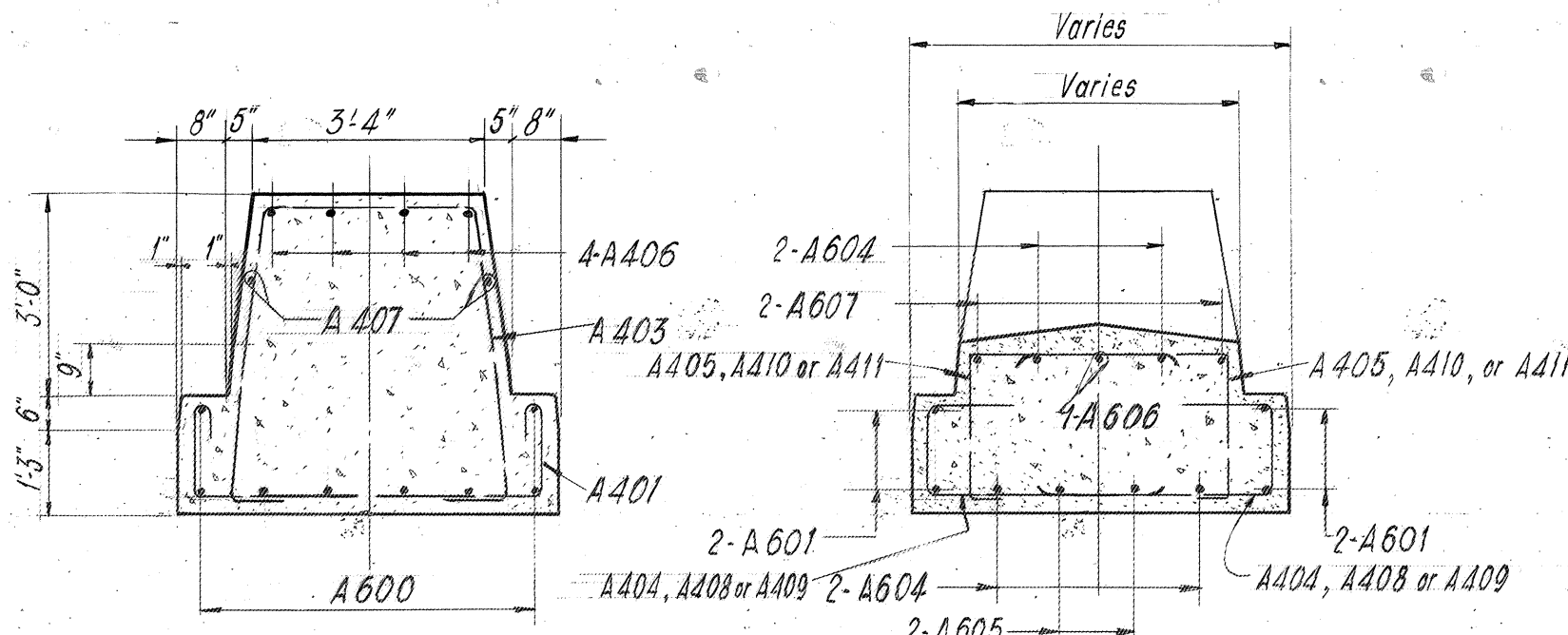
ELEVATION



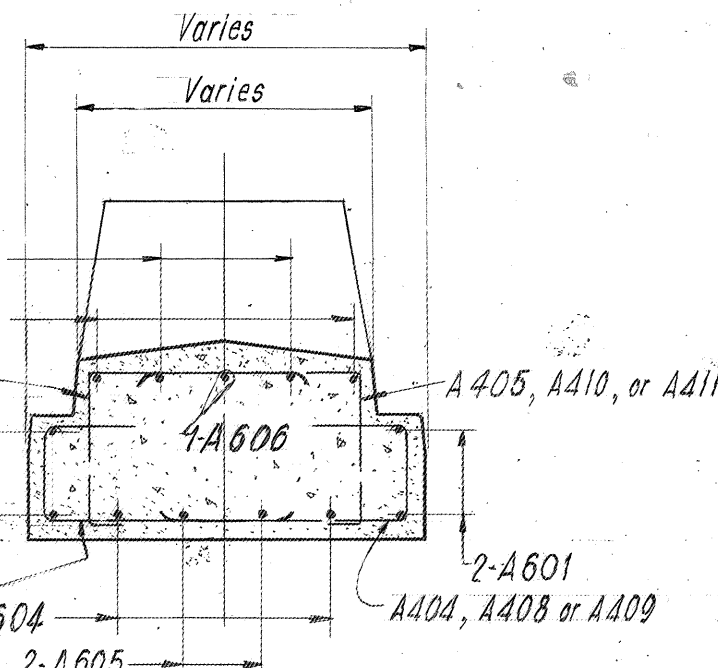
SECTION a-a



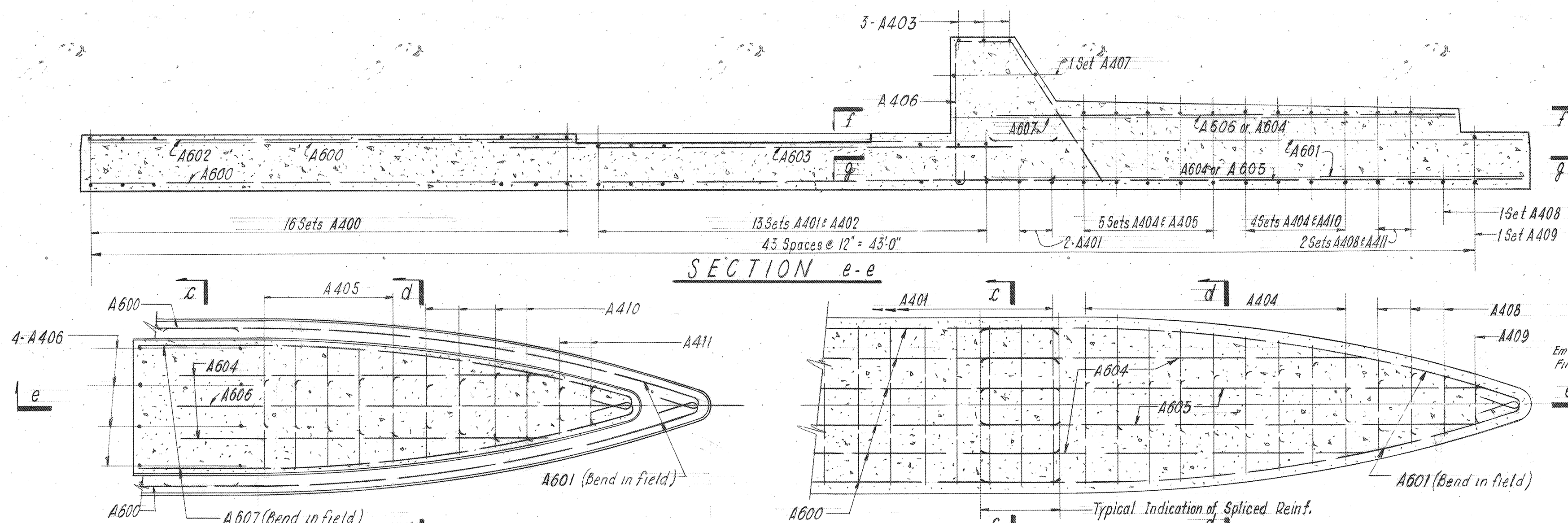
SECTION b-b



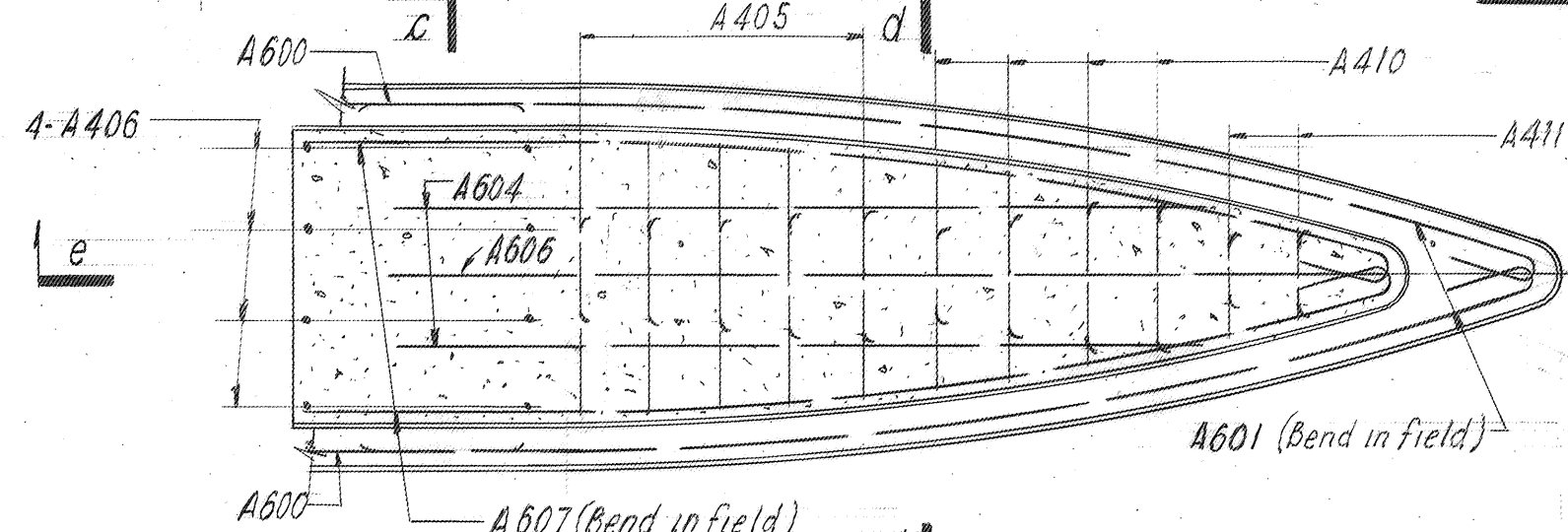
SECTION c-c



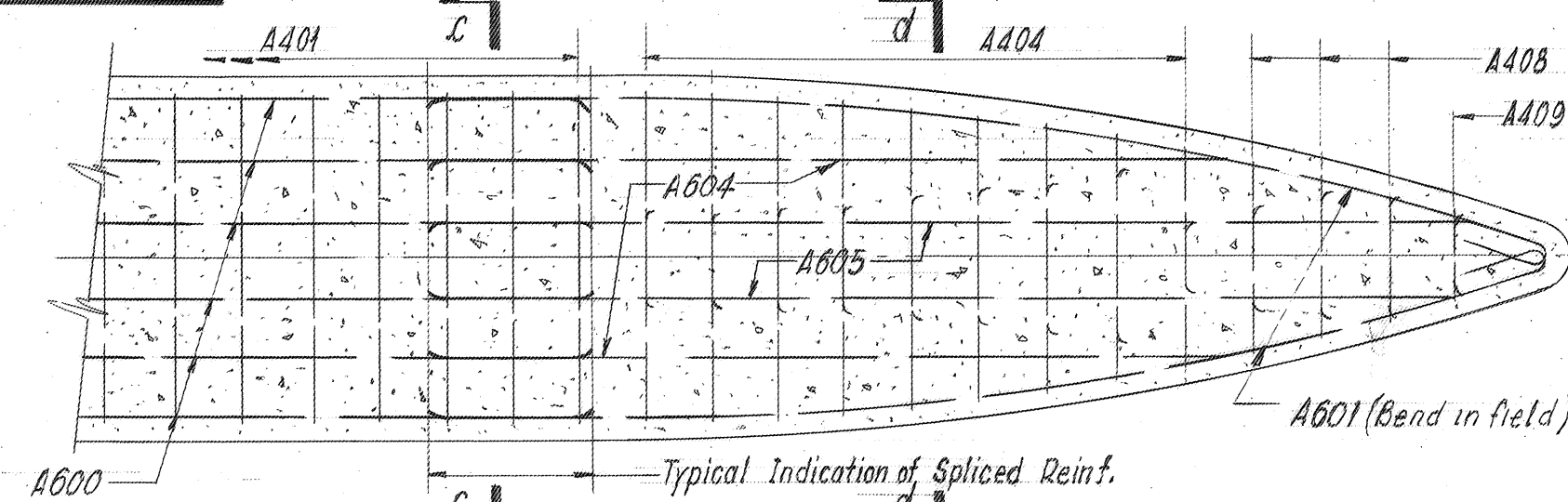
SECTION d-d



SECTION e-e



SECTION f-f



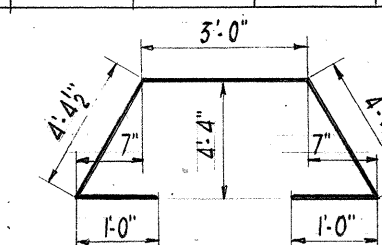
SECTION g-g

DO NOT SCALE THIS DRAWING. FOLLOW DIMENSIONS.

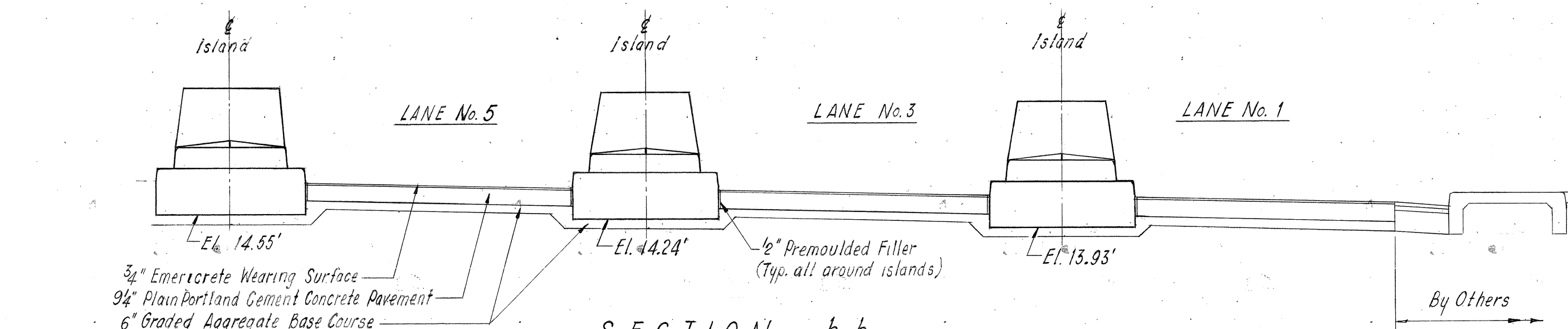
BAR LIST - TOLL BOOTH ISLAND

| No. Req'd | Size | Length | MK | Type | Location | Dimensions for Bending | | | | | | | | | | | |
|-----------|------|--------|------|------|-------------|------------------------|--------|-------|-------|---|---|----|-------|----|---|---|---|
| | | | | | | A | B | C | D | E | F | G | H | J | K | R | O |
| 32 | #4 | 7'-2" | A400 | SIO | Base | | | | | | | | | | | | |
| 15 | #4 | 8'-2" | A401 | S 9 | Base | 6" | 1'-4" | 5'-0" | 1'-4" | | | 6" | 4'-2" | 4" | | | |
| 15 | #4 | 8'-2" | A402 | STR | Base | | | | | | | | | | | | |
| 5 | #4 | 15'-9" | A403 | SPCL | Bumper | See Detail Below | | | | | | | | | | | |
| 16 | #4 | 6'-0" | A404 | SIO | Base | | | | | | | | | | | | |
| 10 | #4 | 5'-1" | A405 | SIO | Nose | 6" | 1'-4" | 5'-0" | 1'-4" | | | | | | | | |
| 4 | #4 | 11'-6" | A406 | SIO | Bumper | | | | | | | | | | | | |
| 2 | #4 | 7'-6" | A407 | SIO | Bumper | | | | | | | | | | | | |
| 6 | #4 | 4'-10" | A408 | SIO | Base | | | | | | | | | | | | |
| 2 | #4 | 3'-10" | A409 | SIO | Base | | | | | | | | | | | | |
| 8 | #4 | 4'-8" | A410 | SIO | Nose | | | | | | | | | | | | |
| 4 | #4 | 3'-9" | A411 | SIO | Nose | | | | | | | | | | | | |
| 8 | #6 | 30'-0" | A600 | STR | Base | | | | | | | | | | | | |
| 4 | #6 | 18'-3" | A601 | I | Base | 1'-3" | 17'-0" | | | | | | | | | | |
| 4 | #6 | 15'-0" | A602 | STR | Base | | | | | | | | | | | | |
| 4 | #6 | 15'-6" | A603 | STR | Base | | | | | | | | | | | | |
| 4 | #6 | 12'-0" | A604 | STR | Nose & Base | | | | | | | | | | | | |
| 2 | #6 | 16'-0" | A605 | STR | Base | | | | | | | | | | | | |
| 1 | #6 | 14'-0" | A606 | STR | Nose | | | | | | | | | | | | |
| 2 | #6 | 16'-3" | A607 | I | Nose | 1'-3" | 15'-0" | | | | | | | | | | |

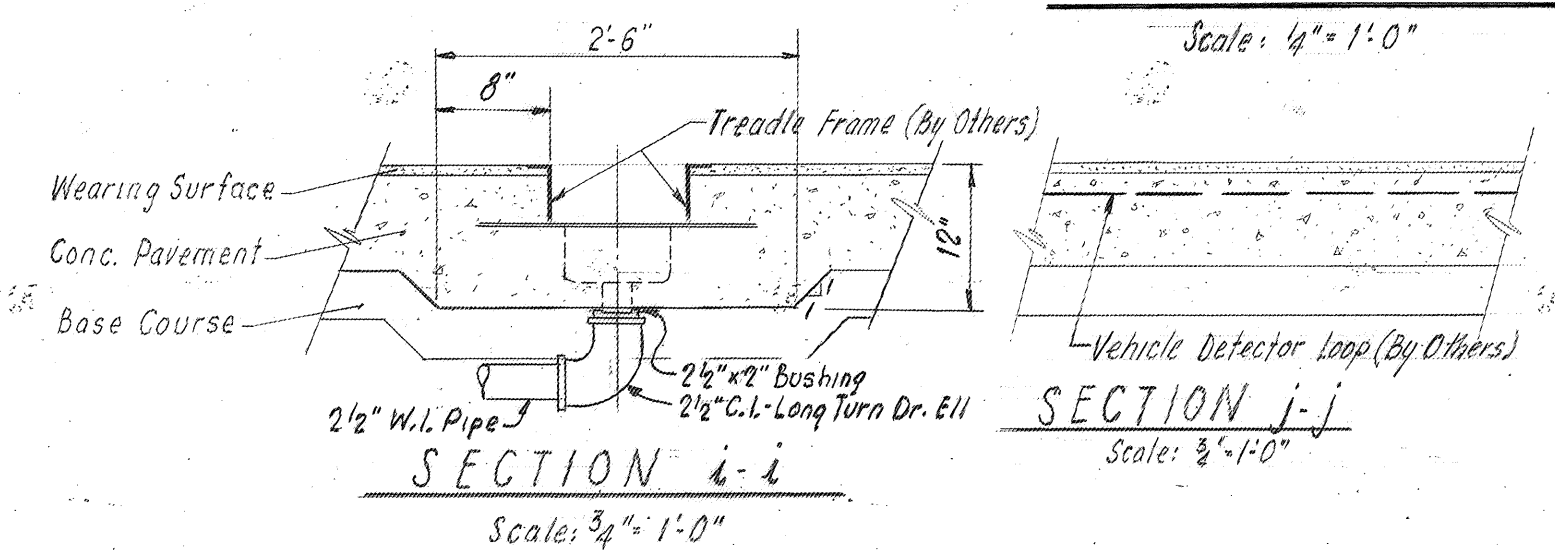
1. The quantities shown in the No. Req'd. column is for one Island only.
2. A dash is used in the appropriate dimension column to indicate that a hook, bend or portion of the standard bar type is to be omitted.
3. See Dwg. 26 for typical bar types & hook dimensions.
4. STR = Straight Bars
5. SPCL = Special Bars



BAR MARK A403

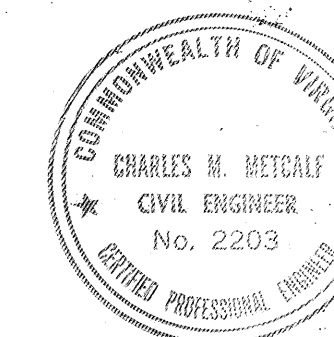


SECTION h-h

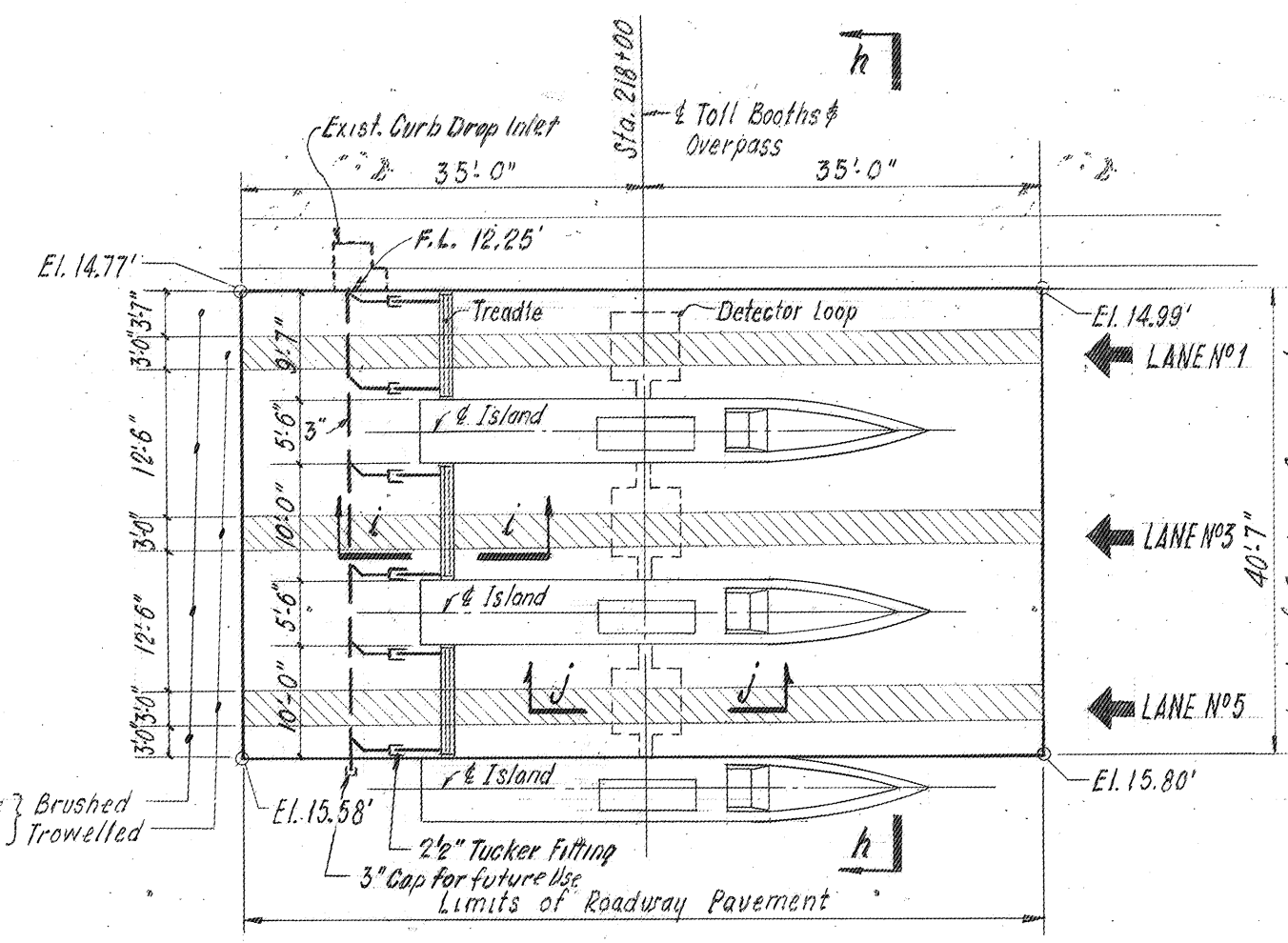


SECTION i-i

| APPR | NO. | REVISION |
|------|-----|------------------------------|
| 1 | 3 | 8-14-64 AS BUILT |
| 2 | 2 | 8-14-63 REVISION FOR TREADLE |
| 3 | 1 | ADDED DRAINAGE FOR TREADLE |
| 4 | 1 | REV. SECT 4-4. 5-22-63 |



- NOTES:
1. FOR GENERAL NOTES SEE DWG. 10.
 2. ANCHOR BOLTS FOR TOLL BOOTHS, BARRIER GATES AND MISCELLANEOUS EQUIPMENT, FURNISHED BY OTHERS, TO BE CAST IN PLACE IN ACCORDANCE WITH EQUIPMENT MANUFACTURER'S SPECIFICATIONS AND RECOMMENDATIONS.
 3. VEHICLE DETECTOR LOOPS AND TREADLES, FURNISHED BY OTHERS, TO BE CAST IN PLACE IN ACCORDANCE WITH EQUIPMENT MANUFACTURER'S SPECIFICATIONS AND RECOMMENDATIONS.
 4. FOR PIPING AND CONDUIT TO BE CAST IN PLACE, SEE MECHANICAL AND ELECTRICAL DRAWINGS.



KEY PLAN

NOTE: PIPE SHALL BE WROUGHT IRON WITH CAST IRON DRAINAGE FITTINGS. COAT ALL PIPE WITH TWO COATS OF BITUMASTIC AFTER INSTALLATION.

COMMONWEALTH OF VIRGINIA
CHESAPEAKE BAY BRIDGE AND TUNNEL DISTRICT
NORFOLK I, VIRGINIA

SVERDRUP & PARCEL, CONSULTING ENGINEERS
NEW YORK, N. Y. - ST. LOUIS, MO. - NORFOLK, VA.

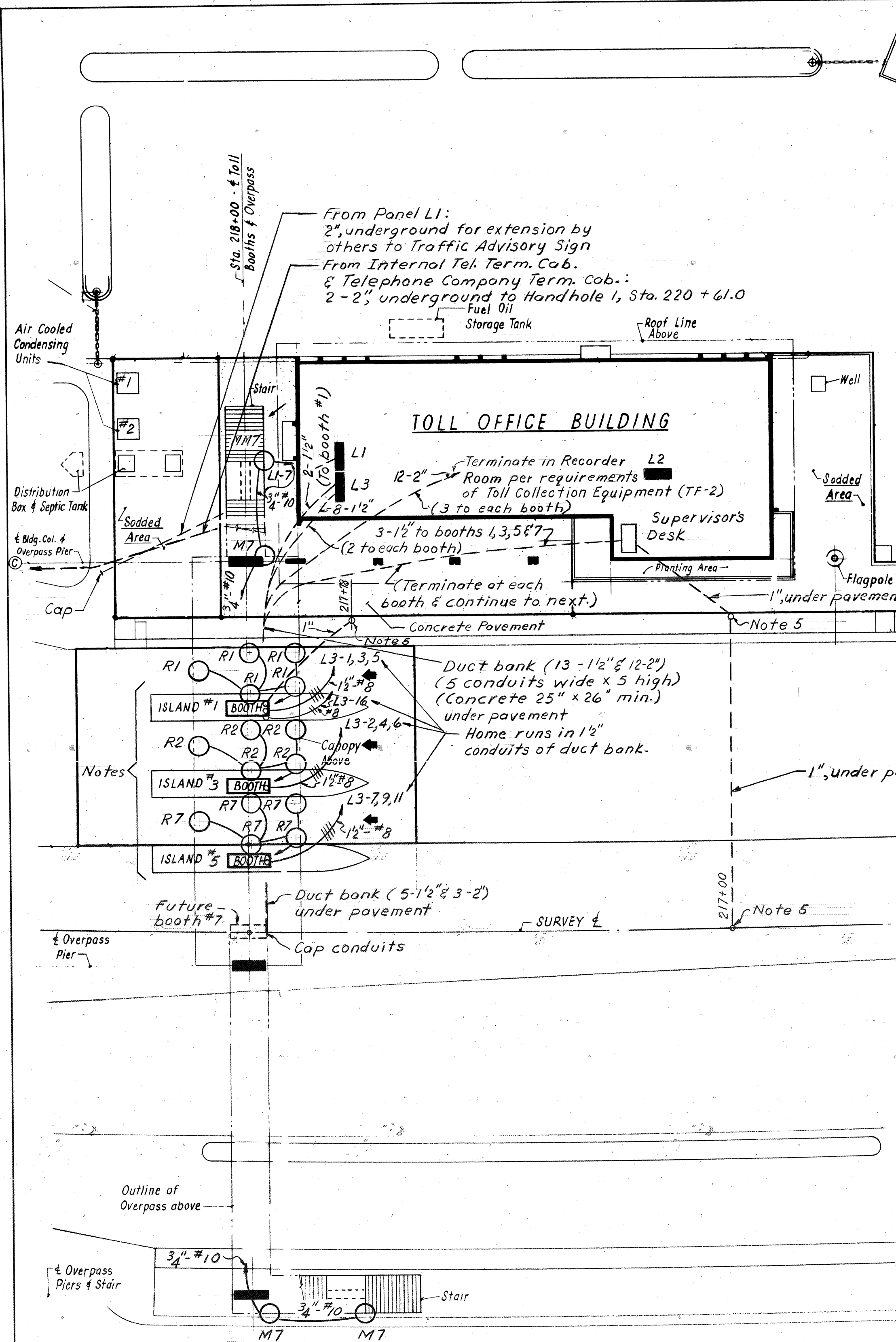
CHESAPEAKE BAY BRIDGE-TUNNEL CROSSING
SOUTH TOLL PLAZA
PAVING, ISLANDS AND MISC. PLAZA DETAILS

RECOMMENDED: *P.E. Eglund*
APPROVED: *Ch. Metcalf*

DRAWN BY: *Donato* SCALE: 3/8" Unless Noted
CHECKED BY: *Chavetta* DATE: 1-16-63
DWG. NO. 2426

SECTION NO. TF-3

AS BUILT



PLAN

- Notes:
1. For each toll booth & lane: Overhead traffic signals (furnished by others) to be installed & 3/4" conduit with 4 #12 to be installed from signals to control device (by others) in booth.
 2. For each toll booth & lane, 3/4" conduit with 2 #12 to be installed from type R fixtures to switch (by others) in booth.
 3. Thru Toll booth #1, 3/4" conduit with 2 #12, Ckt. L3-16, to be installed to sign on overpass.
 4. Conduits underground or under pavement are shown as follows: _____
 5. 1" Conduits to be run within concrete foundations for Overheight Tell Tales and terminated above concrete in base of support. See Dwg. 8T-444.

Ltg.-Toll Booth #1
Recp.-Toll Booth #1
H & V - Toll Booth #1
Ltg.-Toll Booth #5
Recp.-Toll Booth #5
H & V - Toll Booth #5
Spare
Spare
Spare

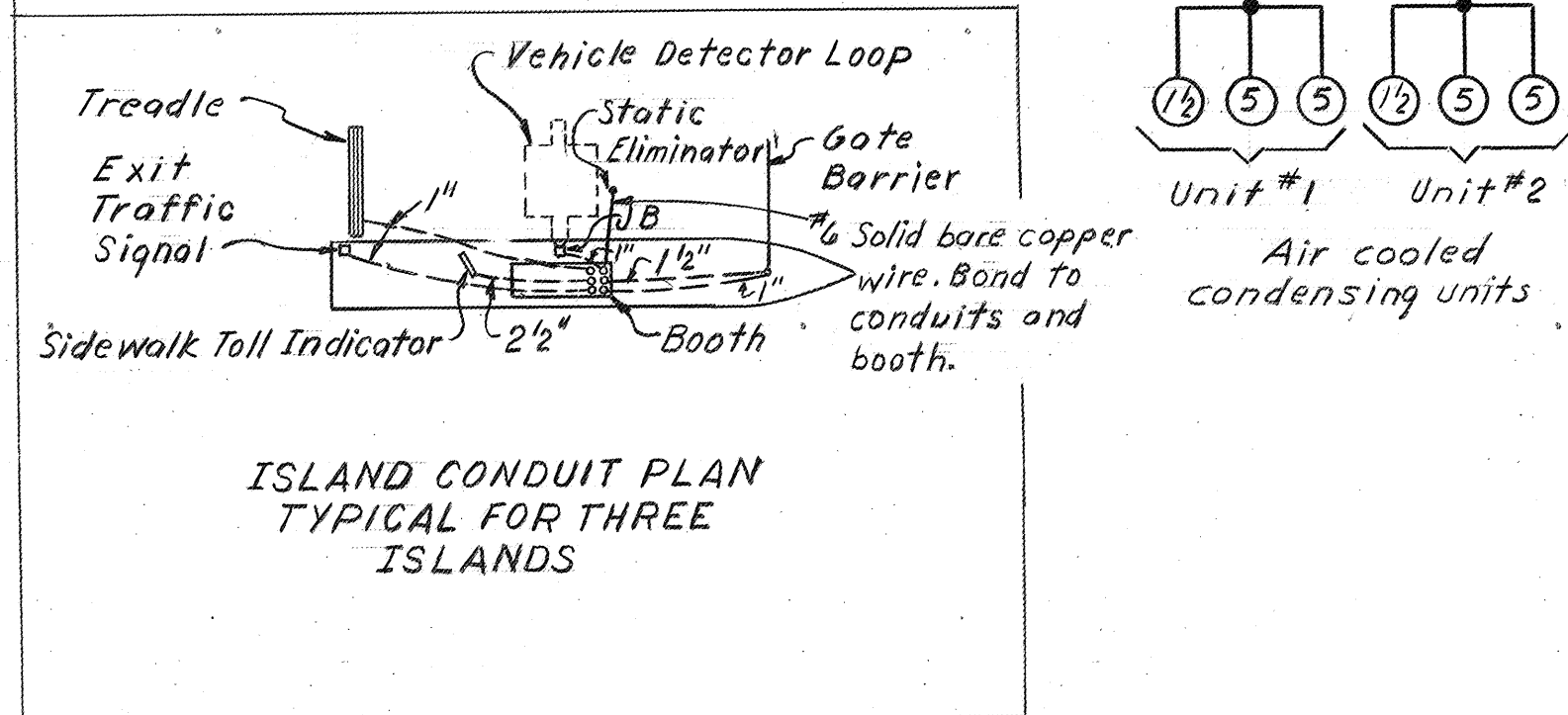
SOLID NEUTRAL

| | | | |
|----|-------------------------|----|-------------------------|
| 1 | 990 W 20A-1P | 2 | 990 W 20A-1P |
| 3 | 20A-1P | 4 | 20A-1P |
| 5 | 1/8" E 1/20HP 20A-1P | 6 | 1/8" E 1/20HP 20A-1P |
| 7 | 990 W 20A-1P | 8 | 20A-1P |
| 9 | 20A-1P | 10 | 20A-1P |
| 11 | 1/8" E 1/20HP 20A-1P | 12 | 20A-1P |
| 13 | 20A-1P | 14 | 20A-1P |
| 15 | 20A-1P | 16 | 1A 83A 20A-1P |
| 17 | 20A-1P | 18 | 20A-1P |

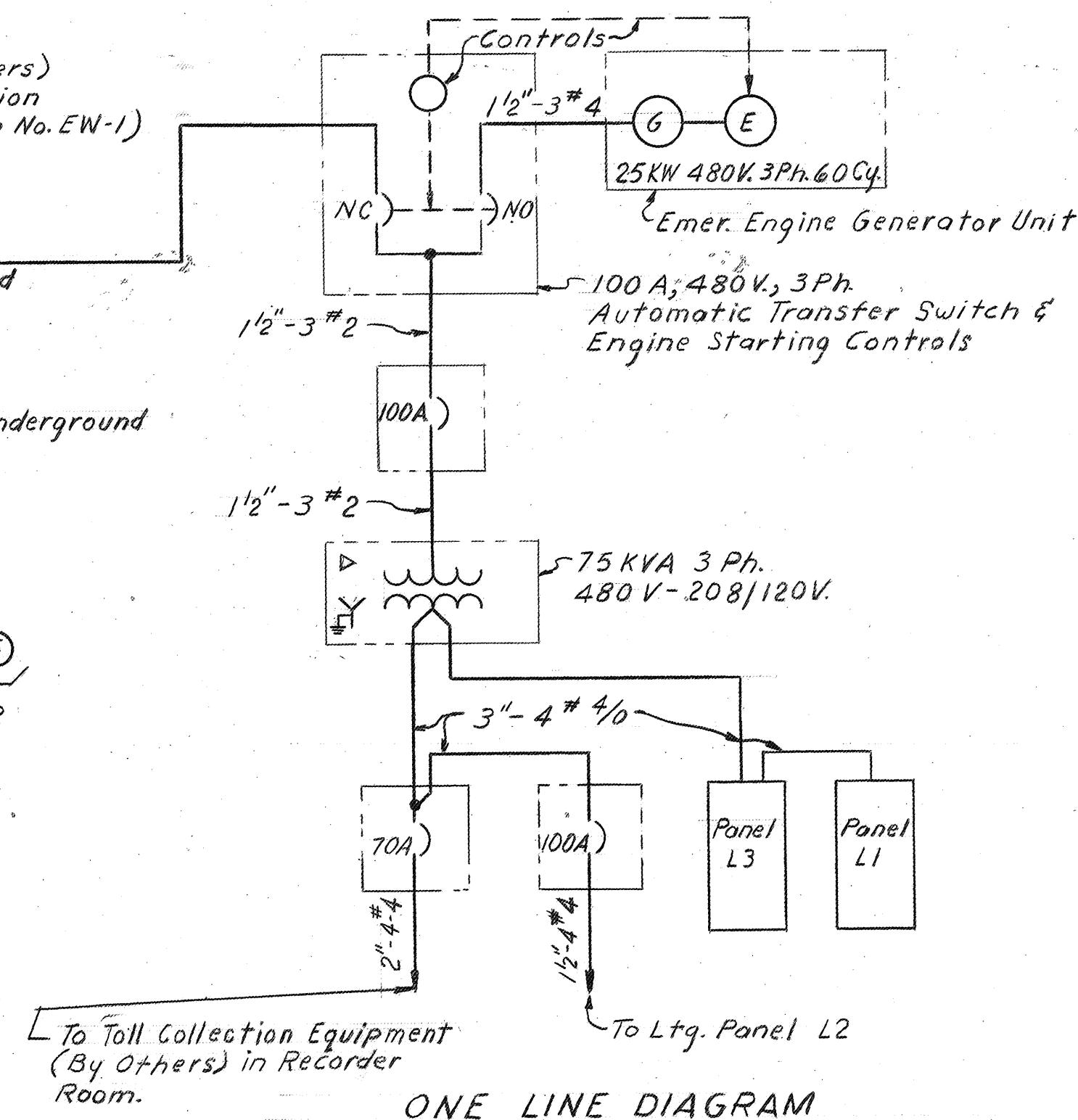
MAIN BREAKER
100A-3 P

3 PHASE, 4 WIRE, 120/208 V
MOUNTING: SURFACE
PANEL L3

Ltg.-Toll Booth #3
Recp.-Toll Booth #3
H & V - Toll Booth #3
Ltg.-Future Toll Booth #7
Recp.-Future Toll Booth #7
H & V - Future Toll Booth #7
Spare
Sign on overpass - "Chesapeake
Bay Bridge-Tunnel Crossing"
Spare

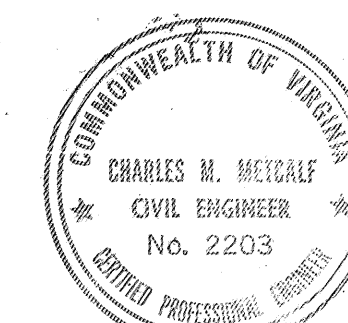


NOTE: DO NOT SCALE THIS DRAWING. FOLLOW DIMENSIONS.



ONE LINE DIAGRAM

| APPR. NO. | REVISION |
|-----------|------------------|
| 2 | GENERAL REVISION |
| 1 | GENERAL REVISION |
| 1 | REVISION |



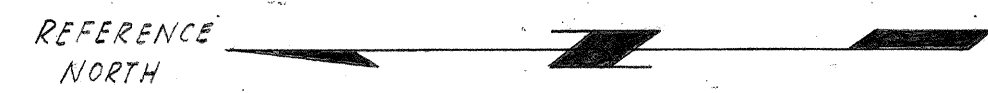
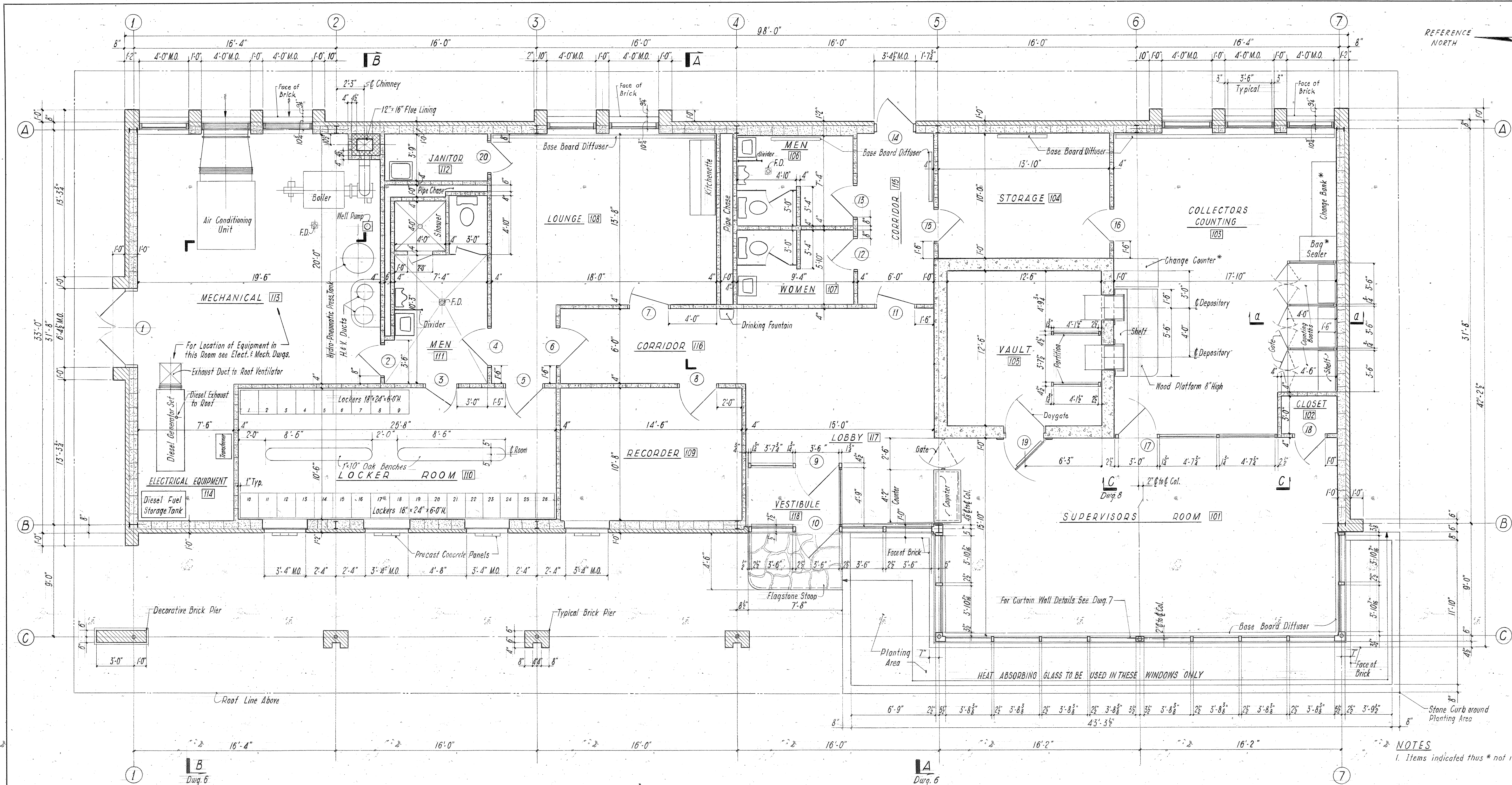
COMMONWEALTH OF VIRGINIA
CHESAPEAKE BAY BRIDGE AND TUNNEL DISTRICT
NORFOLK 1, VIRGINIA

SVERDRUP & PARCEL, CONSULTING ENGINEERS
NEW YORK, N. Y. - ST. LOUIS, MO. - NORFOLK, VA.

CHESAPEAKE BAY BRIDGE-TUNNEL CROSSING
SOUTH TOLL PLAZA
ELECTRICAL

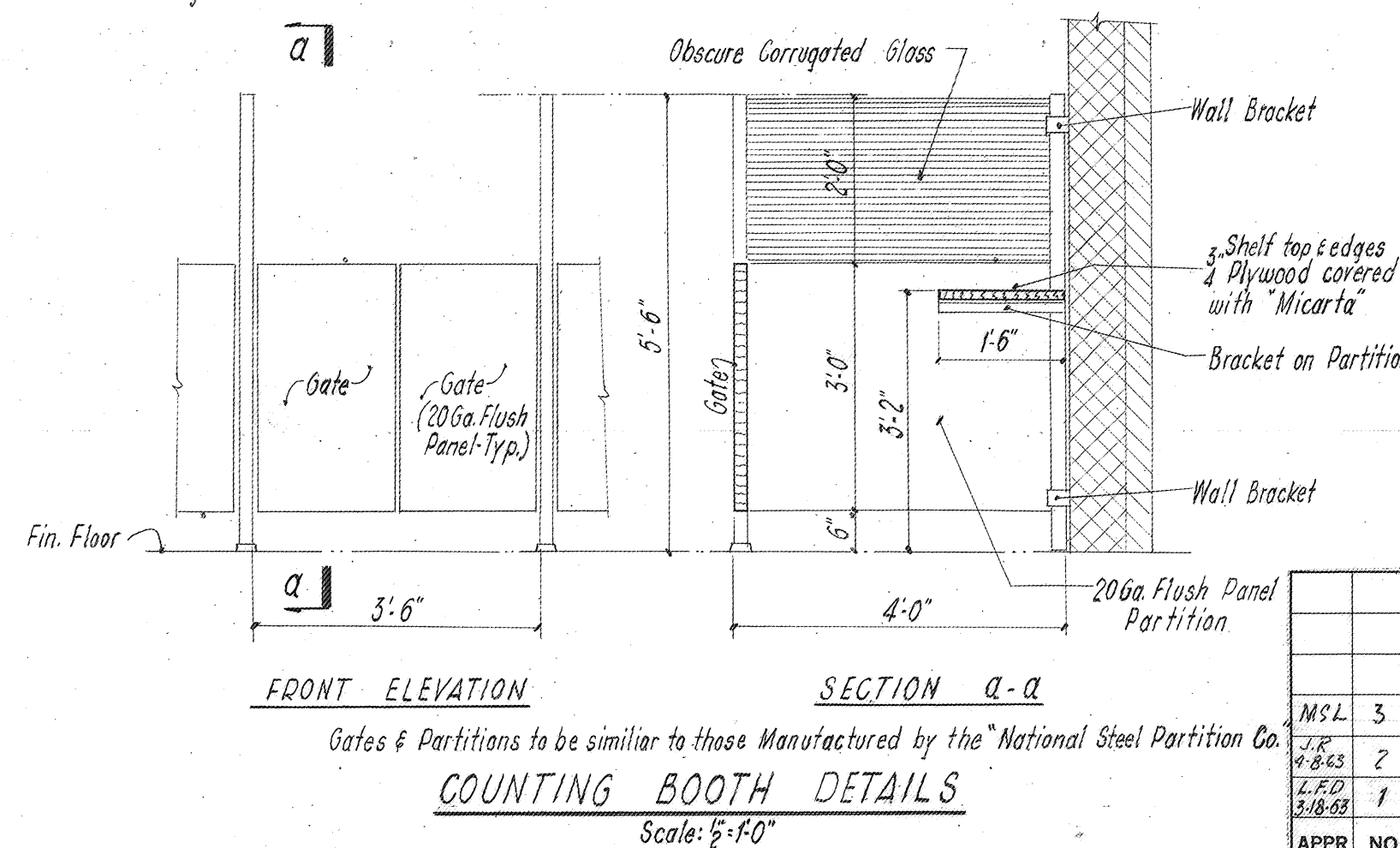
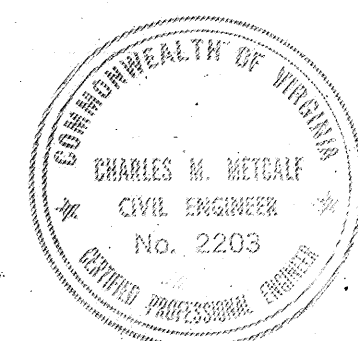
RECOMMENDED: *R.E. Eglund*
APPROVED: *Ch. Metcalf*

DRAWN BY: J.A. SCALE: 1/8" = 1'-0"
CHECKED BY: DHK DATE: 1-16-63
DWG. NO. 3 of 26
SECTION NO. 7F-3



FLOOR PLAN

NOTES
1. Items indicated thus * not included in this contract.

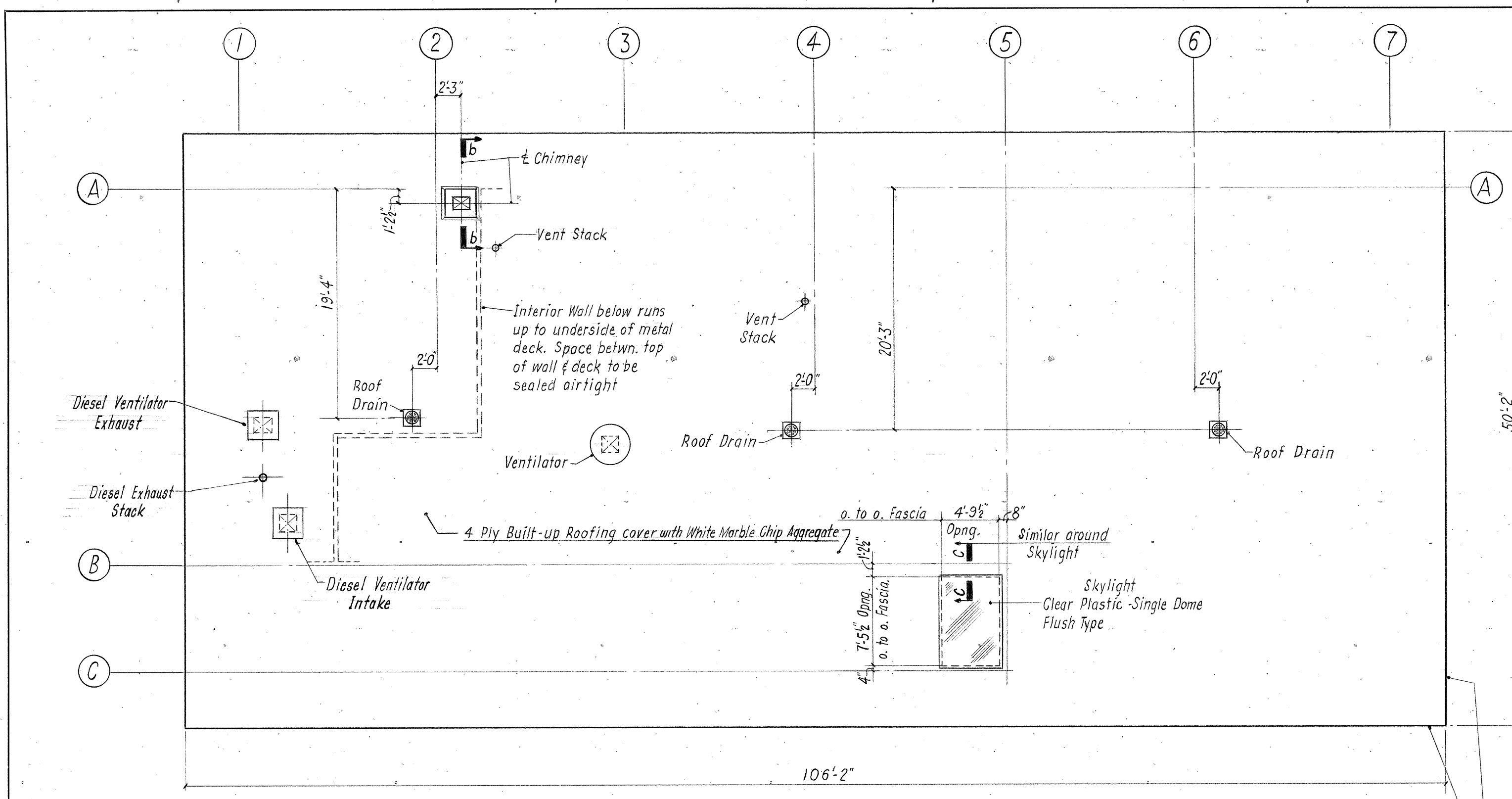


FRONT ELEVATION
SECTION A-A
Gates & Partitions to be similar to those Manufactured by the "National Steel Partition Co."
COUNTING BOOTH DETAILS
Scale: 1/2" = 1'-0"

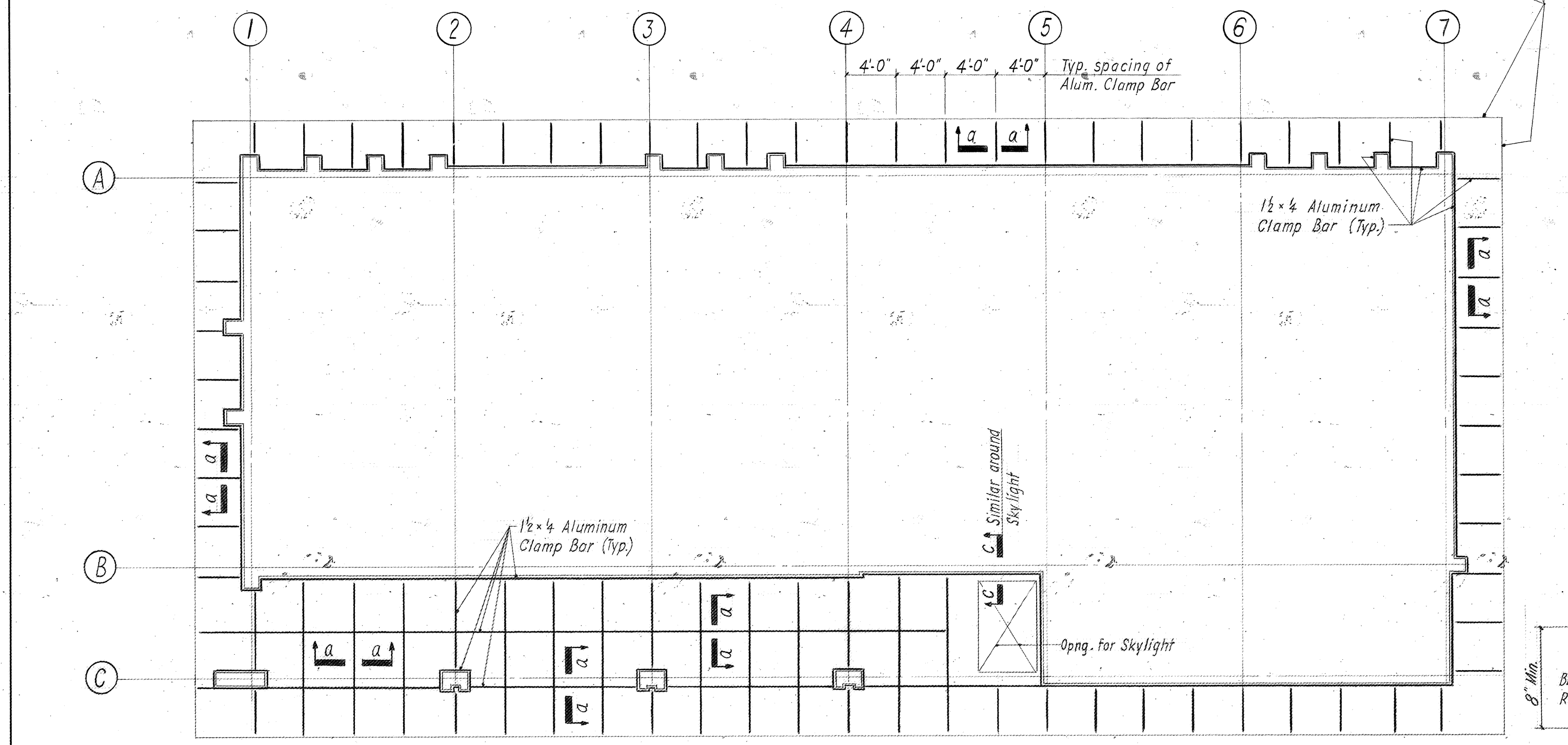
| | |
|---|-----------------------------------|
| COMMONWEALTH OF VIRGINIA | |
| CHESAPEAKE BAY BRIDGE AND TUNNEL DISTRICT | |
| NORFOLK 1, VIRGINIA | |
| SVERDRUP & PARCEL, CONSULTING ENGINEERS NEW YORK, N. Y. - ST. LOUIS, MO. - NORFOLK, VA. | |
| CHESAPEAKE BAY BRIDGE-TUNNEL CROSSING SOUTH TOLL PLAZA TOLL OFFICE BUILDING FLOOR PLAN | |
| RECOMMENDED: <i>P.E. Egan</i> | DRAWN BY: <i>Chas. H. Metcalf</i> |
| CHECKED BY: <i>Chas. H. Metcalf</i> | DATE: 1.16.63 |
| DWG. NO. 4 of 26 | SECTION NO. TF-3 |

1555
62N-174

DO NOT SCALE THIS DRAWING. FOLLOW DIMENSIONS.

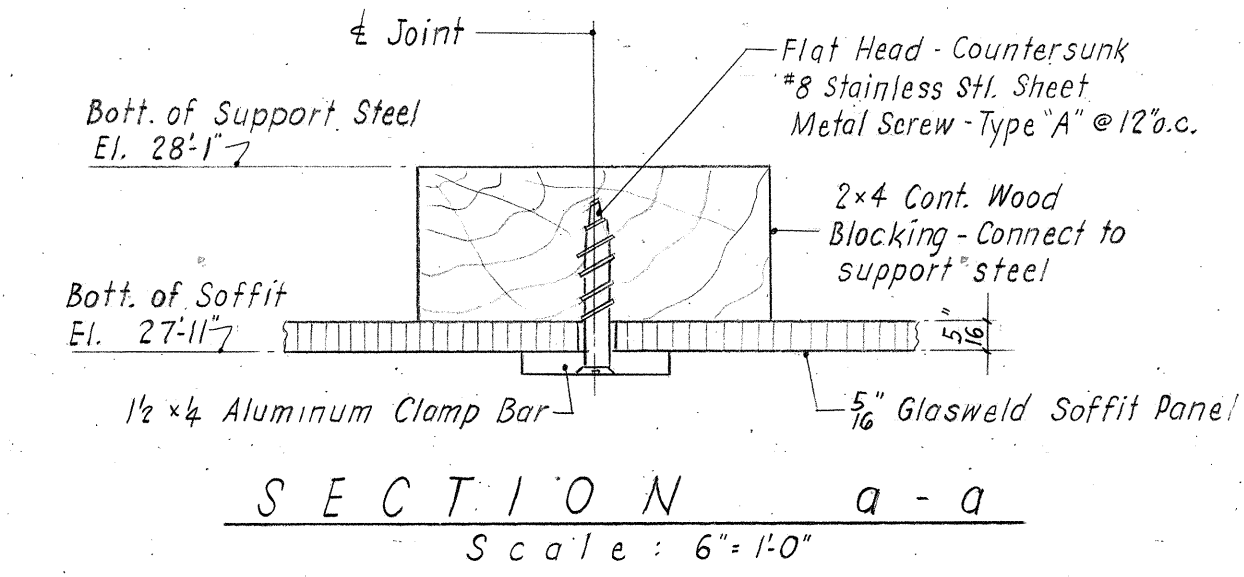


ROOF PLAN
Scale: 1/8" = 1'-0"
For Location of Vents, Diesel Exhaust Stack & Ventilators See Mech. Dwg.

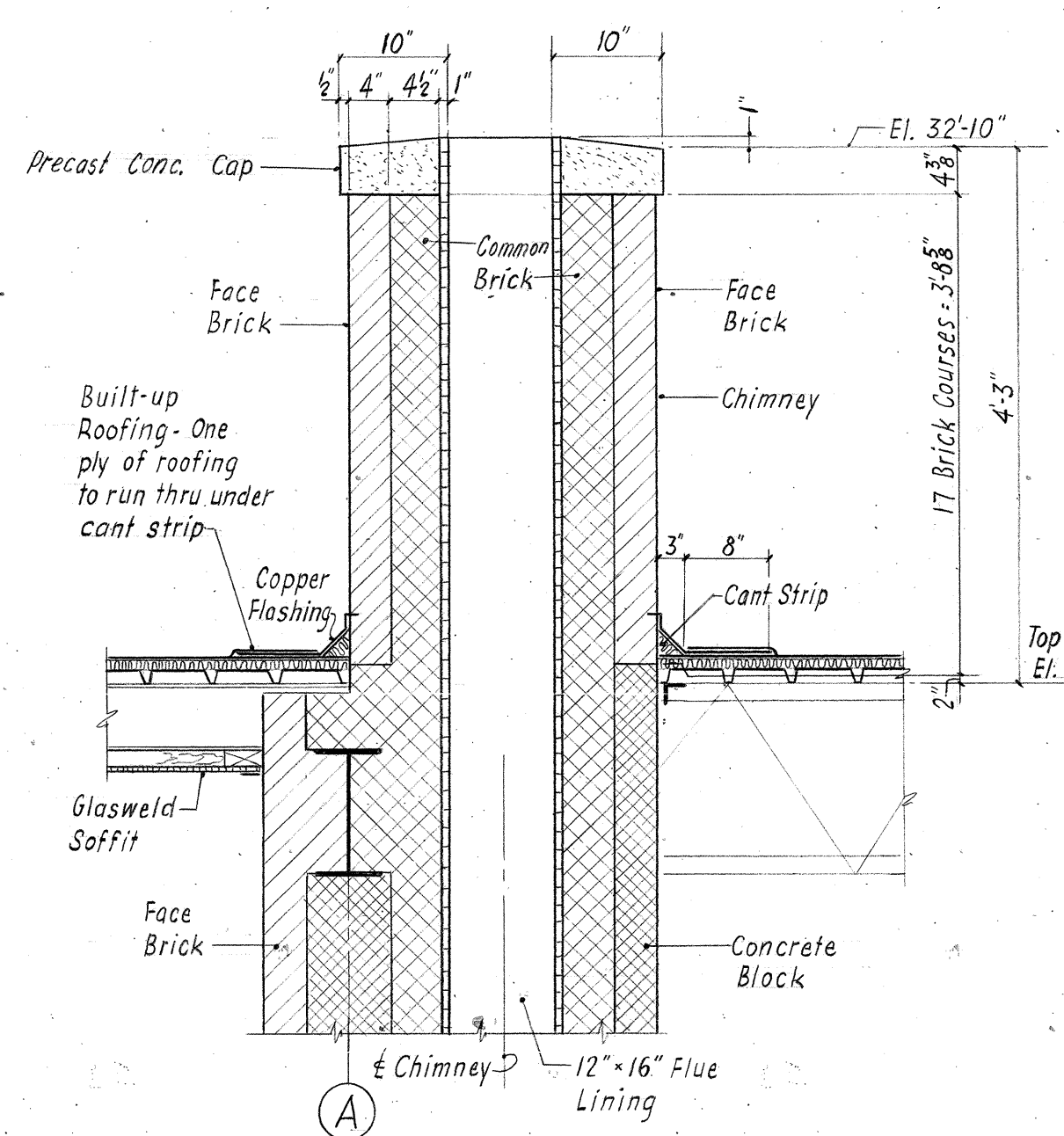


REFLECTED SOFFIT PLAN
Scale: 1/8" = 1'-0"

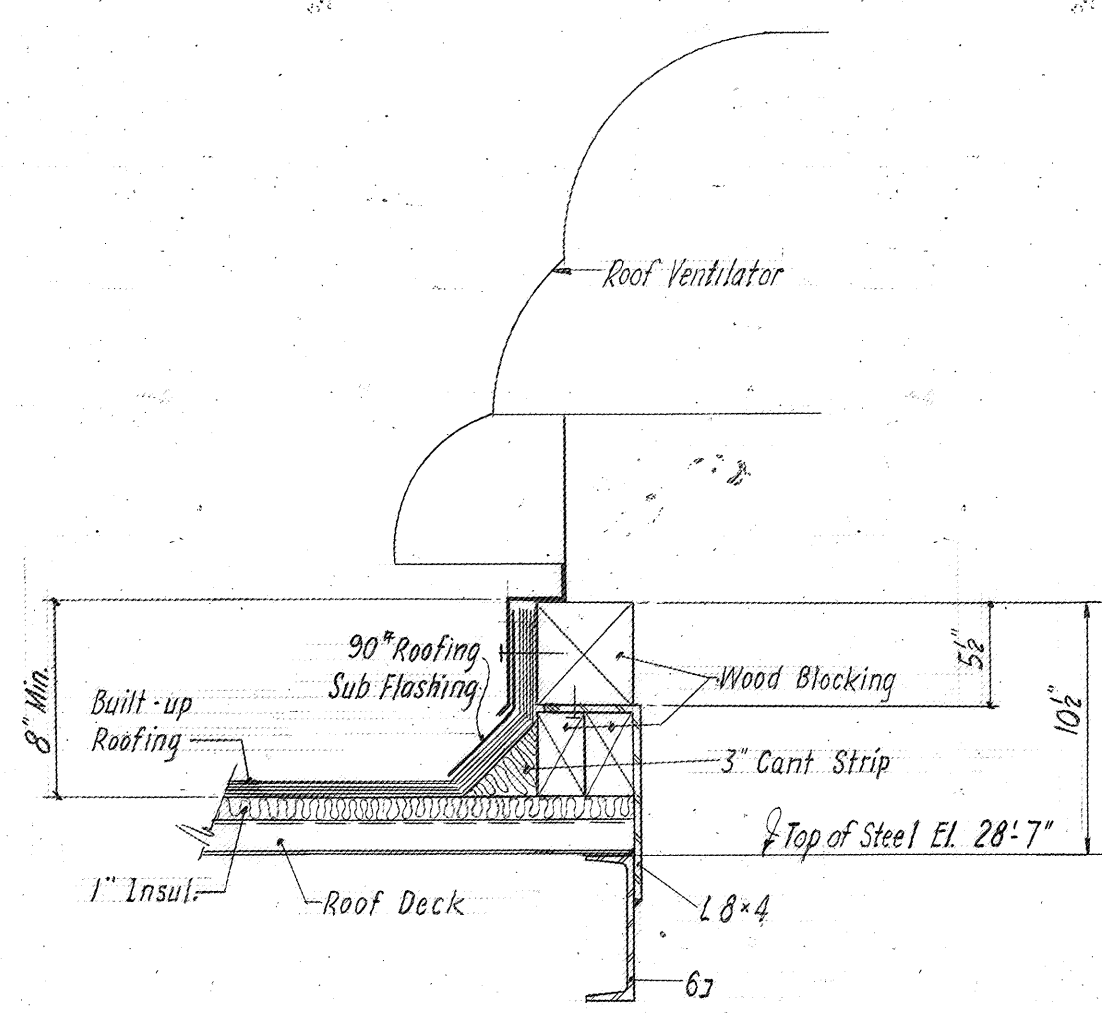
1. Soffit to be set level & flush
2. For Elect. Lighting Fixtures inserted into soffit see Elect. Dwg.



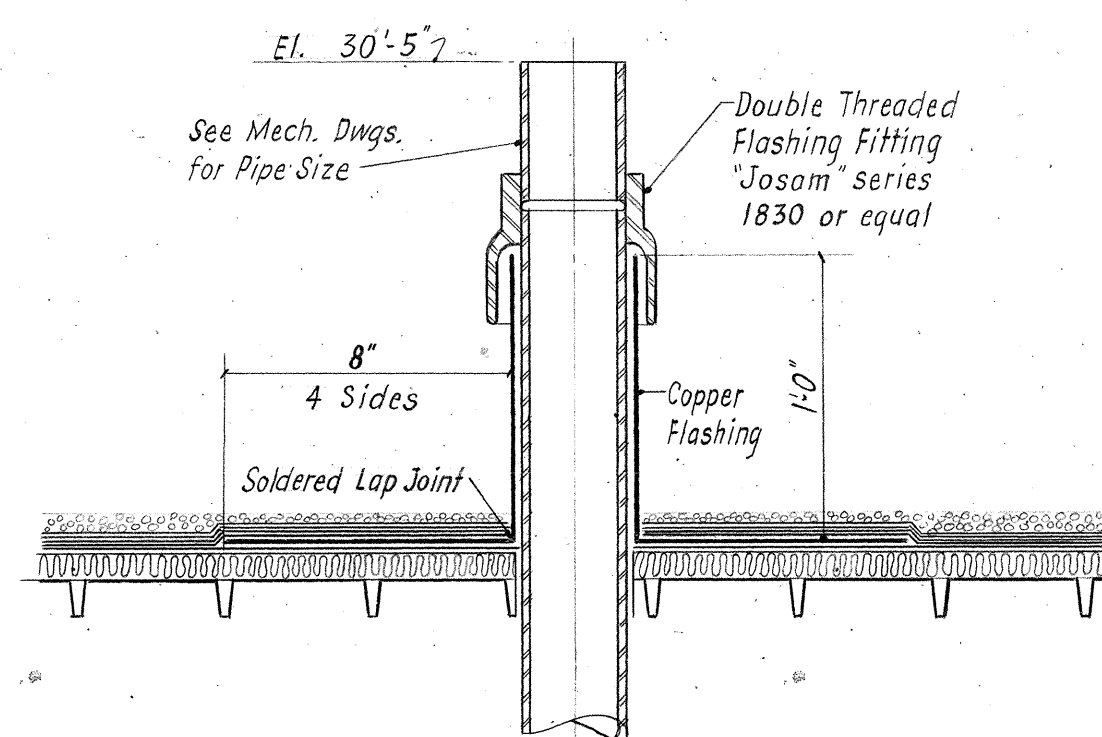
SECTION a-a
Scale: 6" = 1'-0"



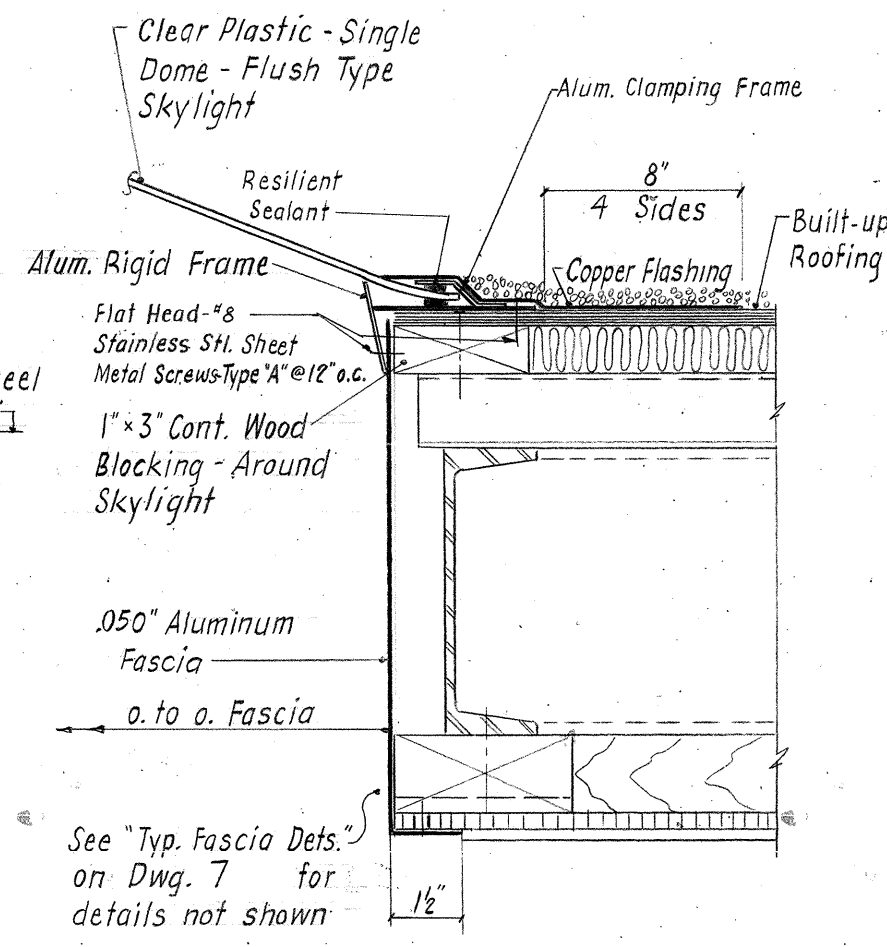
SECTION b-b
SHOWING CHIMNEY
Scale: 3/4" = 1'-0"



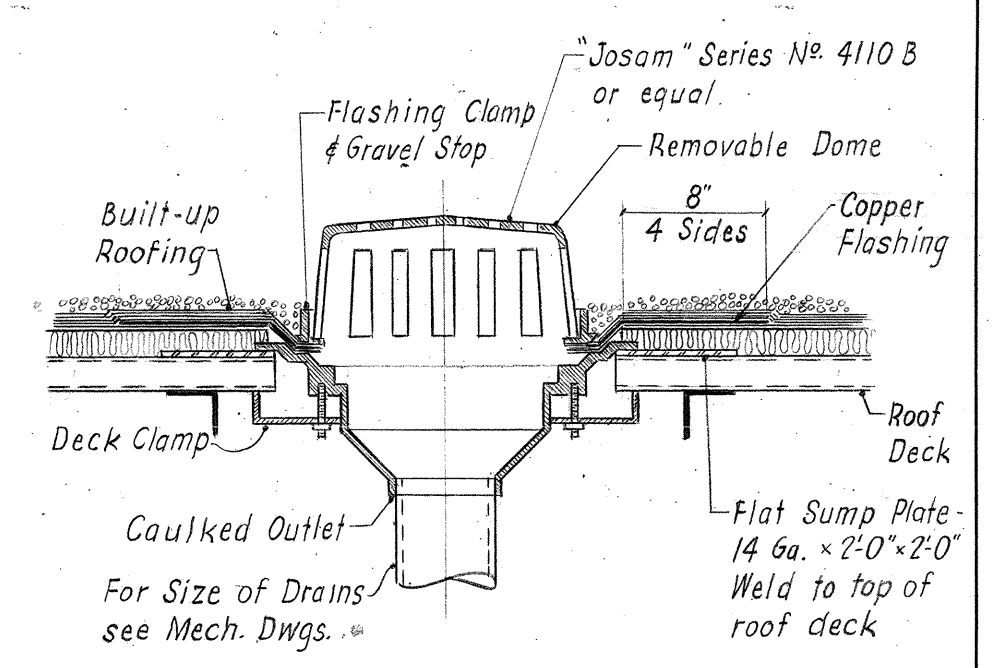
TYPICAL ROOF VENTILATOR DETAIL
Scale: 1/2" = 1'-0"



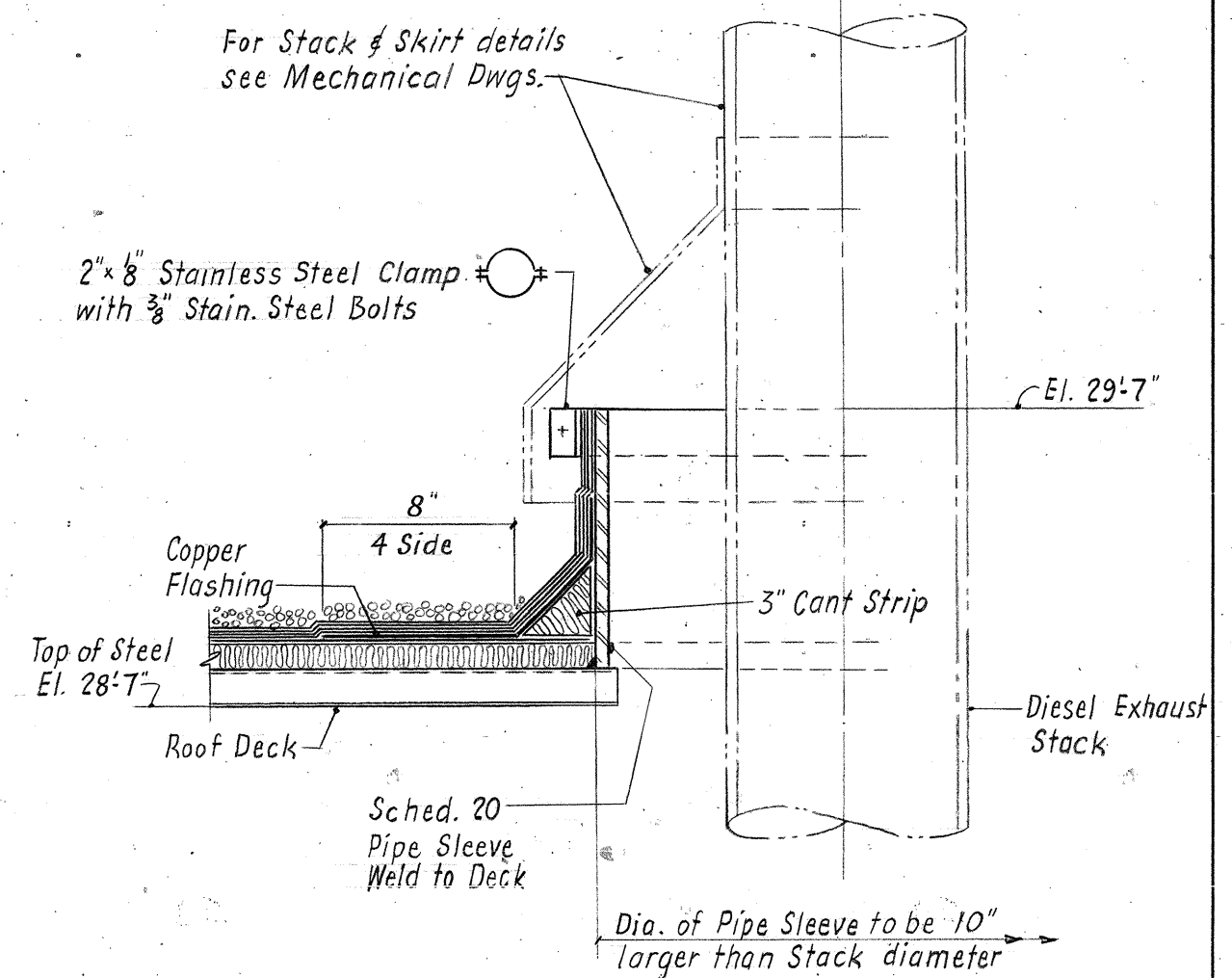
VENT STACK DETAILS
Scale: 1/2" = 1'-0"



SECTION c-c
SHOWING DOME SKYLIGHT
Scale: 3" = 1'-0"



TYP. ROOF DRAIN DETAIL
Scale: 1/2" = 1'-0"



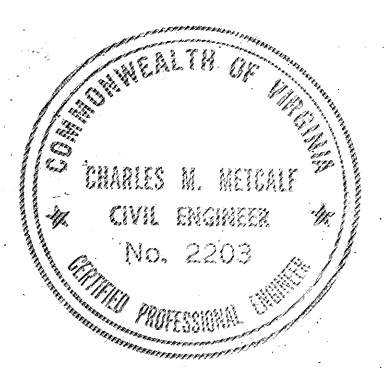
DIESEL EXHAUST STACK DETAILS
Scale: 1/2" = 1'-0"

NOTE: DO NOT SCALE THIS DRAWING. FOLLOW DIMENSIONS.

| APPR. NO. | REVISION |
|-----------|--|
| MTL 2 | 8-14-64 AS BUILT |
| L.F.D. 1 | REV. TYPED LINES, LINES, CAP & ROOFING COVER |
| 5-12-63 | |

RECOMMENDED:
P.E. Egan
APPROVED:
C.M. Hite

DRAWN BY: *W.H.E.* SCALE: AS NOTED
CHECKED BY: *Chivetta* DATE: 1-16-63
DWG. NO. 5 of 26
SECTION NO. TF-3



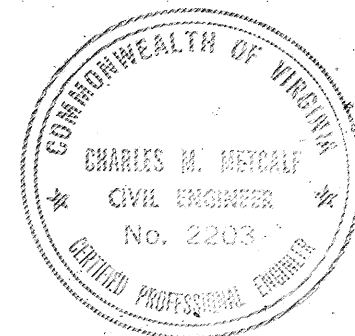
COMMONWEALTH OF VIRGINIA
CHESAPEAKE BAY BRIDGE AND TUNNEL DISTRICT
NORFOLK 1, VIRGINIA
SVERDRUP & PARCEL, CONSULTING ENGINEERS
NEW YORK, N. Y. - ST. LOUIS, MO. - NORFOLK, VA.
CHESAPEAKE BAY BRIDGE-TUNNEL CROSSING
SOUTH TOLL PLAZA
TOLL OFFICE BUILDING
ROOF & SOFFIT PLANS & DETAILS

AS BUILT

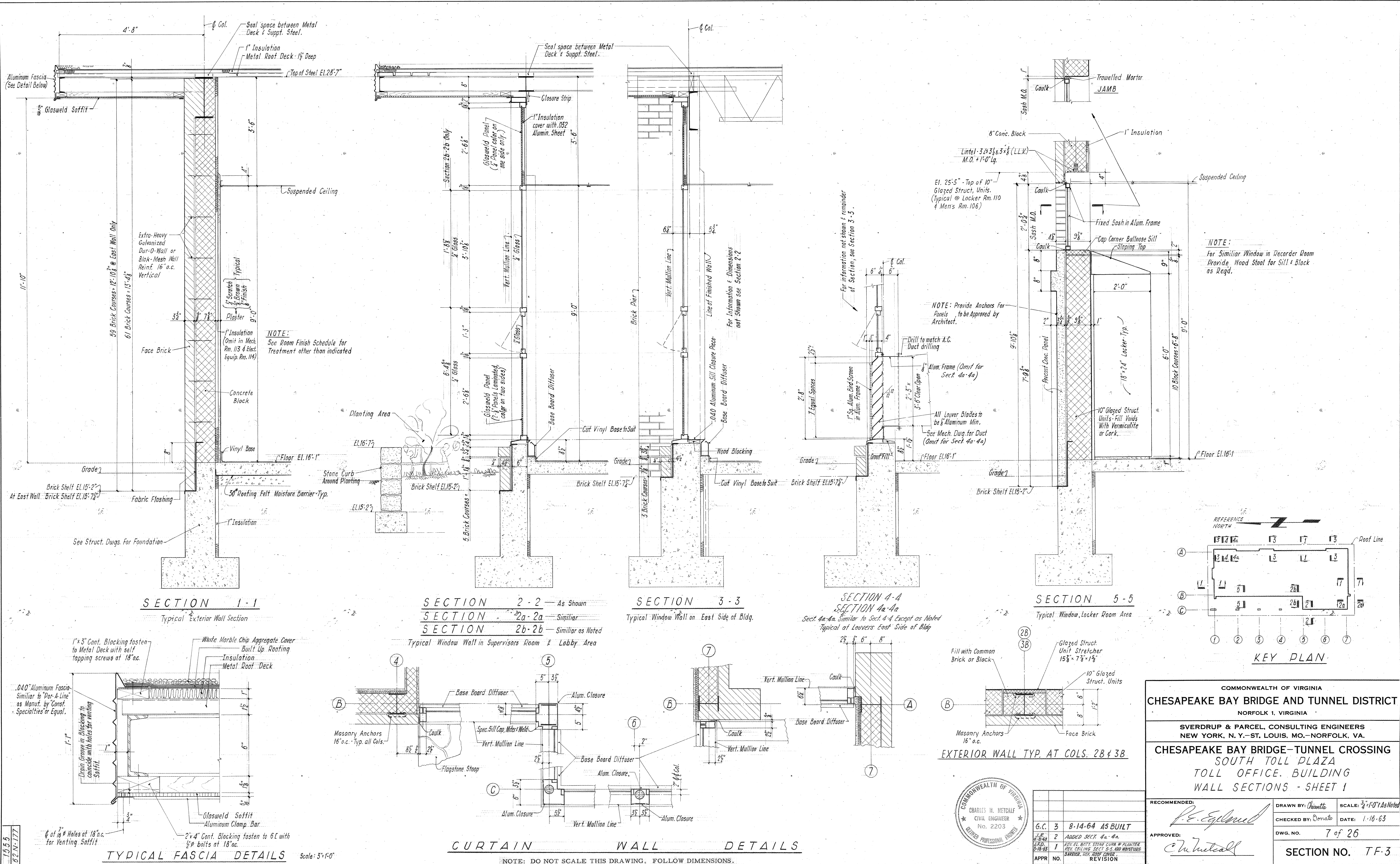


NOTE: DO NOT SCALE THIS DRAWING. FOLLOW DIMENSIONS.

| APPR. NO. | REVISION |
|-----------|---|
| 1 | CHANGED GLASWELD PANELS TO LOUVER AND SCREEN IN EAST ELEVATION |
| 2 | KEY DOORS 108, 116, 117, 118, 119, 120, 121, 122, 123, 124, 125, 126, 127, 128, 129, 130, 131, 132, 133, 134, 135, 136, 137, 138, 139, 140, 141, 142, 143, 144, 145, 146, 147, 148, 149, 150, 151, 152, 153, 154, 155, 156, 157, 158, 159, 160, 161, 162, 163, 164, 165, 166, 167, 168, 169, 170, 171, 172, 173, 174, 175, 176, 177, 178, 179, 180, 181, 182, 183, 184, 185, 186, 187, 188, 189, 190, 191, 192, 193, 194, 195, 196, 197, 198, 199, 200, 201, 202, 203, 204, 205, 206, 207, 208, 209, 210, 211, 212, 213, 214, 215, 216, 217, 218, 219, 220, 221, 222, 223, 224, 225, 226, 227, 228, 229, 230, 231, 232, 233, 234, 235, 236, 237, 238, 239, 240, 241, 242, 243, 244, 245, 246, 247, 248, 249, 250, 251, 252, 253, 254, 255, 256, 257, 258, 259, 260, 261, 262, 263, 264, 265, 266, 267, 268, 269, 270, 271, 272, 273, 274, 275, 276, 277, 278, 279, 280, 281, 282, 283, 284, 285, 286, 287, 288, 289, 290, 291, 292, 293, 294, 295, 296, 297, 298, 299, 300, 301, 302, 303, 304, 305, 306, 307, 308, 309, 310, 311, 312, 313, 314, 315, 316, 317, 318, 319, 320, 321, 322, 323, 324, 325, 326, 327, 328, 329, 330, 331, 332, 333, 334, 335, 336, 337, 338, 339, 340, 341, 342, 343, 344, 345, 346, 347, 348, 349, 350, 351, 352, 353, 354, 355, 356, 357, 358, 359, 360, 361, 362, 363, 364, 365, 366, 367, 368, 369, 370, 371, 372, 373, 374, 375, 376, 377, 378, 379, 380, 381, 382, 383, 384, 385, 386, 387, 388, 389, 390, 391, 392, 393, 394, 395, 396, 397, 398, 399, 400, 401, 402, 403, 404, 405, 406, 407, 408, 409, 410, 411, 412, 413, 414, 415, 416, 417, 418, 419, 420, 421, 422, 423, 424, 425, 426, 427, 428, 429, 430, 431, 432, 433, 434, 435, 436, 437, 438, 439, 440, 441, 442, 443, 444, 445, 446, 447, 448, 449, 450, 451, 452, 453, 454, 455, 456, 457, 458, 459, 460, 461, 462, 463, 464, 465, 466, 467, 468, 469, 470, 471, 472, 473, 474, 475, 476, 477, 478, 479, 480, 481, 482, 483, 484, 485, 486, 487, 488, 489, 490, 491, 492, 493, 494, 495, 496, 497, 498, 499, 500, 501, 502, 503, 504, 505, 506, 507, 508, 509, 510, 511, 512, 513, 514, 515, 516, 517, 518, 519, 520, 521, 522, 523, 524, 525, 526, 527, 528, 529, 530, 531, 532, 533, 534, 535, 536, 537, 538, 539, 540, 541, 542, 543, 544, 545, 546, 547, 548, 549, 550, 551, 552, 553, 554, 555, 556, 557, 558, 559, 560, 561, 562, 563, 564, 565, 566, 567, 568, 569, 570, 571, 572, 573, 574, 575, 576, 577, 578, 579, 580, 581, 582, 583, 584, 585, 586, 587, 588, 589, 590, 591, 592, 593, 594, 595, 596, 597, 598, 599, 600, 601, 602, 603, 604, 605, 606, 607, 608, 609, 610, 611, 612, 613, 614, 615, 616, 617, 618, 619, 620, 621, 622, 623, 624, 625, 626, 627, 628, 629, 630, 631, 632, 633, 634, 635, 636, 637, 638, 639, 640, 641, 642, 643, 644, 645, 646, 647, 648, 649, 650, 651, 652, 653, 654, 655, 656, 657, 658, 659, 660, 661, 662, 663, 664, 665, 666, 667, 668, 669, 670, 671, 672, 673, 674, 675, 676, 677, 678, 679, 680, 681, 682, 683, 684, 685, 686, 687, 688, 689, 690, 691, 692, 693, 694, 695, 696, 697, 698, 699, 700, 701, 702, 703, 704, 705, 706, 707, 708, 709, 710, 711, 712, 713, 714, 715, 716, 717, 718, 719, 720, 721, 722, 723, 724, 725, 726, 727, 728, 729, 730, 731, 732, 733, 734, 735, 736, 737, 738, 739, 740, 741, 742, 743, 744, 745, 746, 747, 748, 749, 750, 751, 752, 753, 754, 755, 756, 757, 758, 759, 760, 761, 762, 763, 764, 765, 766, 767, 768, 769, 770, 771, 772, 773, 774, 775, 776, 777, 778, 779, 780, 781, 782, 783, 784, 785, 786, 787, 788, 789, 790, 791, 792, 793, 794, 795, 796, 797, 798, 799, 800, 801, 802, 803, 804, 805, 806, 807, 808, 809, 810, 811, 812, 813, 814, 815, 816, 817, 818, 819, 820, 821, 822, 823, 824, 825, 826, 827, 828, 829, 830, 831, 832, 833, 834, 835, 836, 837, 838, 839, 840, 841, 842, 843, 844, 845, 846, 847, 848, 849, 850, 851, 852, 853, 854, 855, 856, 857, 858, 859, 860, 861, 862, 863, 864, 865, 866, 867, 868, 869, 870, 871, 872, 873, 874, 875, 876, 877, 878, 879, 880, 881, 882, 883, 884, 885, 886, 887, 888, 889, 890, 891, 892, 893, 894, 895, 896, 897, 898, 899, 900, 901, 902, 903, 904, 905, 906, 907, 908, 909, 910, 911, 912, 913, 914, 915, 916, 917, 918, 919, 920, 921, 922, 923, 924, 925, 926, 927, 928, 929, 930, 931, 932, 933, 934, 935, 936, 937, 938, 939, 940, 941, 942, 943, 944, 945, 946, 947, 948, 949, 950, 951, 952, 953, 954, 955, 956, 957, 958, 959, 960, 961, 962, 963, 964, 965, 966, 967, 968, 969, 970, 971, 972, 973, 974, 975, 976, 977, 978, 979, 980, 981, 982, 983, 984, 985, 986, 987, 988, 989, 990, 991, 992, 993, 994, 995, 996, 997, 998, 999, 1000 |



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|---|---|
| COMMONWEALTH OF VIRGINIA | |
| CHESAPEAKE BAY BRIDGE AND TUNNEL DISTRICT | |
| NORFOLK 1, VIRGINIA | |
| SVERDRUP & PARCEL, CONSULTING ENGINEERS | |
| NEW YORK, N. Y. - ST. LOUIS, MO. - NORFOLK, VA. | |
| CHESAPEAKE BAY BRIDGE-TUNNEL CROSSING | |
| SOUTH TOLL PLAZA | |
| TOLL OFFICE BUILDING | |
| ELEVATIONS & SECTIONS | |
| RECOMMENDED: | DRAWN BY: <i>W.H.R.</i> SCALE: 1/4" = 1'-0" |
| CHECKED BY: <i>Chapman</i> DATE: 1-16-63 | |
| DWG. NO. 6 of 26 | |
| SECTION NO. TF-3 | |



COMMONWEALTH OF VIRGINIA
CHESAPEAKE BAY BRIDGE AND TUNNEL DISTRICT
NORFOLK 1, VIRGINIA

SVERDRUP & PARCEL, CONSULTING ENGINEERS
NEW YORK, N. Y. - ST. LOUIS, MO. - NORFOLK, VA.

CHESAPEAKE BAY BRIDGE-TUNNEL CROSSING
SOUTH TOLL PLAZA
TOLL OFFICE BUILDING
WALL SECTIONS - SHEET 1

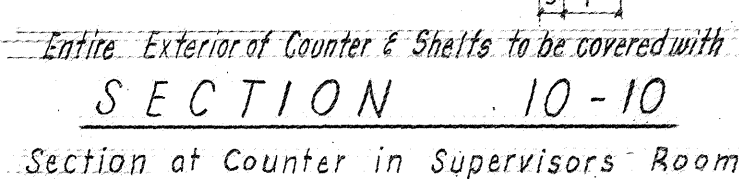
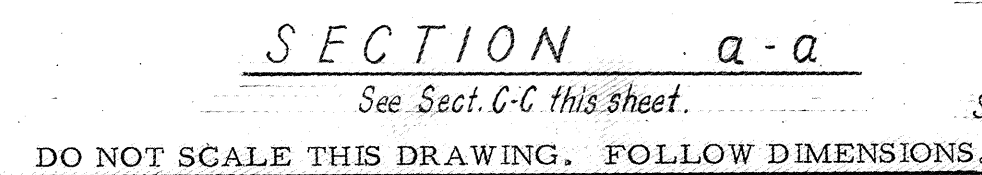
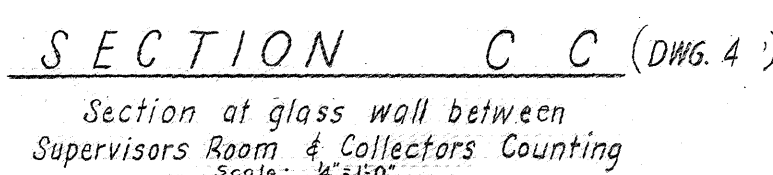
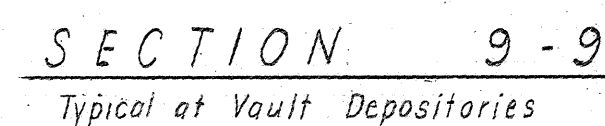
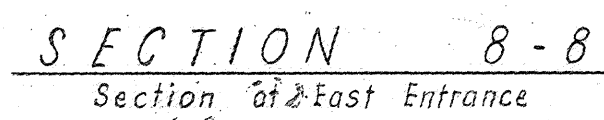
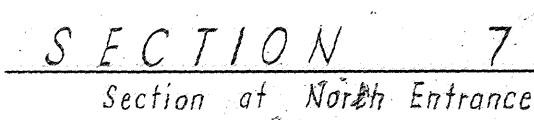
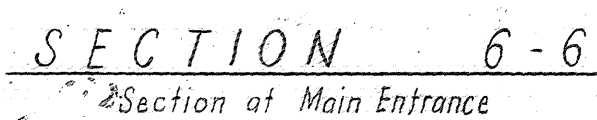
RECOMMENDED:
P. E. Egle

APPROVED:
C. H. Hittell

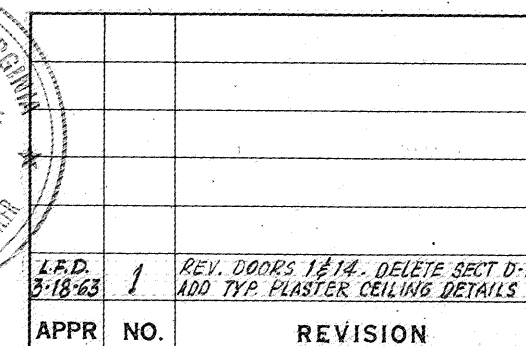
DRAWN BY: J. H. Hittell
CHECKED BY: J. H. Hittell
DWG. NO. 7 of 26
SCALE: 3/4"=1'-0" As Noted
DATE: 1-16-63
SECTION NO. TF-3

REVISION
G.C. 3 8-14-64 AS BUILT
J.E. 2 8-14-64
J.E. 1 8-14-64
APPR NO.

AS BUILT



Note:
Provide anchorage for tubular framed partitions.

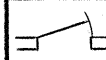

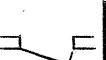

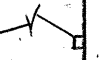




CHESAPEAKE BAY BRIDGE-TUNNEL CROSSING
SOUTH TOLL PLAZA
TOLL OFFICE BUILDING
WALL SECTIONS - SHEET 2

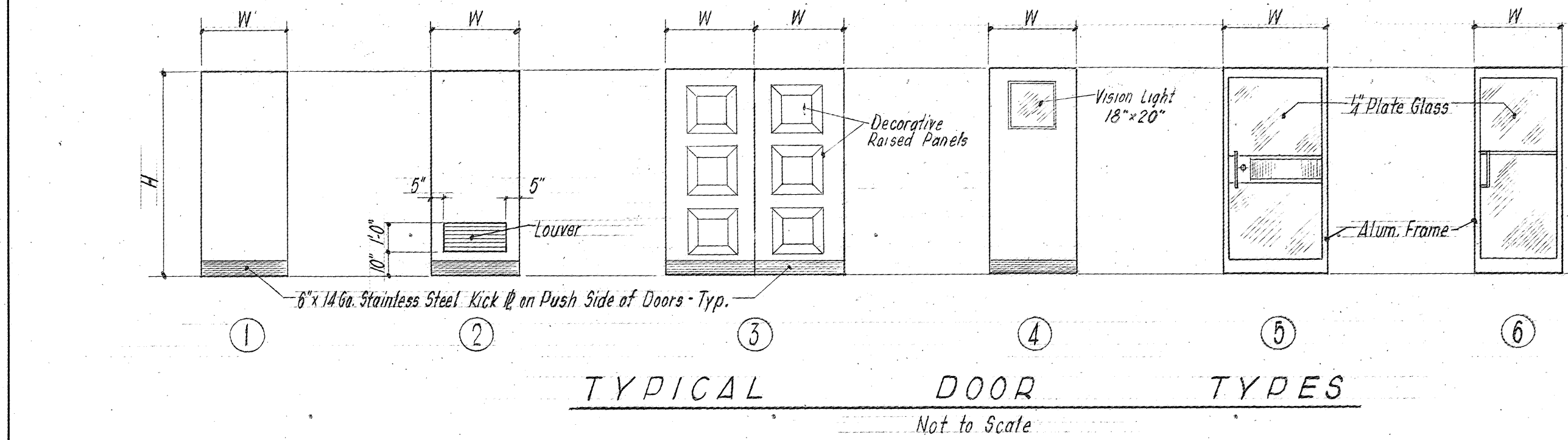
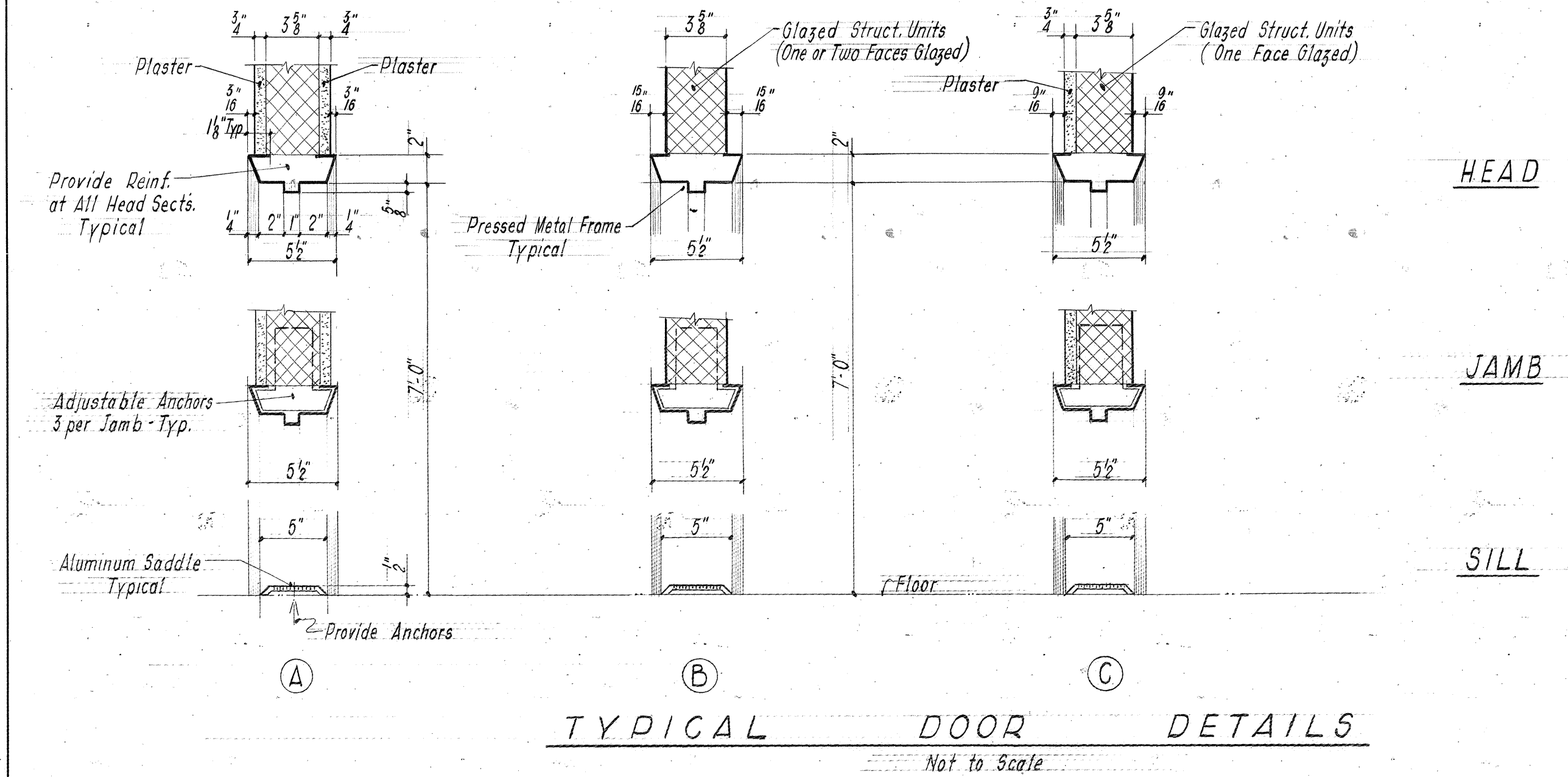
Chittell

SECTION NO. TF-3

| DOOR SCHEDULE | | | | | | | | | |
|---------------|---------------------------|-------|-------|--------|-------|----------------------|----------------------------|--------------------|--------------|
| DOOR NO. | LOCATION | SIZE | | | DOOR | | HEAD, JAMB AND SILL DETAIL | REMARKS | |
| | | "W" | "H" | "T" | KIND | MATERIAL | | | |
| 1 | MECHANICAL (ENTRANCE) | 3'-0" | 7'-0" | 1 1/2" | Swing | Hollow Metal | 3 | See Dwg. 8 | Double Doors |
| 2 | MECHANICAL | 2'-6" | 7'-0" | 1 1/2" | | Hollow Metal | 1 | B | |
| 3 | LOCKER ROOM | 2'-6" | 7'-0" | 1 1/2" | | | 1 | B | |
| 4 | MEN | 3'-0" | 7'-0" | 1 1/2" | | | 2 | C | |
| 5 | LOCKER ROOM | 3'-0" | 7'-0" | 1 1/2" | | | 2 | C | |
| 6 | CORRIDOR | 3'-0" | 7'-0" | 1 1/2" | | | 2 | A | |
| 7 | LOUNGE | 3'-0" | 7'-0" | 1 1/2" | | | 1 | A | |
| 8 | RECORDED | 3'-0" | 7'-0" | 1 1/2" | | | 1 | A | |
| 9 | VESTIBULE (MAIN ENTRANCE) | 3'-6" | 7'-0" | | | Glass in Alum. Frame | 6 | See Dwg. 8 | |
| 10 | VESTIBULE (MAIN ENTRANCE) | 3'-6" | 7'-0" | | | Glass in Alum. Frame | 5 | See Dwg. 8 | |
| 11 | CORRIDOR | 3'-0" | 7'-0" | 1 1/2" | | Hollow Metal | 4 | A | |
| 12 | WOMEN | 2'-6" | 7'-0" | 1 1/2" | | | 2 | C | |
| 13 | MEN | 2'-6" | 7'-0" | 1 1/2" | | | 2 | C | |
| 14 | CORRIDOR (ENTRANCE) | 3'-0" | 7'-0" | 1 1/2" | | | 4 | See Dwg. 8 | |
| 15 | STORAGE | 2'-6" | 7'-0" | 1 1/2" | | Hollow Metal | 2 | A | |
| 16 | STORAGE | 2'-6" | 7'-0" | 1 1/2" | | Hollow Metal | 1 | A | |
| 17 | COLLECTORS COUNTING | 3'-0" | 7'-0" | | | Glass in Alum. Frame | 6 | See Dwg. 8 | |
| 18 | CLOSET | 2'-6" | 7'-0" | 1 1/2" | | Hollow Metal | 1 | A | |
| 19 | VAULT | | | | | | | SEE SPECIFICATIONS | |
| 20 | JANITOR | 2'-6" | 7'-0" | 1 1/2" | | Hollow Metal | 2 | A | |

| HARDWARE SCHEDULE | | | | | | | | | | | | | | | REMARKS | |
|-------------------|---|--|--|--|--|--|-------|----------|----------|-----------|-------------------|--------|--------|--|---------|---|
| DOOR No | DOOR SWING | | | | | | HINGE | LOCK "A" | LOCK "B" | LATCH "C" | FLUSH EXTEN. BOLT | CLOSER | KICK Ø | | | |
| |  LH |  RH |  LH |  RB |  RH |  RB | | | | | | | | | |  DOUBLE RH |
| 1 | | | | | | | • | • | | | • | | • | | | |
| 2 | • | | | | | | • | | • | | | • | • | | | |
| 3 | | • | | | | | • | | | • | | • | • | | | |
| 4 | • | | | | | | • | | | • | | • | • | | | |
| 5 | • | | | | | | • | | | • | | • | • | | | |
| 6 | | | | | | | • | | | | | • | • | | | |
| 7 | | • | | | | | • | | | • | | • | • | | | |
| 8 | • | | | | | | • | | • | | | • | • | | | |
| 9 | | | | | • | | | | | | | | | | | DO NOT MASTER KEY |
| 10 | | | | | | • | | | | | | | | | | SEE SPECIFICATIONS |
| 11 | | • | | | | | • | • | | | | • | • | | | SEE SPECIFICATIONS |
| 12 | | • | | | | | • | | • | | | • | • | | | |
| 13 | • | | | | | | • | | • | | | • | • | | | |
| 14 | | | | • | | | • | • | | | | • | • | | | |
| 15 | | • | | | | | • | | • | | | | • | | | |
| 16 | • | | | | | | • | | • | | | | • | | | |
| 17 | • | | | | | | • | | | | | | | | | SEE SPECIFICATIONS |
| 18 | | | | | • | | • | | | • | | | | | | |
| 19 | | | | | • | | | | | | | | | | | SEE SPECIFICATIONS |
| 20 | | | | | • | | • | | | • | | | • | | | |

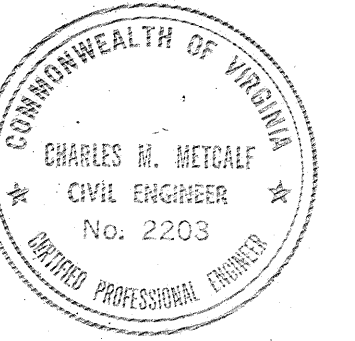
| ROOM FINISH SCHEDULE | | | | | | | | | | | | | | | | REMARKS |
|----------------------|----------------------|----------------|--------------|------------|--------------------|------------------------|--------------------|---------|-----------------|---|--|--|--|--|--|------------------|
| ROOM No | ROOM NAMES | FLOOR | | BASE | | WALLS | | CEILING | | | | | | | | |
| | | VINYL CONCRETE | CERAMIC TILE | VINYL COVE | GLAZED STRUCT COVE | PLASTER CONCRETE BLOCK | GLAZED STRUCT UNIT | HEIGHT | PLASTER ACUSTIC | | | | | | | |
| 101 | SUPERVISORS ROOM | • | | • | | • | | 9' | • | | | | | | | |
| 102 | CLOSET | • | | | | | | | | | | | | | | |
| 103 | COLLECTORS COUNTING | • | | • | | • | | | • | | | | | | | |
| 104 | STORAGE | • | | | | | | | • | | | | | | | |
| 105 | VAULT | • | | • | | • | | | | • | | | | | | |
| 106 | MEN | | • | | • | | • | | • | | | | | | | See Note 2 below |
| 107 | WOMEN | | • | | • | | • | | • | | | | | | | |
| 108 | LOUNGE | • | | • | | • | | | • | | | | | | | |
| 109 | RECORDER | • | | • | | • | | | • | | | | | | | |
| 110 | LOCKER ROOM | • | | | • | | • | | • | | | | | | | See Note 2 below |
| 111 | MEN | | • | | • | | • | | • | | | | | | | |
| 112 | JANITOR | • | | • | | • | | 9' | • | | | | | | | |
| 113 | MECHANICAL | | • | | | | • | | — | — | | | | | | |
| 114 | ELECTRICAL EQUIPMENT | | | | | | • | | — | — | | | | | | |
| 115 | CORRIDOR | • | | • | | • | | 9' | • | | | | | | | |
| 116 | CORRIDOR | • | | • | | • | | | • | | | | | | | |
| 117 | LOBBY | • | | • | | • | | | • | | | | | | | |
| 118 | VESTIBULE | • | | • | | • | | 9' | • | | | | | | | |



NOTES:

1. For Description of Hardware see Specifications.

2. Exterior Wall in Locker Room 110 & Mens Room 106 to be 10" Glazed Structural Units filled with Vermiculite or Cork.



COMMONWEALTH OF VIRGINIA

CHESAPEAKE BAY BRIDGE AND TUNNEL DISTRICT

NORFOLK 1, VIRGINIA

SVERDRUP & PARCEL, CONSULTING ENGINEERS
NEW YORK, N. Y. - ST. LOUIS, MO. - NORFOLK, VA.

CHESAPEAKE BAY BRIDGE-TUNNEL CROSSING
SOUTH TOLL PLAZA
TOLL OFFICE BUILDING
DOOR, HARDWARE & ROOM FINISH SCHEDULES

RECOMMENDED: *P.E. Eglund*

CHECKED BY: *Donato* DATE: 1-16-63

DWG. NO. 9 of 26

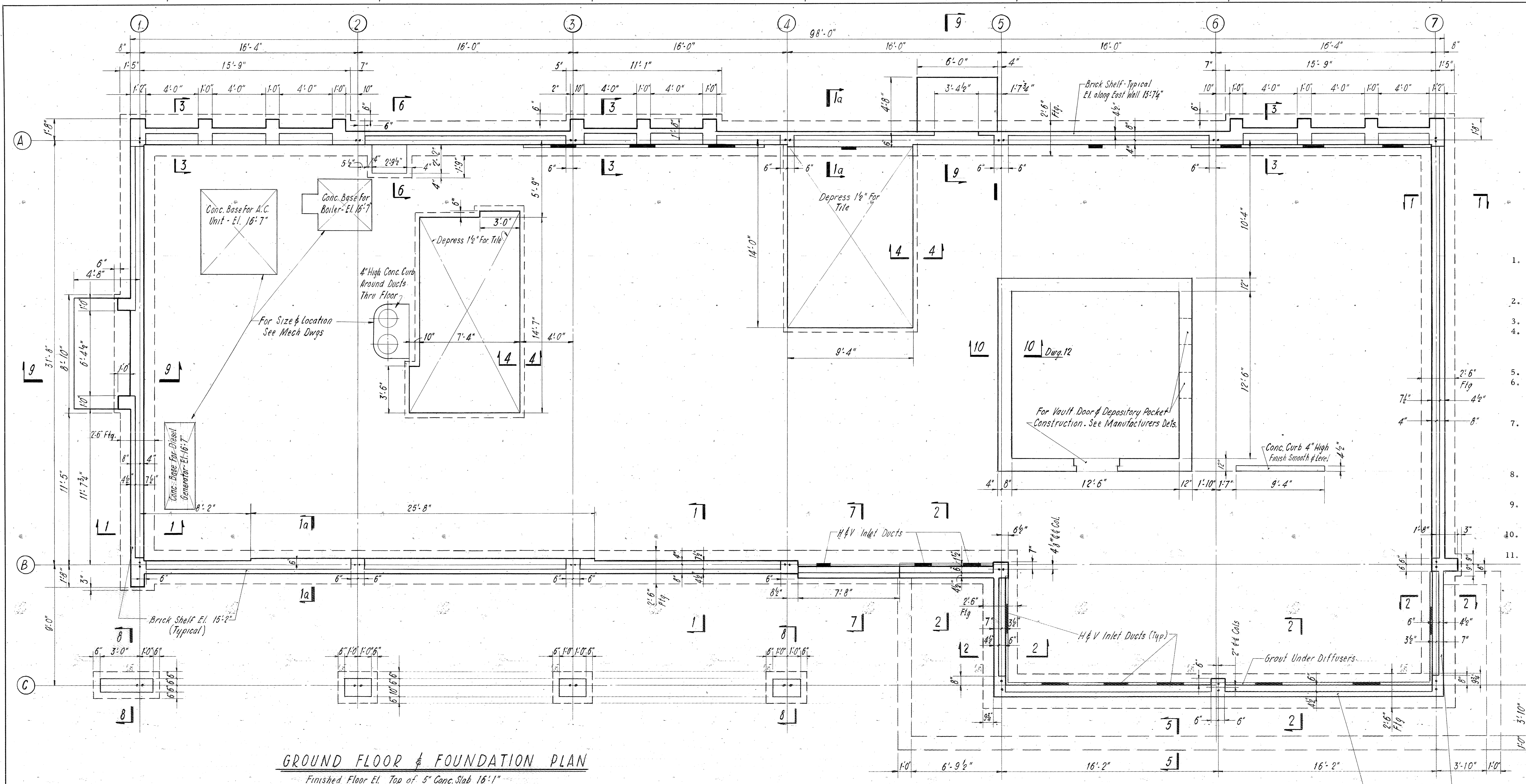
SECTION NO. TF-3

APPROVED: *C.H. Metcalf*

REVISION

1565
62N-179

DO NOT SCALE THIS DRAWING. FOLLOW DIMENSIONS.



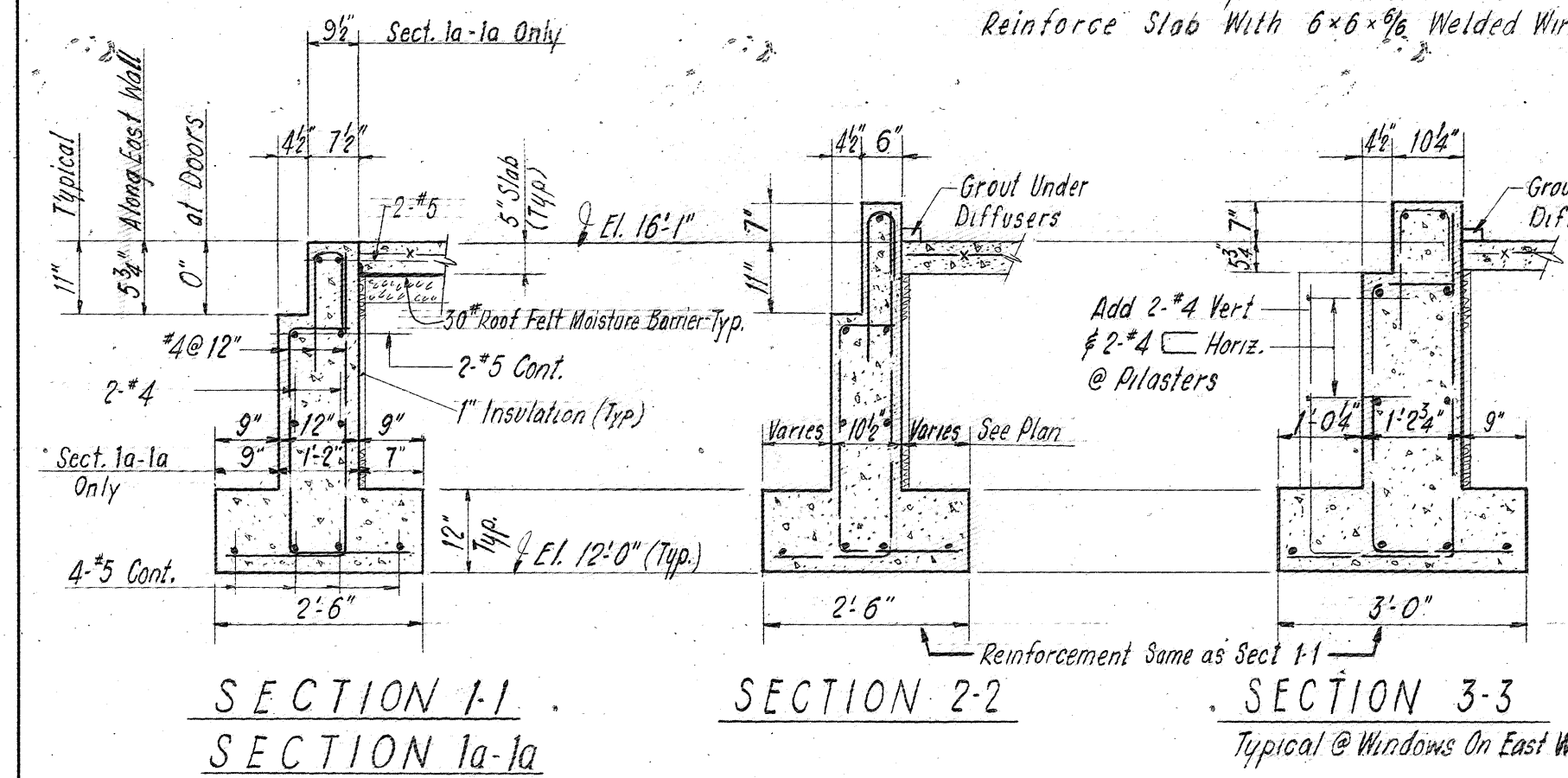
REFERENCE NORTH

GENERAL NOTES

1. CONCRETE DESIGN AND CONSTRUCTION SHALL CONFORM TO THE PROVISIONS OF THE "BUILDING CODE REQUIREMENTS FOR REINFORCED CONCRETE" OF THE AMERICAN CONCRETE INSTITUTE (A. C. I. 318-56).
2. ALL CONCRETE SHALL HAVE AN ULTIMATE COMPRESSIVE STRENGTH OF AT LEAST 3,000 PSI IN 28 DAYS.
3. DESIGN UNIT STRESS FOR REINFORCEMENT IS 20,000 PSI.
4. SPLICES FOR REINFORCEMENT SHALL BE 30 DIAMETERS, EXCEPT FOR BARS HAVING MORE THAN 12 INCHES OF CONCRETE BELOW THEM, WHERE 35 DIAMETERS IS TO BE USED.
5. DESIGN SOIL PRESSURE FOR FOOTINGS IS 3,000 PSF.
6. FOR SIZE AND LOCATION OF PIPE SLEEVES, VENT OPENINGS, DRAINS, ANCHORS, CONDUITS, ETC, SEE ARCHITECTURAL, MECHANICAL AND ELECTRICAL DRAWINGS.
7. CONCRETE COVERING SHALL BE 3 INCHES CLEAR FOR FOOTINGS AND SLABS POURED DIRECTLY ON GROUND, 1/2 INCHES CLEAR FOR WALLS EXPOSED TO GROUND OR WEATHER, AND 1 INCH CLEAR FOR WALLS NOT EXPOSED TO WEATHER.
8. WHEN PADS INDICATED ON DRAWING ARE NOT POURED MONOLITHICALLY WITH FLOOR SLAB, PROVIDE A 1 INCH DEEP POCKET IN SLAB.
9. INSIDE FACE OF FOUNDATION WALL TO HAVE 1 INCH STYROFOAM INSULATION.
10. ALL FILL UNDER GROUND FLOOR SLAB TO BE COMPACTED TO A MINIMUM OF 1,500 PSF.
11. FOR LOCATION, SIZE AND DETAILS OF ANCHOR BOLTS FOR WINDOW WALL SILL ANCHORAGE, SEE MANUFACTURER'S DETAILS. ANCHORS TO BE FURNISHED BY MANUFACTURER.

GROUND FLOOR & FOUNDATION PLAN

Finished Floor El. Top of 5" Conc. Slab 15'-1"
Reinforce Slab With 6x6x1/8 Welded Wire Fabric



SECTION 4-4

SECTION 5-5

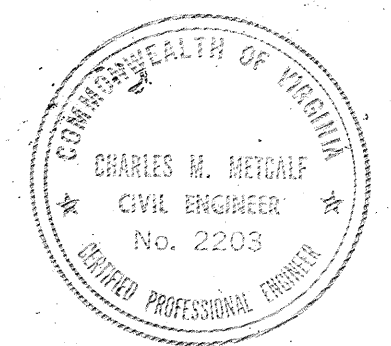
SECTION 6-6

SECTION 7-7

SECTION 8-8

SECTION 9-9

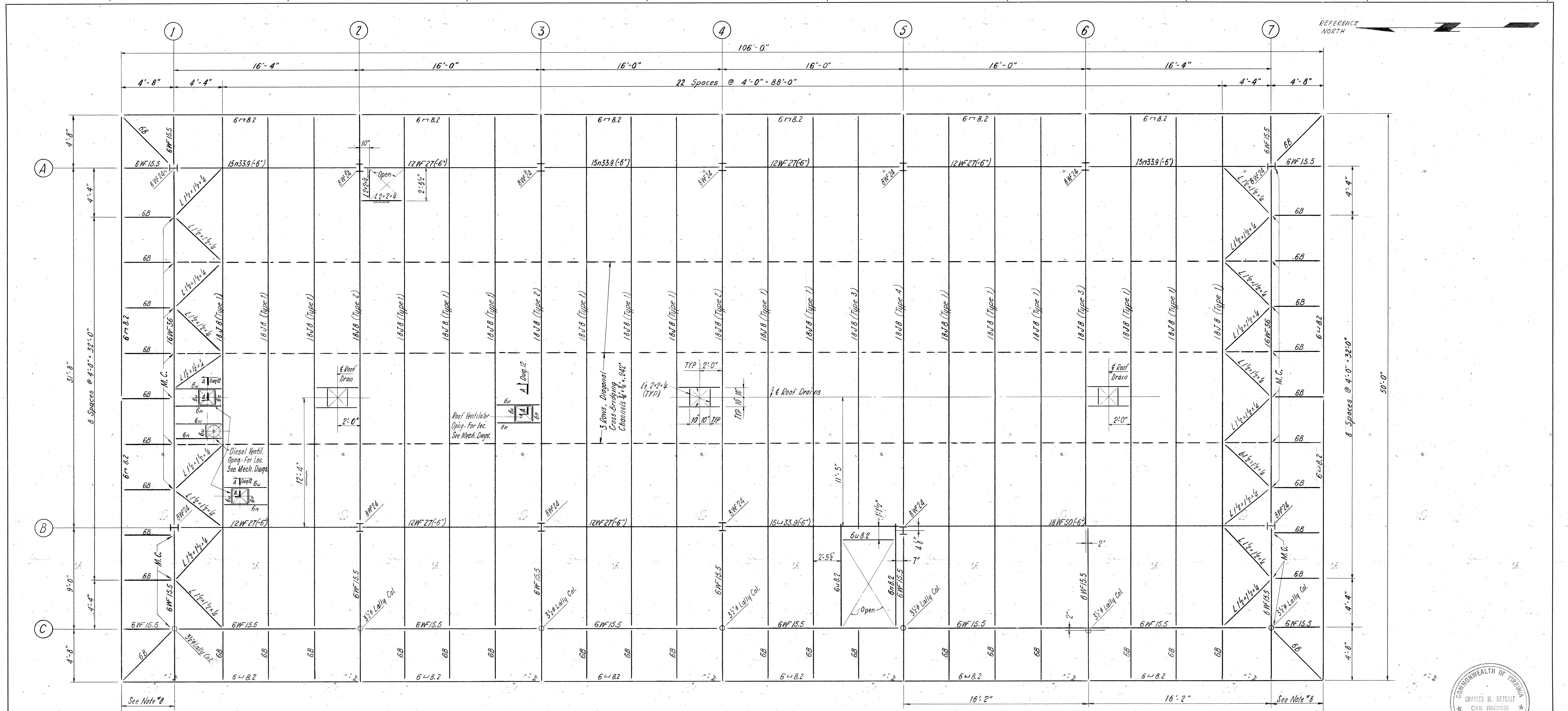
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| COMMONWEALTH OF VIRGINIA | |
| CHESAPEAKE BAY BRIDGE AND TUNNEL DISTRICT | |
| NORFOLK 1, VIRGINIA | |
| SVERDRUP & PARCEL, CONSULTING ENGINEERS NEW YORK, N. Y. - ST. LOUIS, MO. - NORFOLK, VA. | |
| CHESAPEAKE BAY BRIDGE-TUNNEL CROSSING SOUTH TOLL PLAZA TOLL OFFICE BUILDING GROUND FLOOR FOUNDATION PLAN & SECTIONS | |
| RECOMMENDED: <i>[Signature]</i> | DRAWN BY: <i>[Signature]</i> SCALE: 1/4" = 1'-0" |
| CHECKED BY: <i>[Signature]</i> | DATE: 1-16-63 |
| DWG. NO. 10 of 26 | SECTION NO. TF-3 |

| | | | |
|-----------|---|------------------------------|------------------|
| G.C. | 2 | 8-14-64 | AS BUILT |
| C.F.D. | 1 | REV. ELEV. IN SECT. 5-B, ADD | MOISTURE BARRIER |
| APPR. NO. | | REVISION | |

AS BUILT

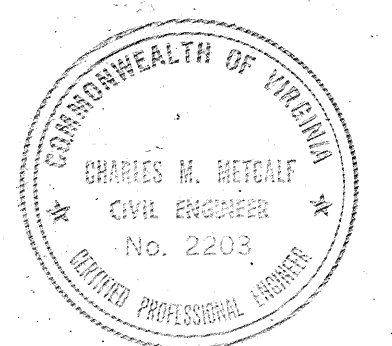


GENERAL NOTES

- ALL STEEL IS TO BE IN ACCORDANCE WITH THE LATEST AMERICAN INSTITUTE OF STEEL CONSTRUCTION "SPECIFICATIONS FOR THE DESIGN, FABRICATION AND ERECTION OF STRUCTURAL STEEL FOR BUILDINGS."
- ALL JOISTS ARE TO BE IN ACCORDANCE WITH THE LATEST STEEL JOIST INSTITUTE "STANDARD SPECIFICATIONS FOR OPEN WEB STEEL JOISTS."
- ALL WELDING TO BE IN ACCORDANCE WITH THE LATEST AMERICAN WELDING SOCIETY SPECIFICATIONS.
- TOP SURFACE OF STEEL TO BE FREE OF ANY PROJECTIONS WHICH WOULD INTERFERE WITH FLUSH SURFACE REQUIRED FOR METAL ROOF DECKING.
- CONNECTIONS MAY BE BOLTED, WELDED OR RIVETED SUBJECT TO ENGINEER'S APPROVAL. USE 3/4" DIAMETER BOLTS OR RIVETS.
- END ANCHORAGE OF JOISTS TO BE ACCOMPLISHED EITHER BY WELDING OR A 2 BOLT CONNECTION.
- ALL CONNECTIONS MARKED M.C. TO HAVE A MOMENT CONNECTION CAPABLE OF DEVELOPING THE FULL CAPACITY OF THE CANTILEVERED MEMBER.
- THIS CANTILEVERED CANOPY MAY BE SHIPPED AS A SHOP WELDED ASSEMBLY.
- ALL STEEL MEMBERS NOTED AS 6B TO BE 6B12.
- DESIGN LIVE LOAD FOR ROOF = 40 P.S.F.
- FOR STEEL DETAILS, SEE DRAWING NO. 12.
- FOR SIZE AND LOCATION OF PIPE SLEEVES, VENT OPENINGS, DRAINS, ETC, SEE ARCHITECTURAL, MECHANICAL AND ELECTRICAL DRAWINGS.
- FURNISH ALL ANCHOR BOLTS WITH ONE WASHER AND HEX NUT UNLESS OTHERWISE NOTED.

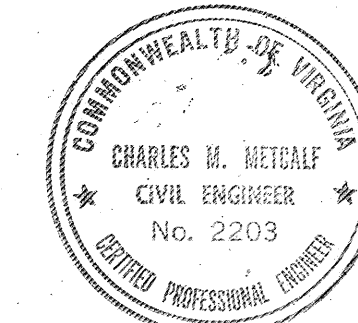
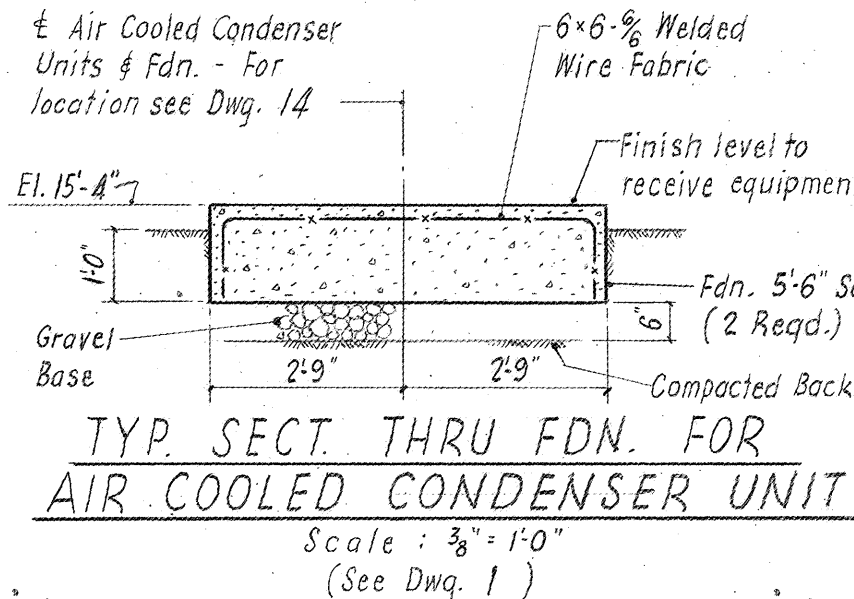
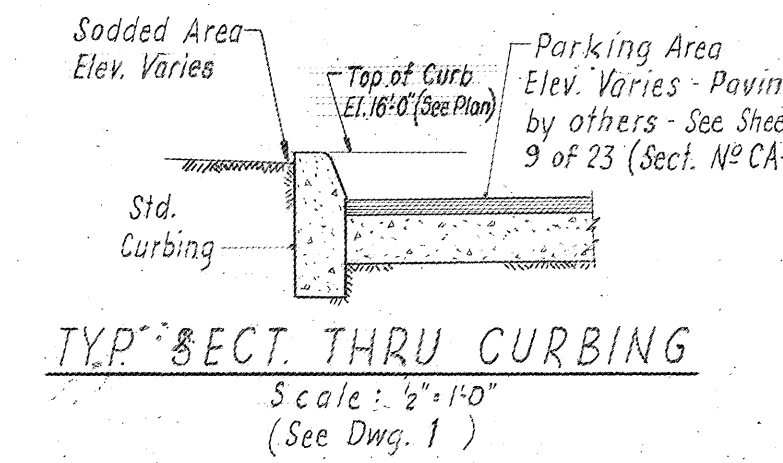
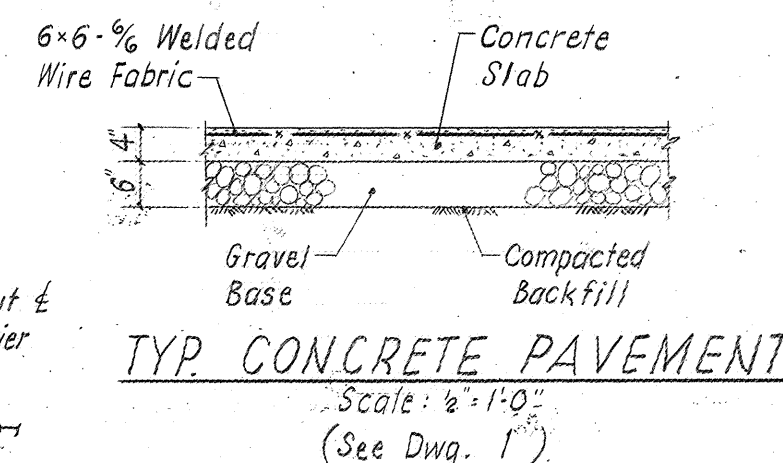
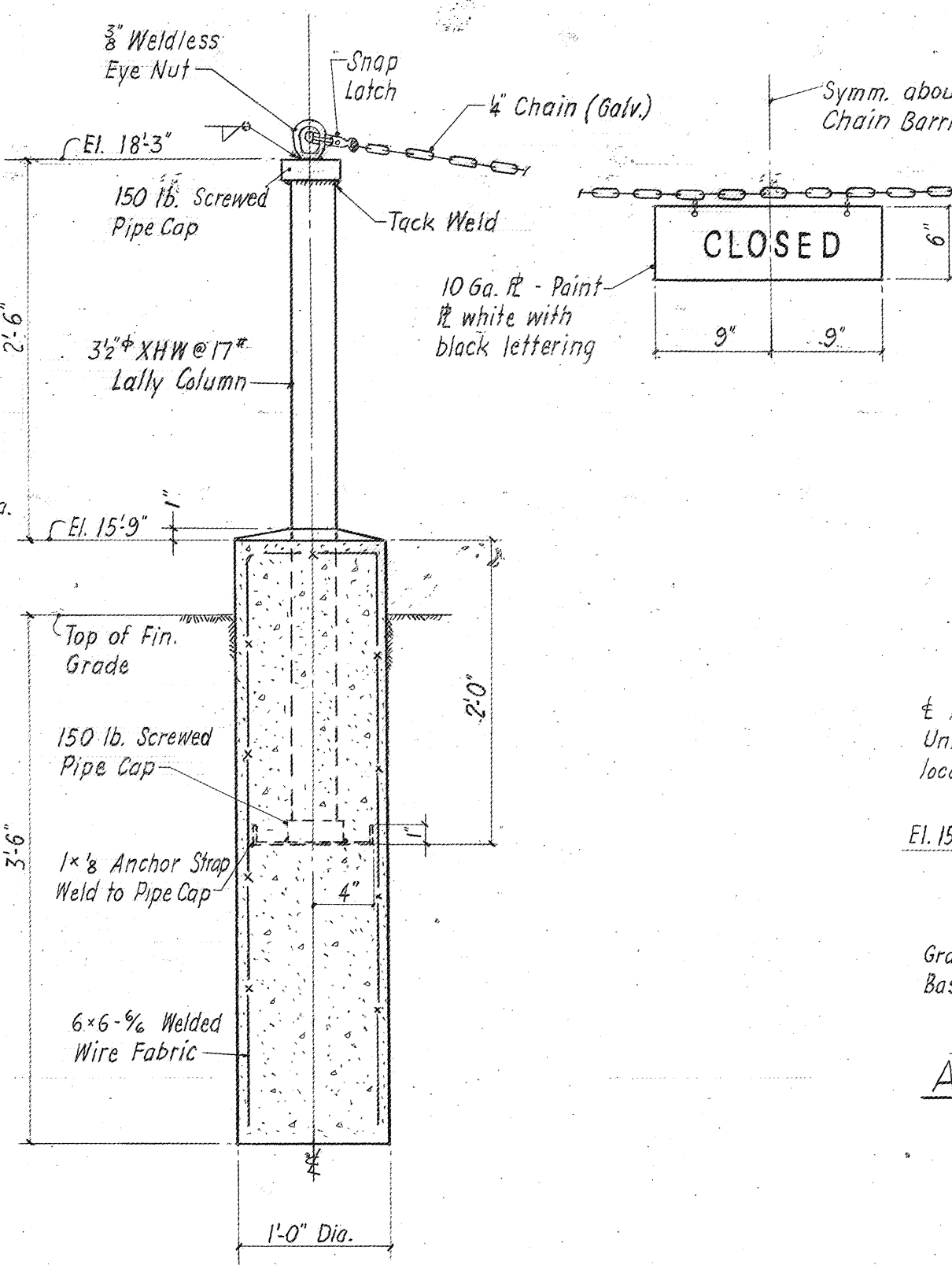
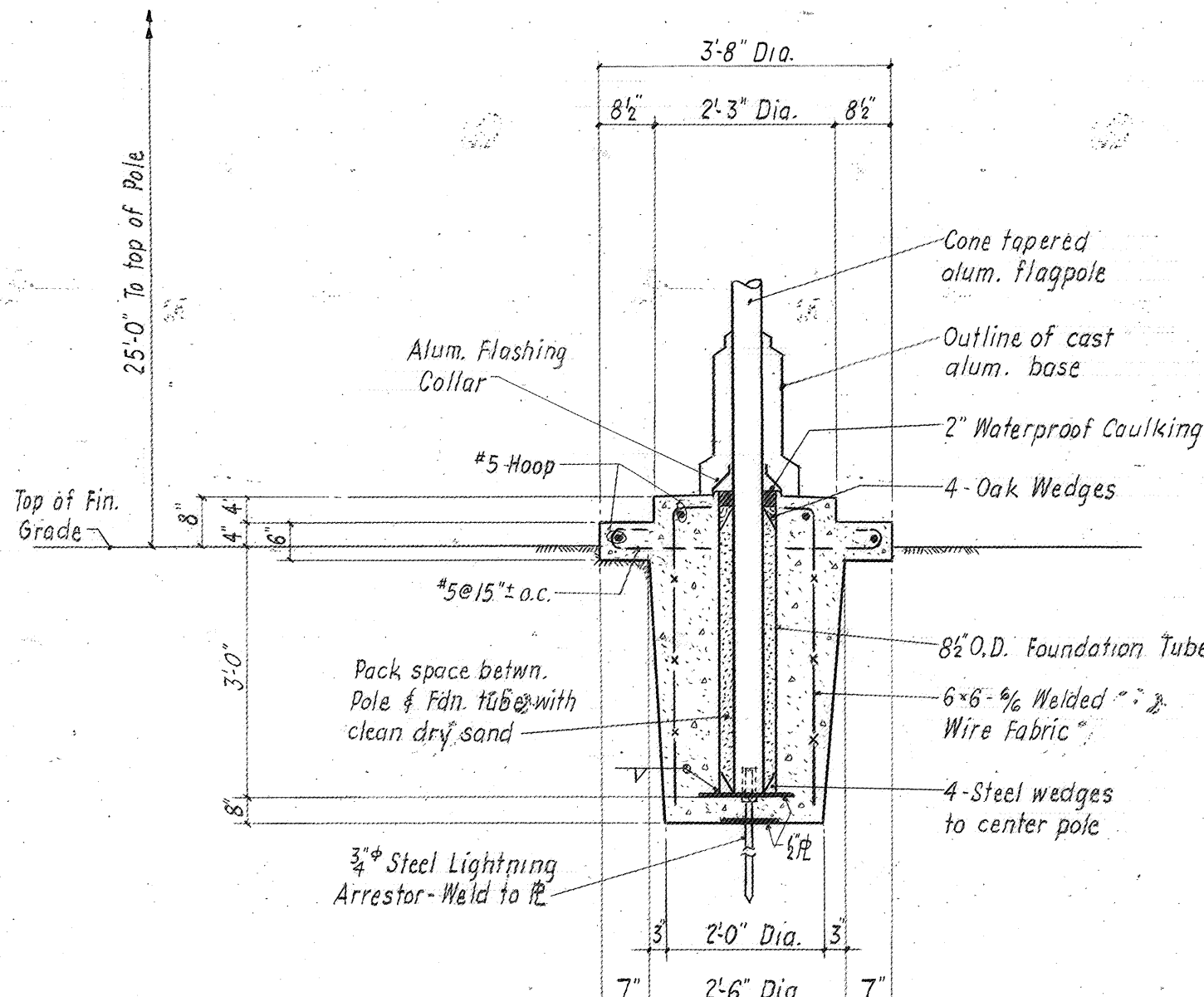
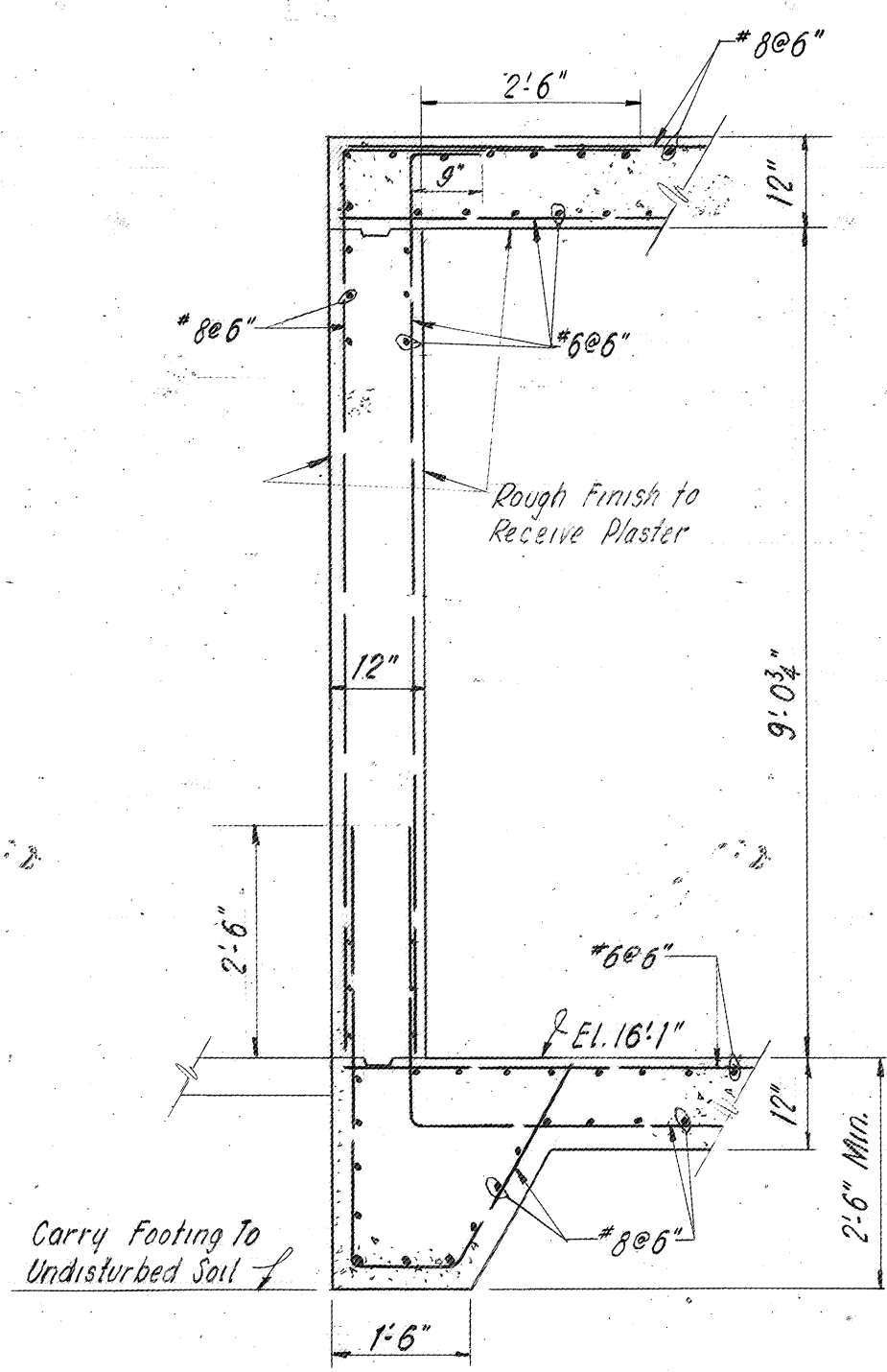
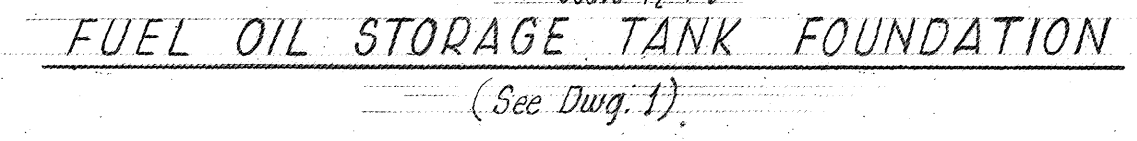
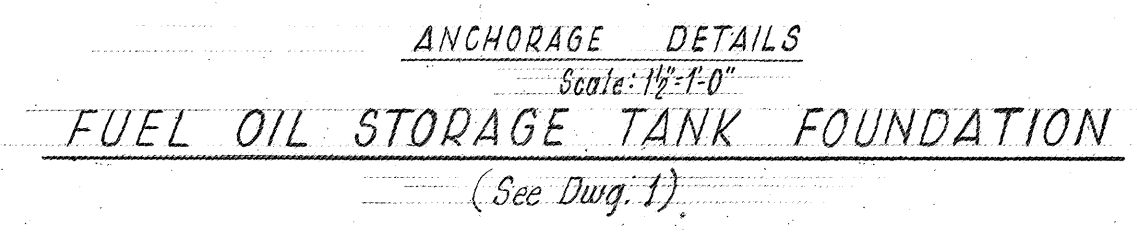
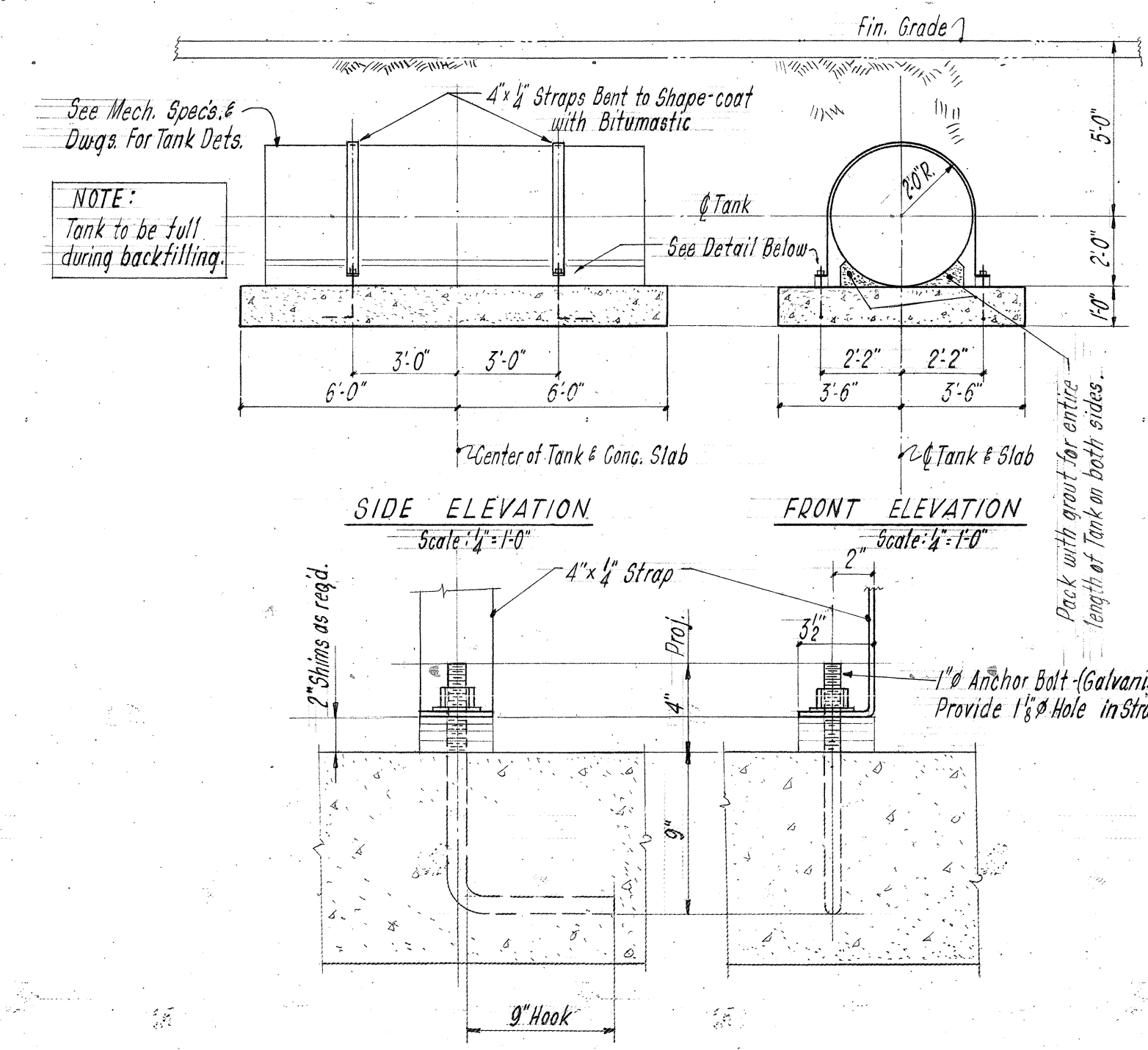
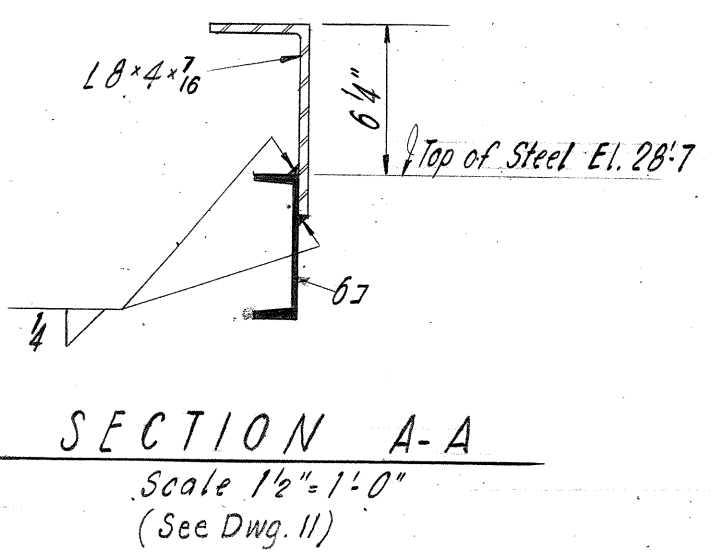
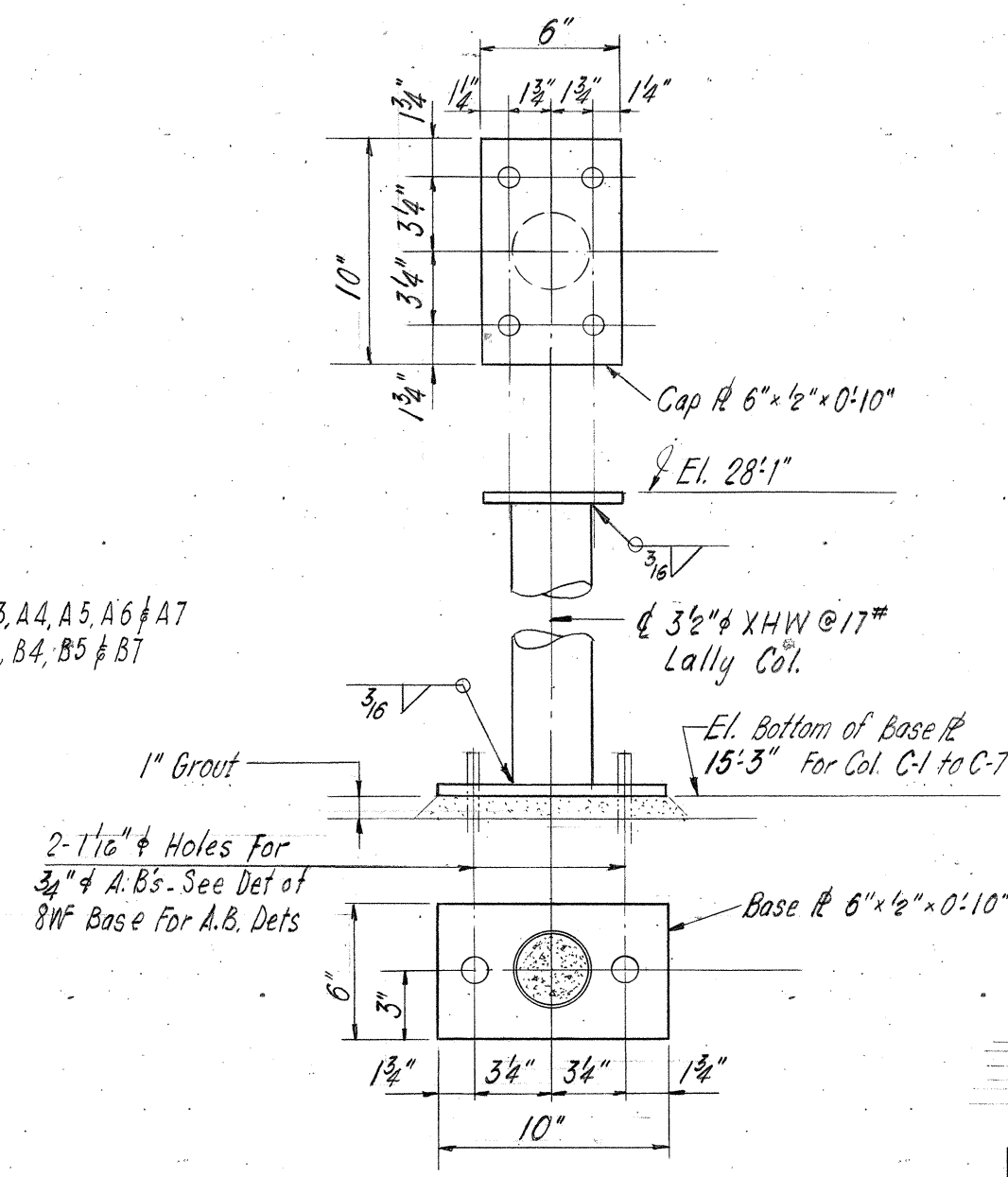
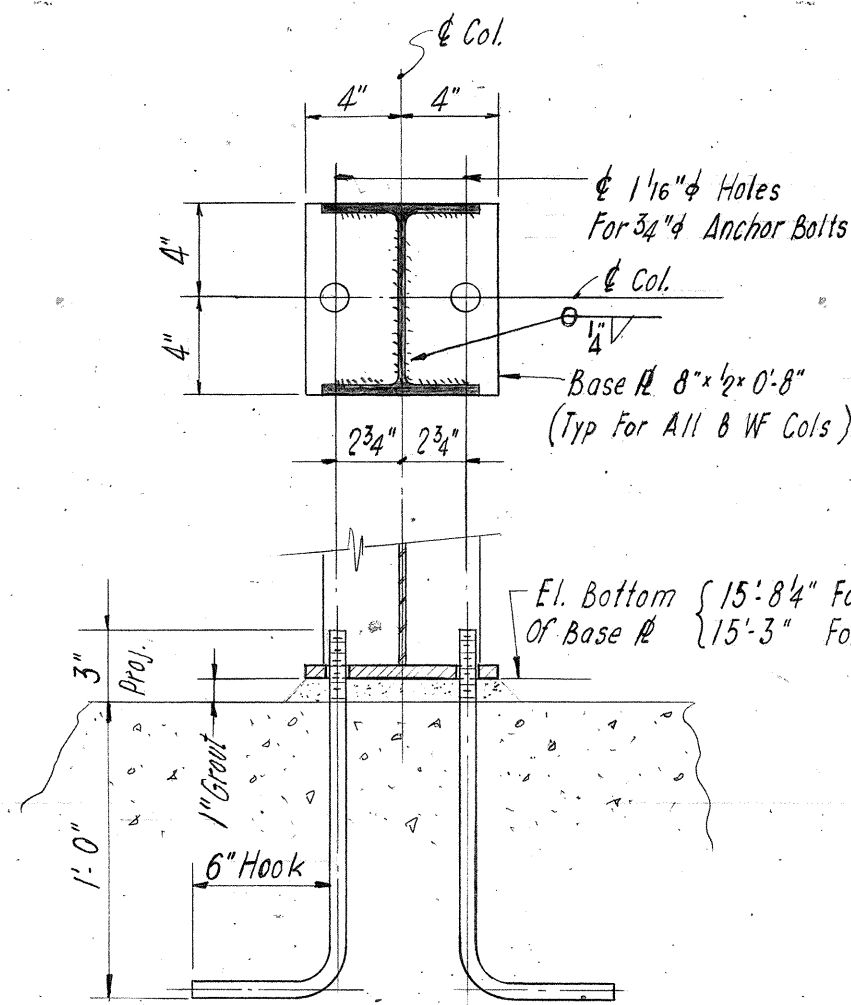
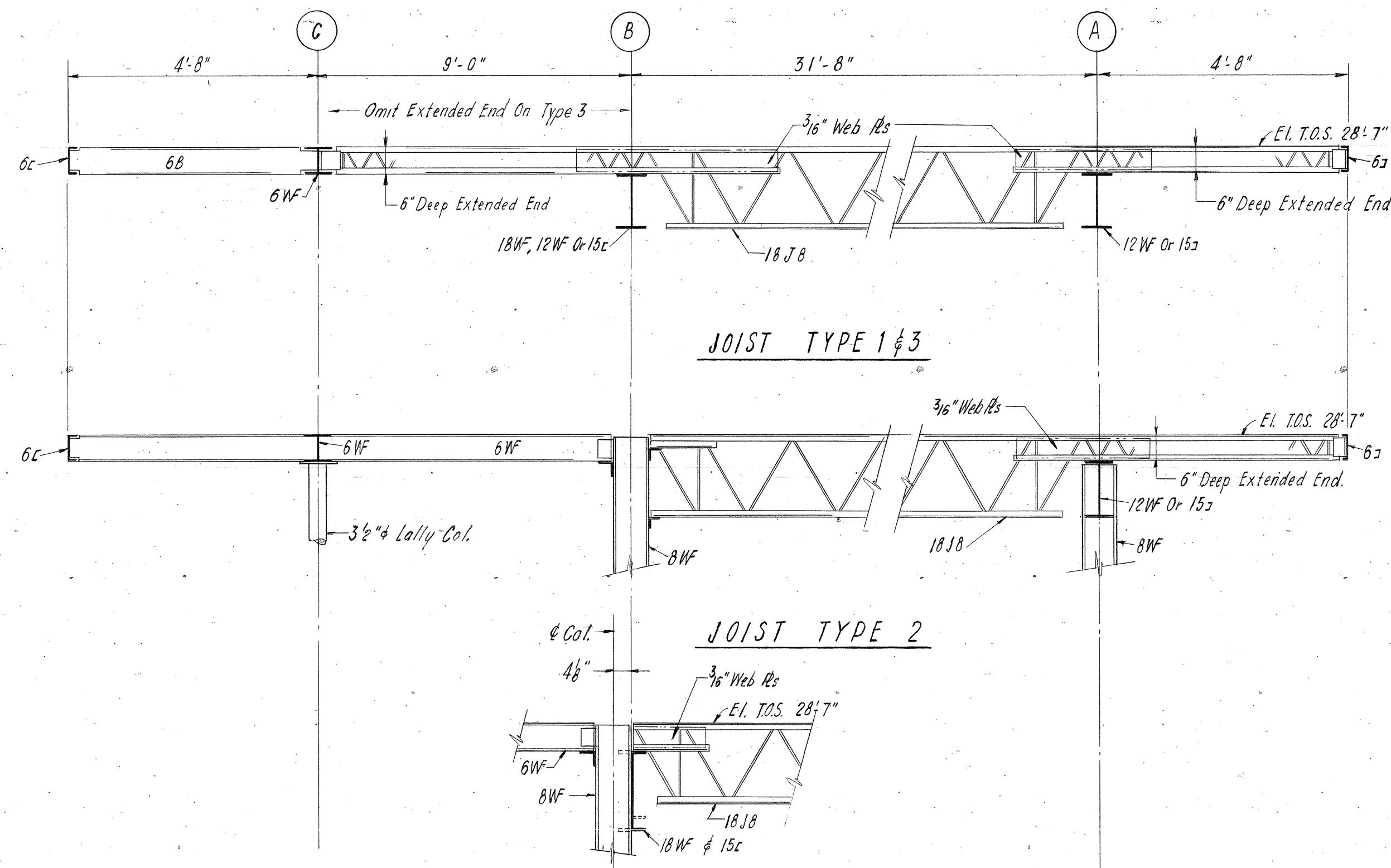
ROOF FRAMING PLAN TOP OF STEEL @ EL. 28'-7" UNLESS NOTED

NOTE: DO NOT SCALE THIS DRAWING. FOLLOW DIMENSIONS.



| | | |
|--|---------------------|---------------------|
| COMMONWEALTH OF VIRGINIA | | |
| CHESAPEAKE BAY BRIDGE AND TUNNEL DISTRICT | | |
| NORFOLK 1, VIRGINIA | | |
| SVERDRUP & PARCEL, CONSULTING ENGINEERS NEW YORK, N. Y. - ST. LOUIS, MO. - NORFOLK, VA. | | |
| CHESAPEAKE BAY BRIDGE-TUNNEL CROSSING SOUTH TOLL PLAZA TOLL OFFICE BUILDING ROOF FRAMING PLAN | | |
| RECOMMENDED: <i>R.E. Eglund</i> | DRAWN BY: J. Rivera | SCALE: 1/4" = 1'-0" |
| CHECKED BY: D. Davis | DATE: 1-16-63 | |
| APPROVED: <i>C. W. Metcalf</i> | DWG. NO. 11 of 26 | |
| SECTION NO. TF-3 | | |

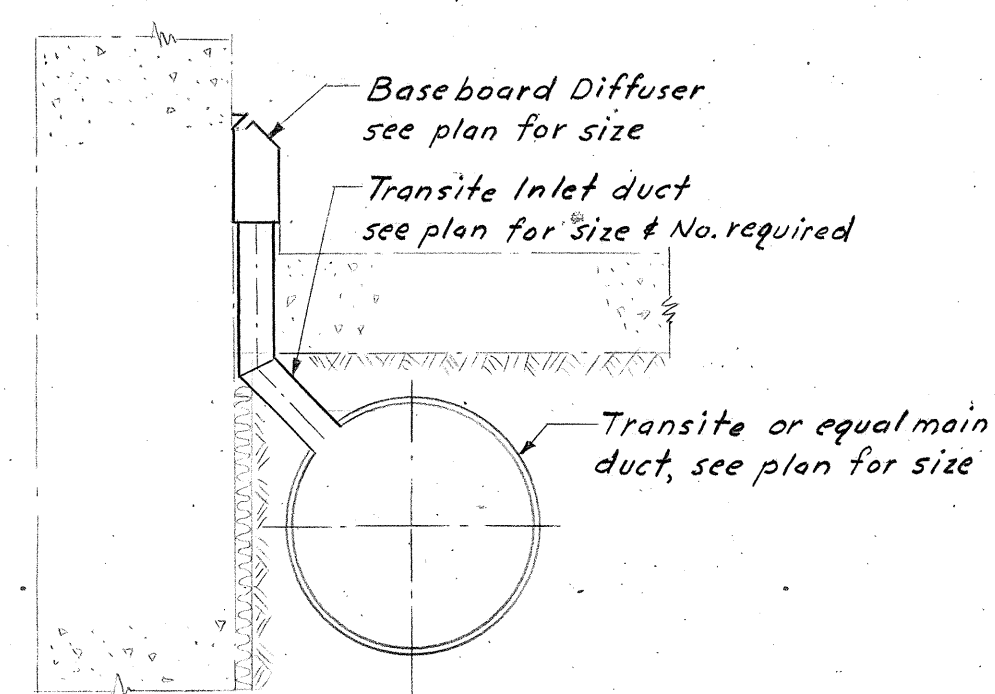
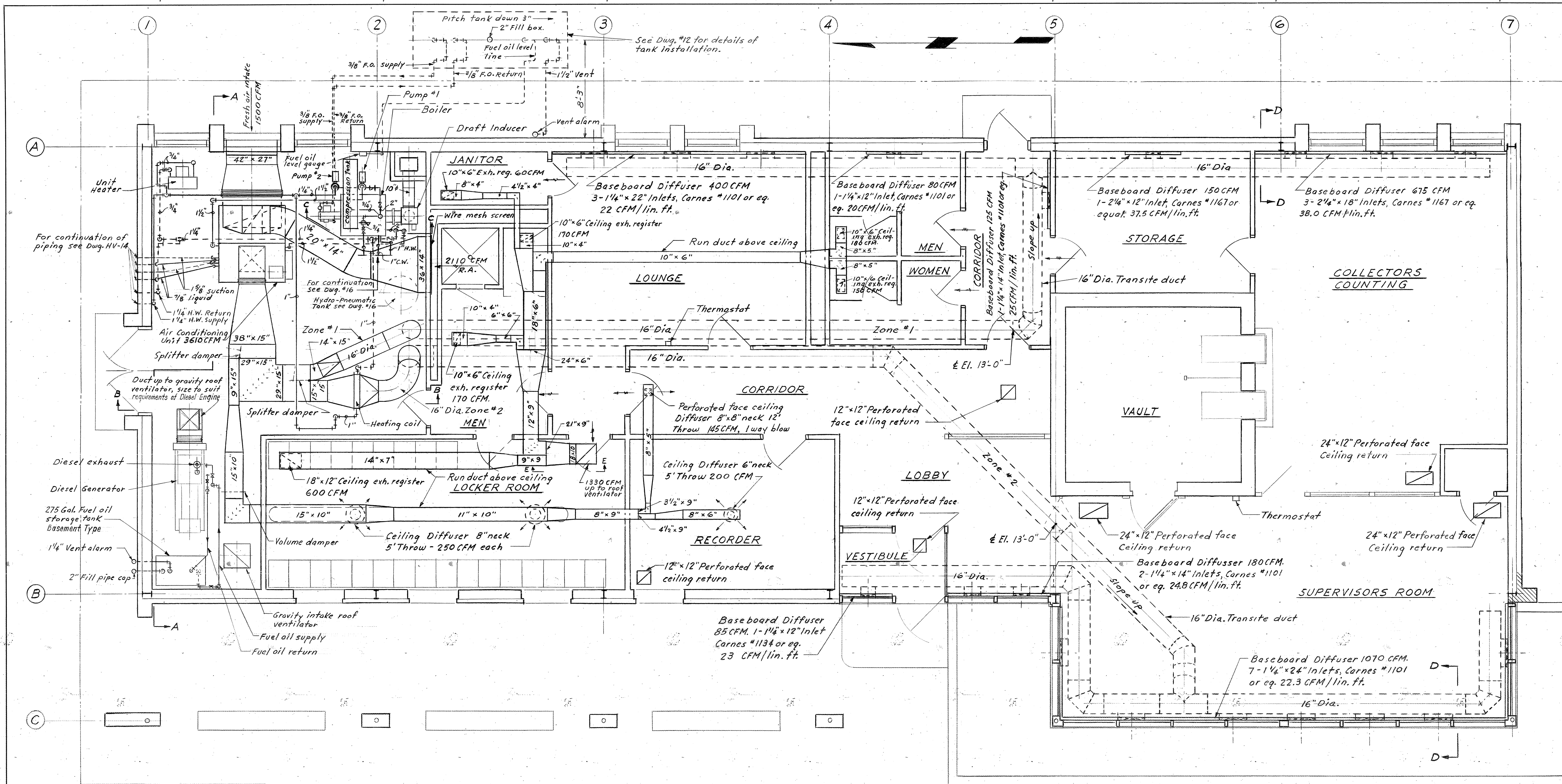
1555
22N171



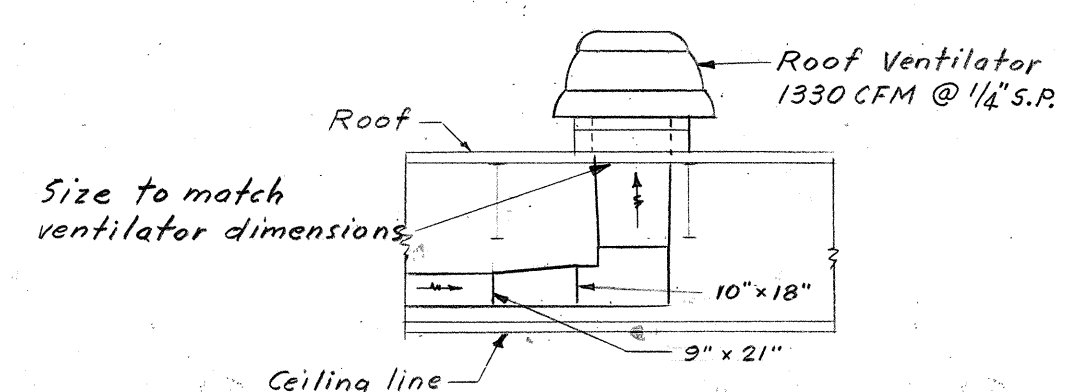
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|--|-------------------------|-----------------|
| COMMONWEALTH OF VIRGINIA | | |
| CHESAPEAKE BAY BRIDGE AND TUNNEL DISTRICT | | |
| NORFOLK 1, VIRGINIA | | |
| SVERDRUP & PARCEL, CONSULTING ENGINEERS NEW YORK, N. Y. - ST. LOUIS, MO. - NORFOLK, VA. | | |
| CHESAPEAKE BAY BRIDGE-TUNNEL CROSSING | | |
| SOUTH TOLL PLAZA | | |
| TOLL OFFICE BUILDING | | |
| SECTIONS & DETAILS | | |
| RECOMMENDED: <i>E. E. Eglund</i> | DRAWN BY: <i>Domato</i> | SCALE: AS NOTED |
| CHECKED BY: <i>Cheneth</i> | DATE: 1-16-63 | |
| APPROVED: <i>Cheneth</i> | DWG. NO. 12 of 26 | |
| SECTION NO. TF-3 | | |

DO NOT SCALE THIS DRAWING. FOLLOW DIMENSIONS.

1555
62N/12



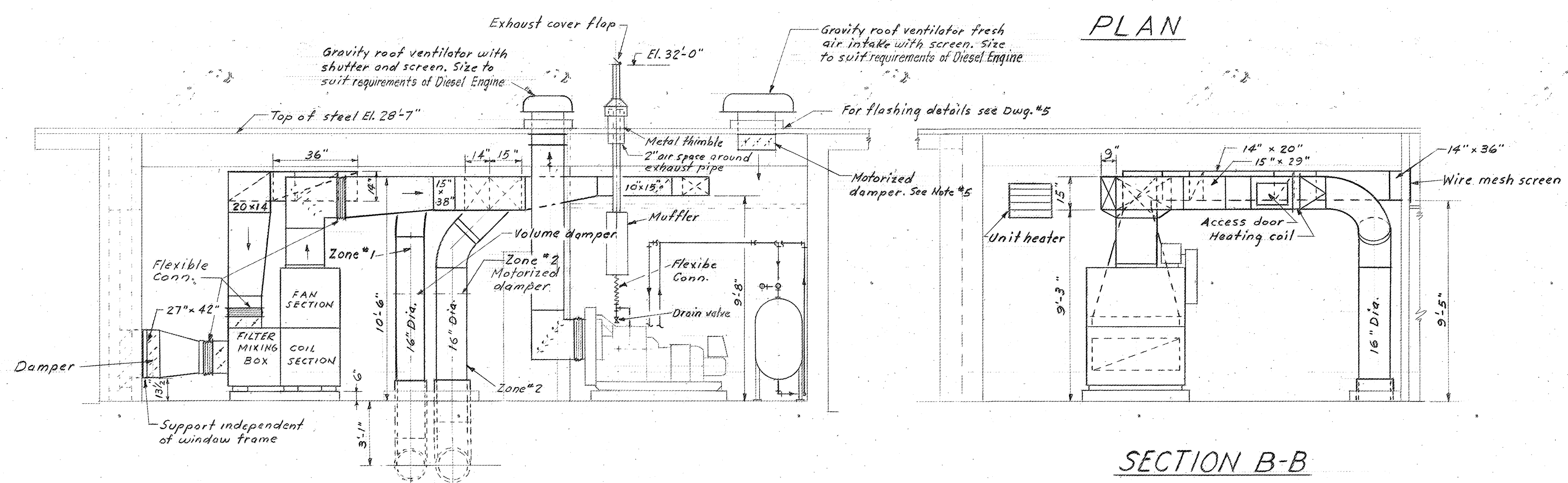
SECTION D-D
NO SCALE



SECTION E-E

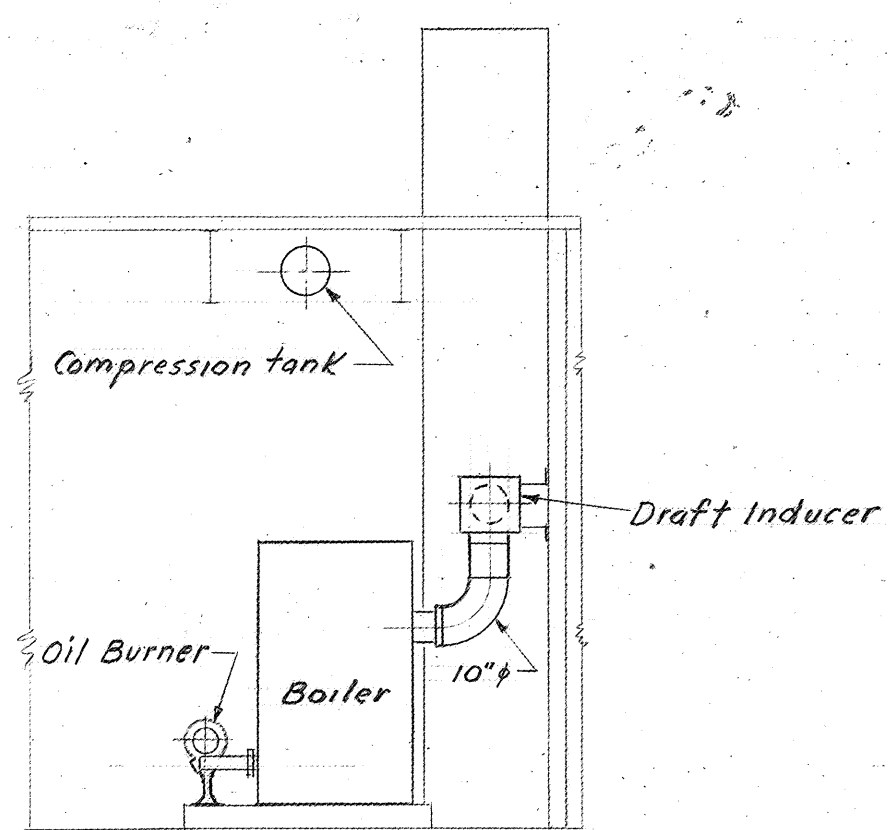
- NOTES:**
1. Work this drawing with drawing #14.
 2. For piping diagram see drawing #14.
 3. For Tankless Heater piping diagram see drawing #16.
 4. Air quantities shown are for cooling cycle only.
 5. Motorized damper on gravity air intake ventilator to open when Diesel Generator is running and close when Generator is stopped.
 6. Refrigerant piping to be dehydrated, installed & charged with refrigerant in accordance with refrigeration equipment manufacturer's recommendations.

PLAN



SECTION A-A

SECTION B-B

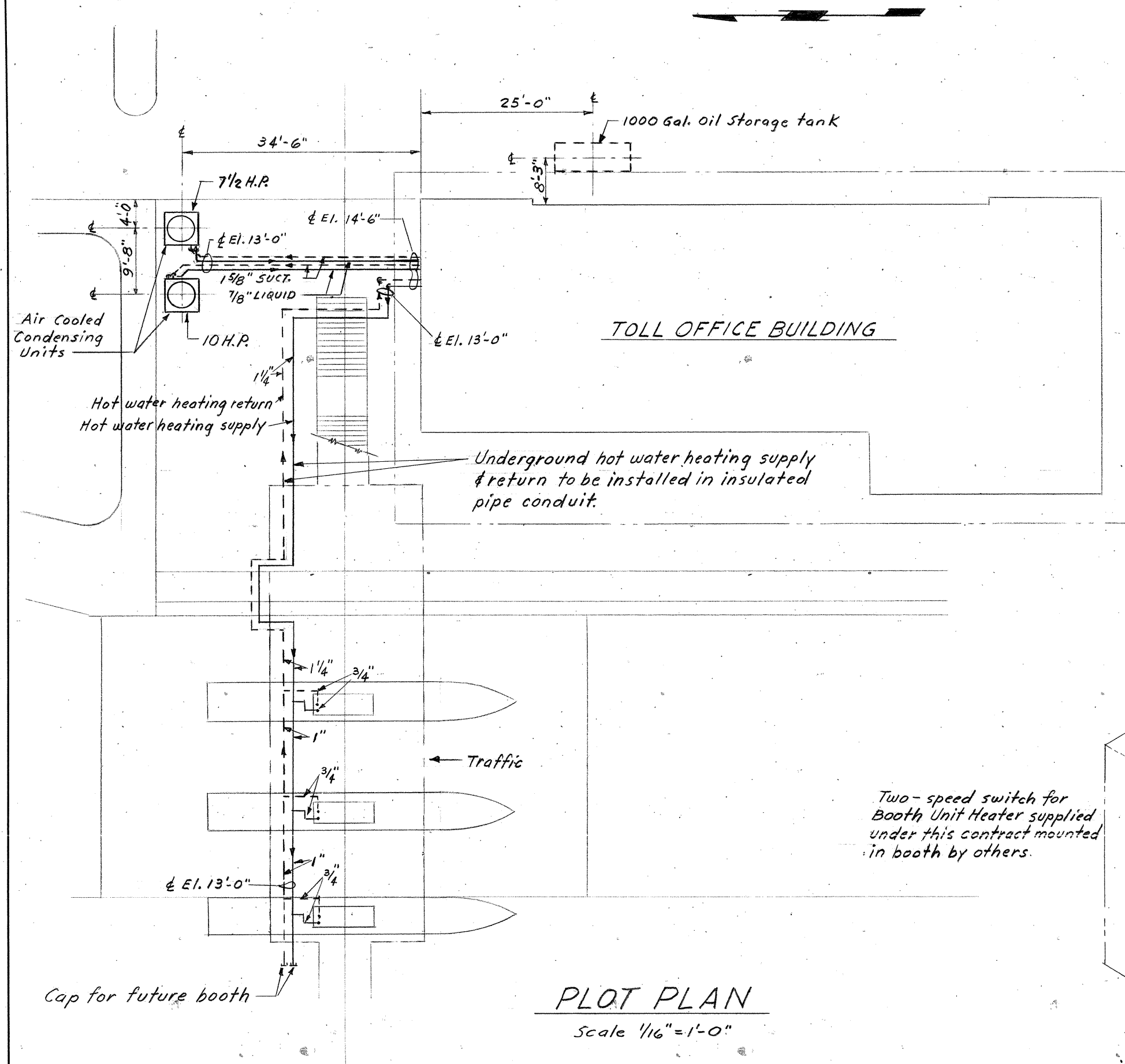


SECTION C-C



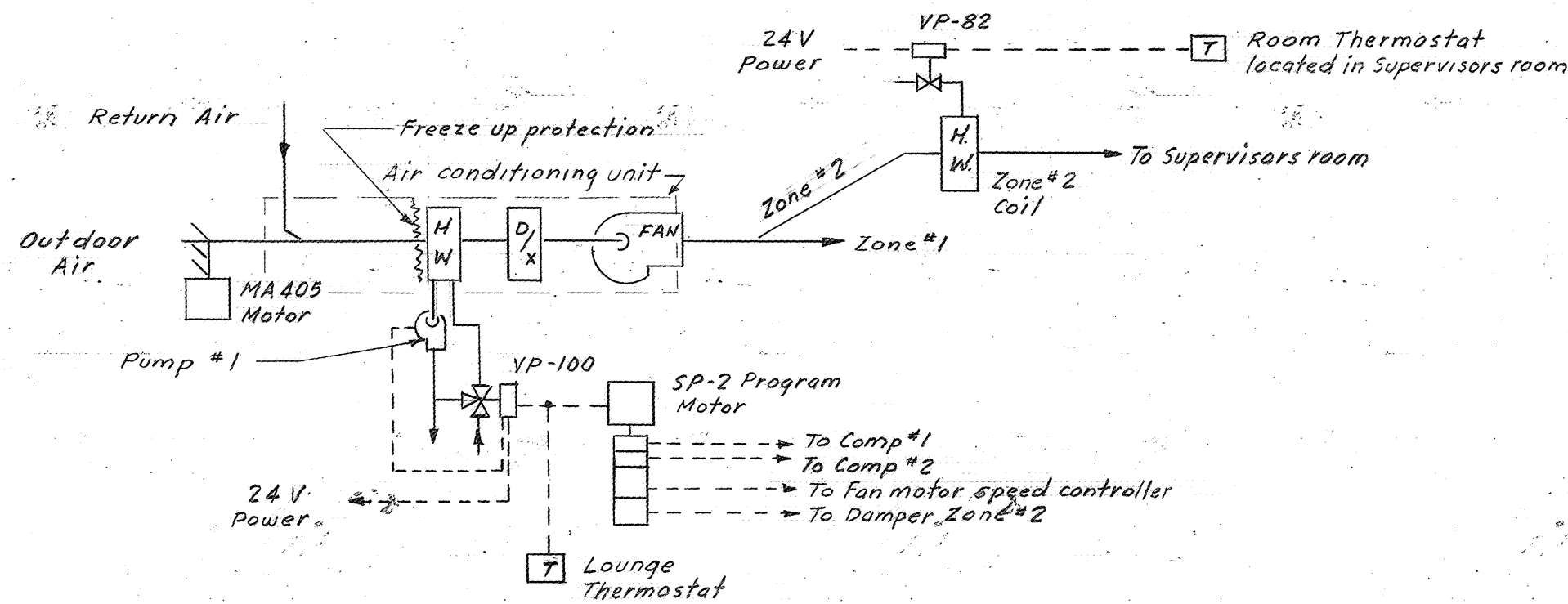
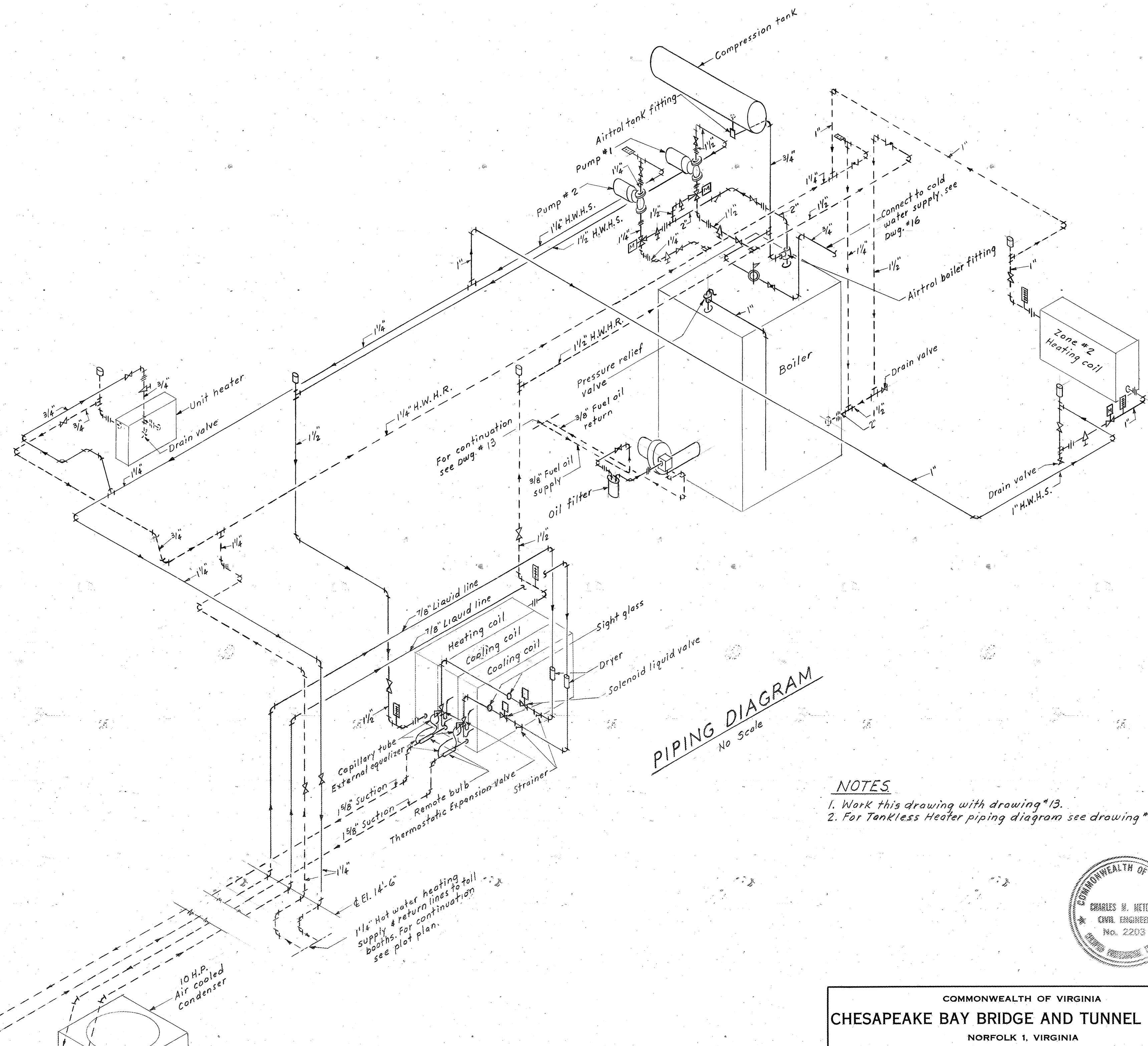
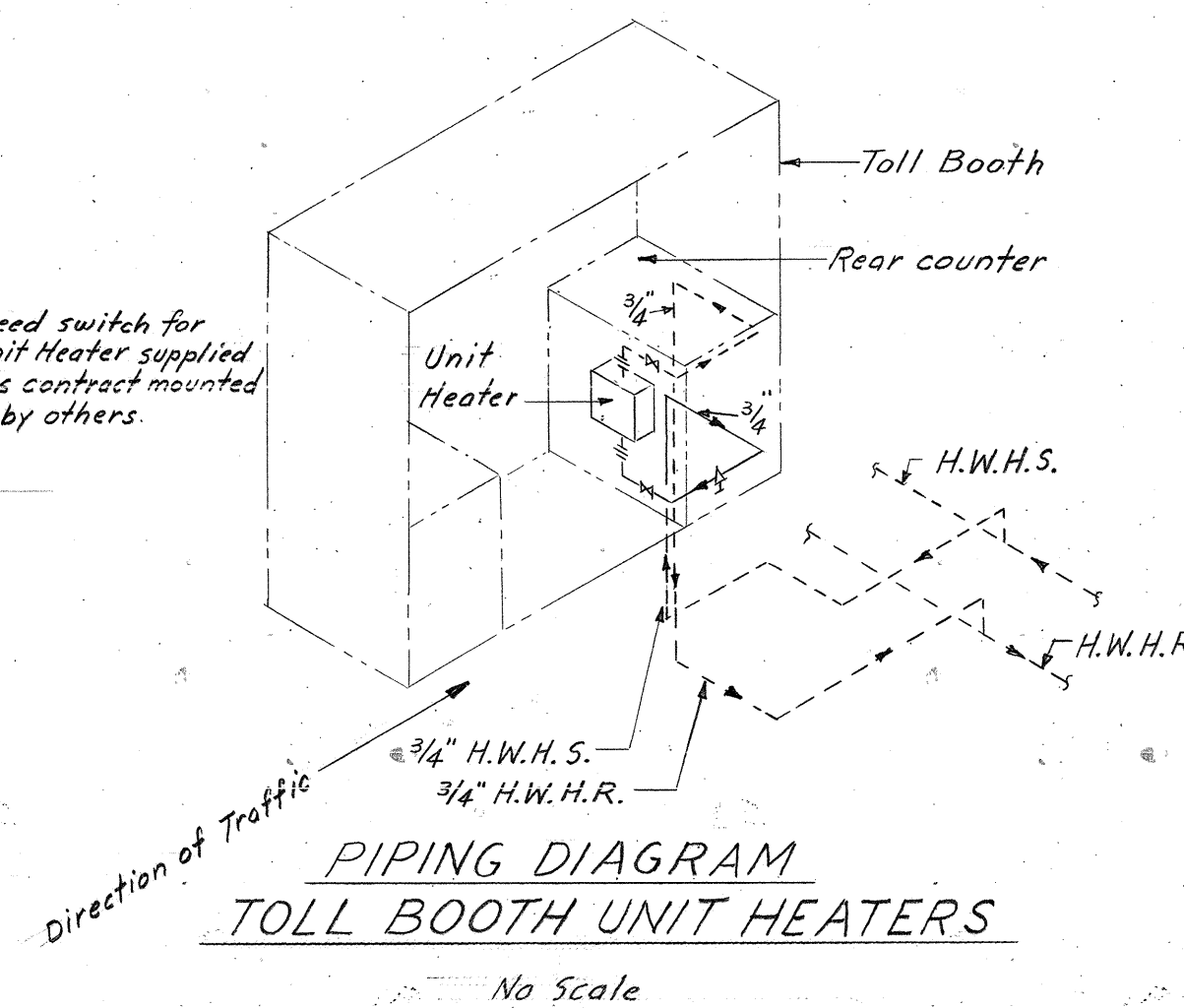
| | | |
|--|---------------------|---------------------|
| COMMONWEALTH OF VIRGINIA | | |
| CHESAPEAKE BAY BRIDGE AND TUNNEL DISTRICT | | |
| NORFOLK 1, VIRGINIA | | |
| SVERDRUP & PARCEL, CONSULTING ENGINEERS NEW YORK, N. Y. - ST. LOUIS, MO. - NORFOLK, VA. | | |
| CHESAPEAKE BAY BRIDGE-TUNNEL CROSSING SOUTH TOLL PLAZA TOLL OFFICE BUILDING HEATING, VENTILATING & AIR CONDITIONING | | |
| RECOMMENDED: <i>R.E. Egland</i> | DRAWN BY: R. OLIVER | SCALE: 1/4" = 1'-0" |
| CHECKED BY: <i>RE</i> | DATE: 1-16-63 | |
| APPROVED: <i>C. Metcalf</i> | DWG. NO. 13 OF 26 | |
| SECTION NO. TF-3 | | |

1555
62N/90

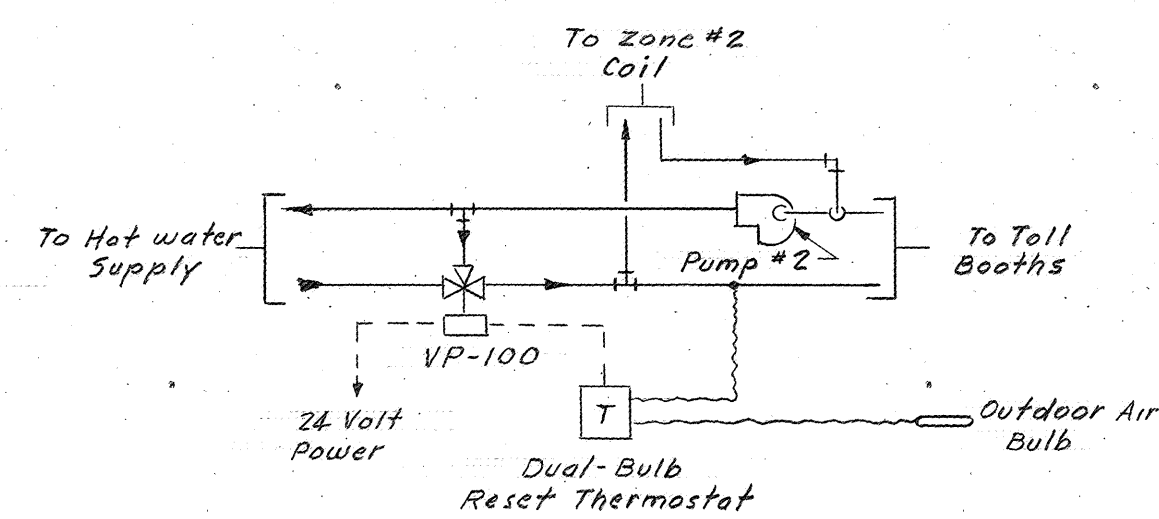


LEGEND

3 Way Motorized Control Valve
 Motorized Control Valve
 Globe Valve
 Gate Valve
 Balancing cock
 Pressure Relief Valve
 Strainer
 Union
 Thermometer
 Manual Air Vent
 H.W.H.S.
 H.W.H.R.
 H.P.
 Hot water heating supply
 Hot water heating return
 Horsepower



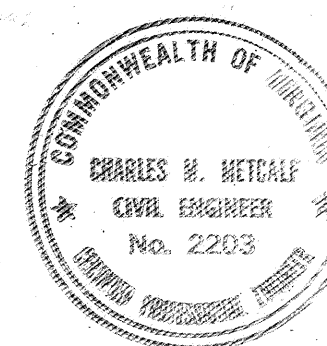
**CONTROL DIAGRAM
SUMMER & WINTER AIR CONDITIONING
SYSTEM**



**CONTROL DIAGRAM
TEMPERATURE RESET HOT WATER
CIRCULATING SYSTEM**

Note: Model designations refer to Barber-Coleman or approved equal.

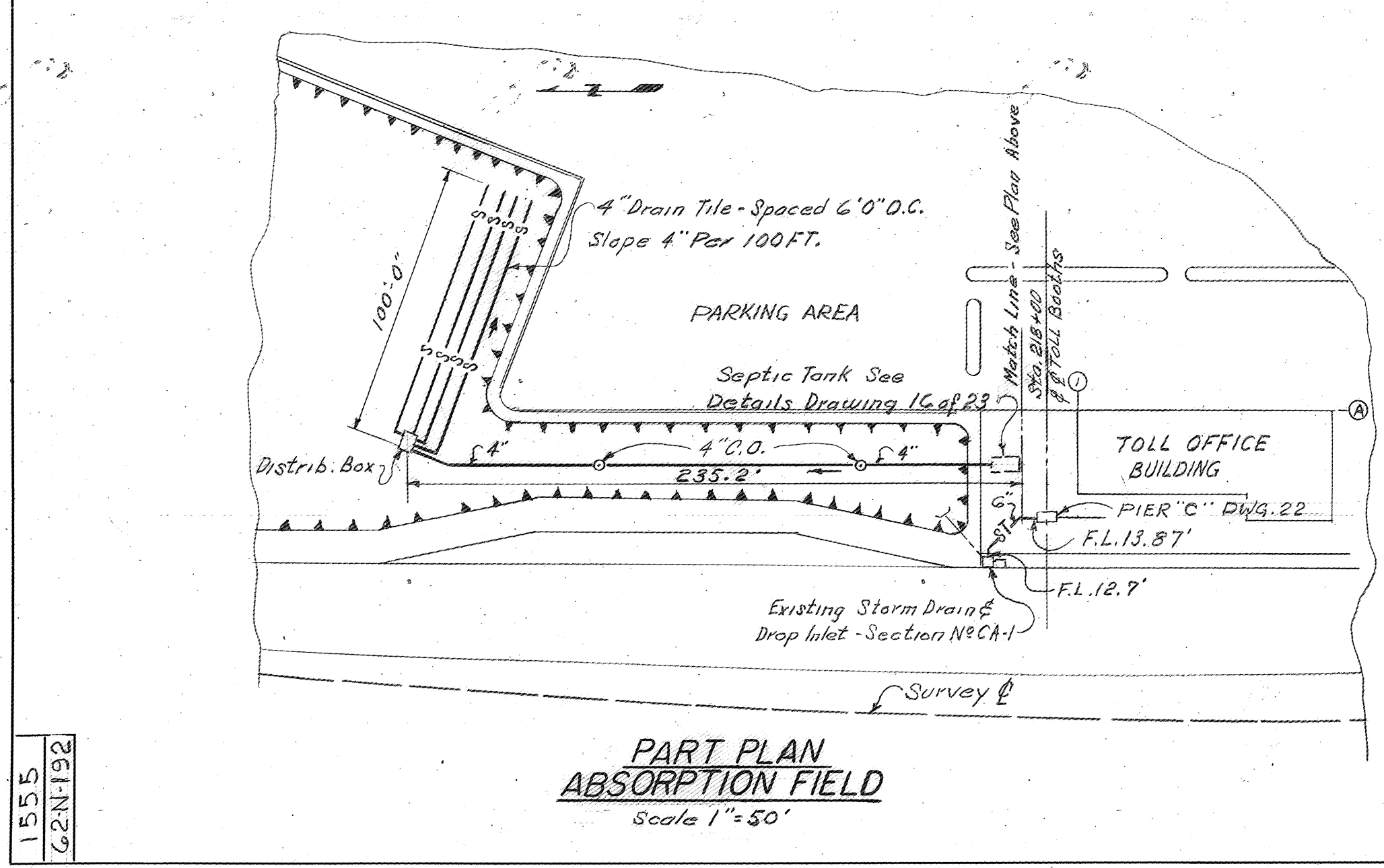
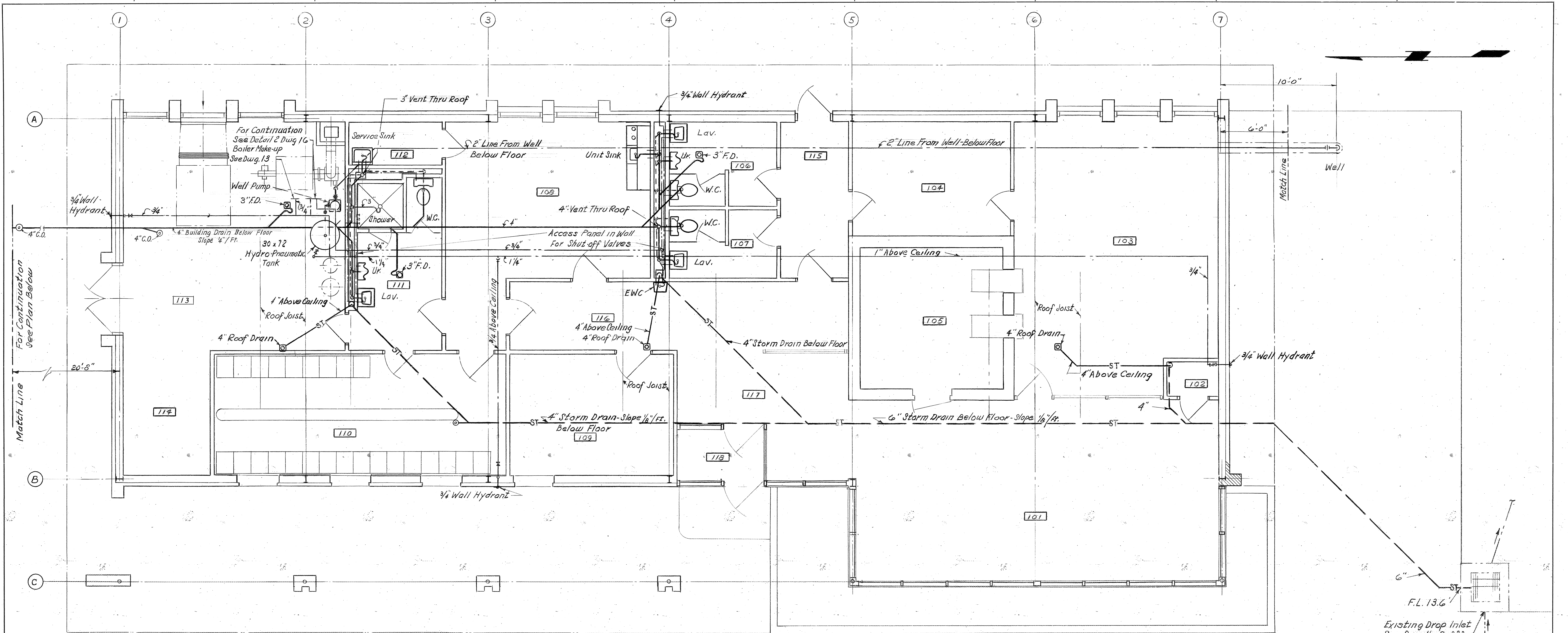
- NOTES**
1. Work this drawing with drawing #13.
 2. For Tankless Heater piping diagram see drawing #16.



| | |
|--|-------------------------------------|
| COMMONWEALTH OF VIRGINIA | |
| CHESAPEAKE BAY BRIDGE AND TUNNEL DISTRICT | |
| NORFOLK 1, VIRGINIA | |
| SVERDRUP & PARCEL, CONSULTING ENGINEERS NEW YORK, N. Y. - ST. LOUIS, MO. - NORFOLK, VA. | |
| CHESAPEAKE BAY BRIDGE-TUNNEL CROSSING SOUTH TOLL PLAZA TOLL BOOTHS & TOLL OFFICE BUILDING HEATING, VENTILATING & AIR CONDITIONING | |
| RECOMMENDED: <i>P.E. Eggen</i> | DRAWN BY: R. OLIVER SCALE: AS NOTED |
| CHECKED BY: <i>RE</i> | DATE: 1/16/63 |
| APPROVED: <i>Ch. Metcalf</i> | DWG. NO. 14 OF 26 |
| SECTION NO. TF-3 | |

| APPR. | NO. | REVISION |
|-------|-----|----------|
| | | |
| | | |
| | | |
| | | |

1555
22N191



PLAN
scale 1/4"=1'-0"

- PLUMBING LEGEND**
- ⌵ Gate Valve
 - ⌵ Check Valve
 - ⊙ Clean Out
 - ⊙ Floor or Roof Drain
 - Soil or Waste Line
 - ST — Storm Drain
 - ST — Storm Drain Below Ground
 - S — Sanitary Drain Tile
 - Vent
 - Cold Water
 - Hot Water

| APPR. NO. | REVISION |
|-----------|--|
| 1 | REVISED SEWAGE DISP. SYS. & DELETED WELL SECTION |
| 2 | RELOCATED ABSORPTION FIELD |
| 3 | AS BUILT |

COMMONWEALTH OF VIRGINIA

CHESAPEAKE BAY BRIDGE AND TUNNEL DISTRICT

NORFOLK 1, VIRGINIA

SVERDRUP & PARCEL, CONSULTING ENGINEERS
NEW YORK, N. Y. - ST. LOUIS, MO. - NORFOLK, VA.

CHESAPEAKE BAY BRIDGE-TUNNEL CROSSING
SOUTH TOLL PLAZA
TOLL OFFICE BUILDING
PLUMBING PLANS

RECOMMENDED:

R.E. Eglund

APPROVED:

Ch. H. Hitt

DRAWN BY: F.S.

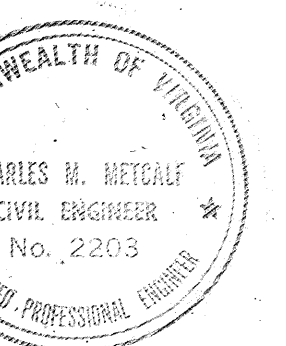
CHECKED BY: *R*

DWG. NO. 15 OF 26

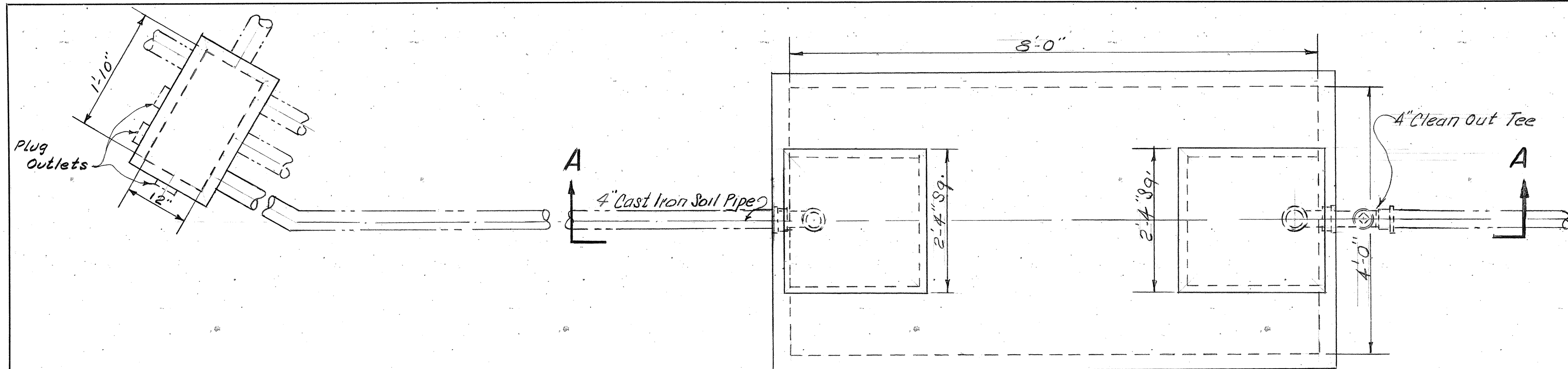
SCALE: AS SHOWN

DATE: 1.16.63

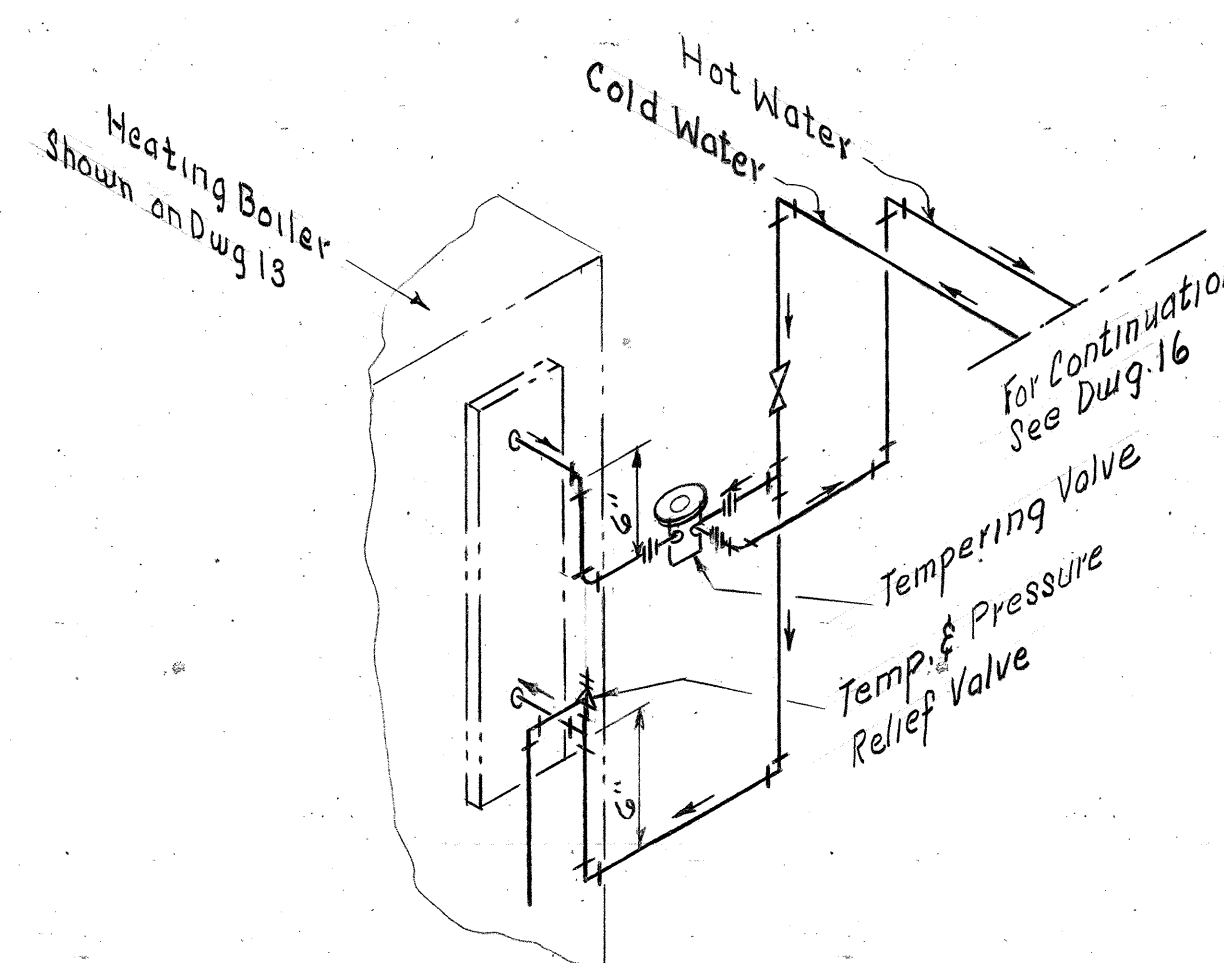
SECTION NO. T F - 3



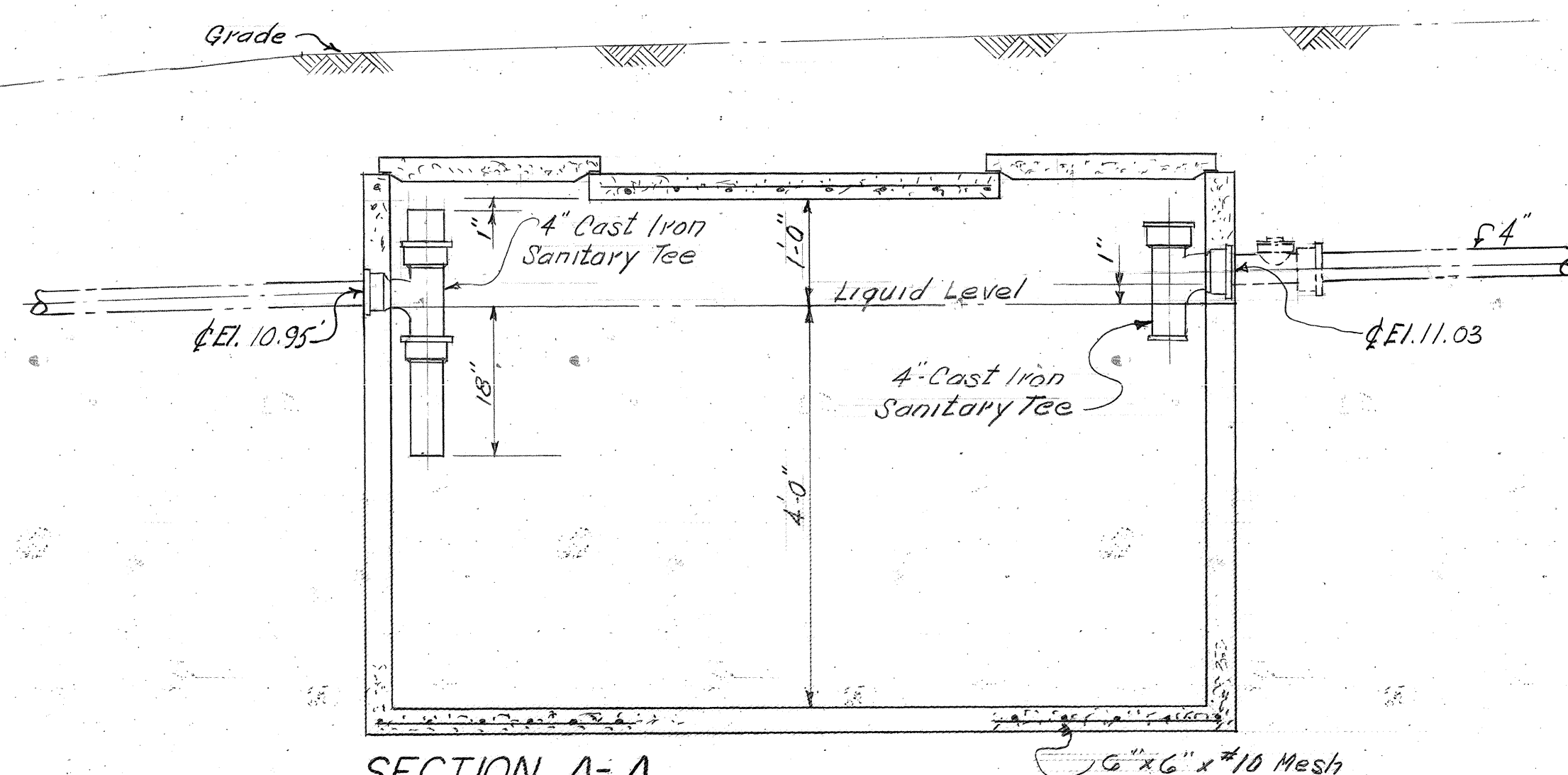
AS BUILT



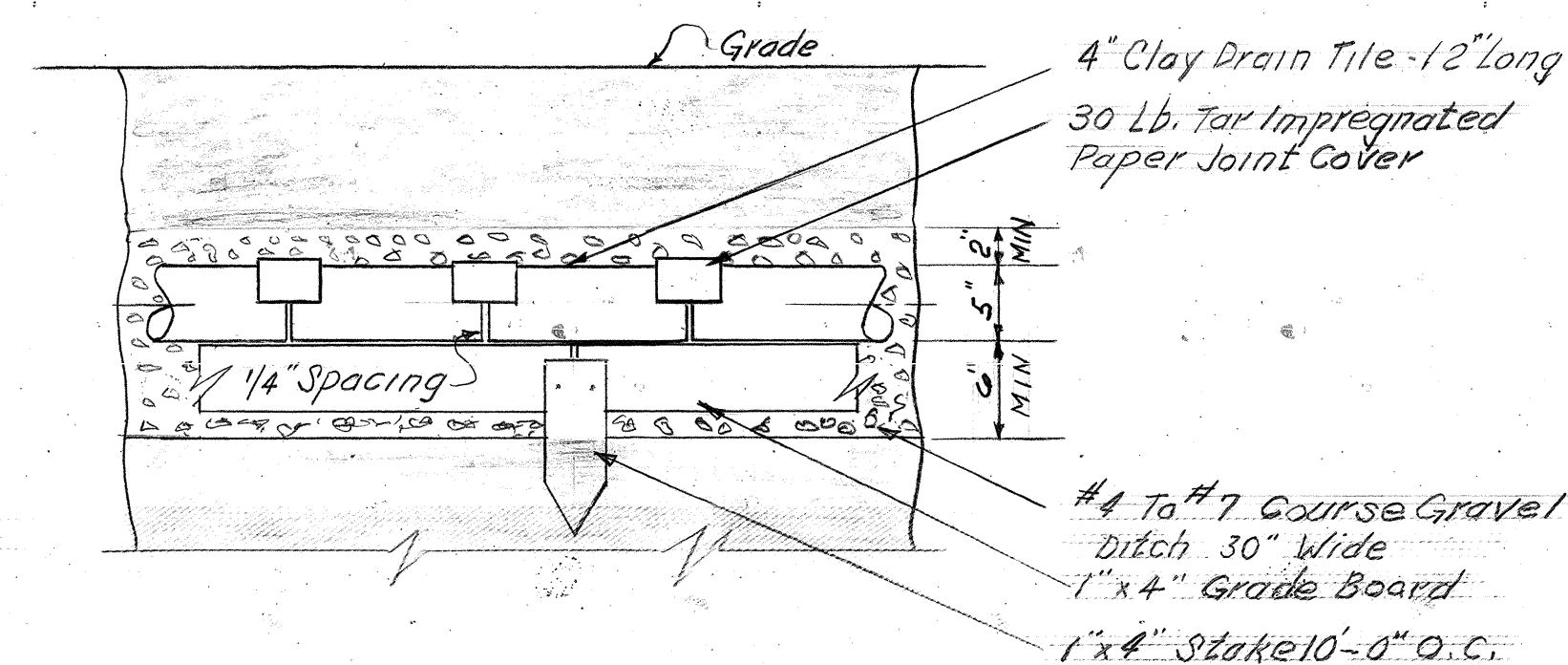
PLAN
DISTRIBUTION BOX AND SEPTIC TANK
 SCALE 3/4"=1'-0"



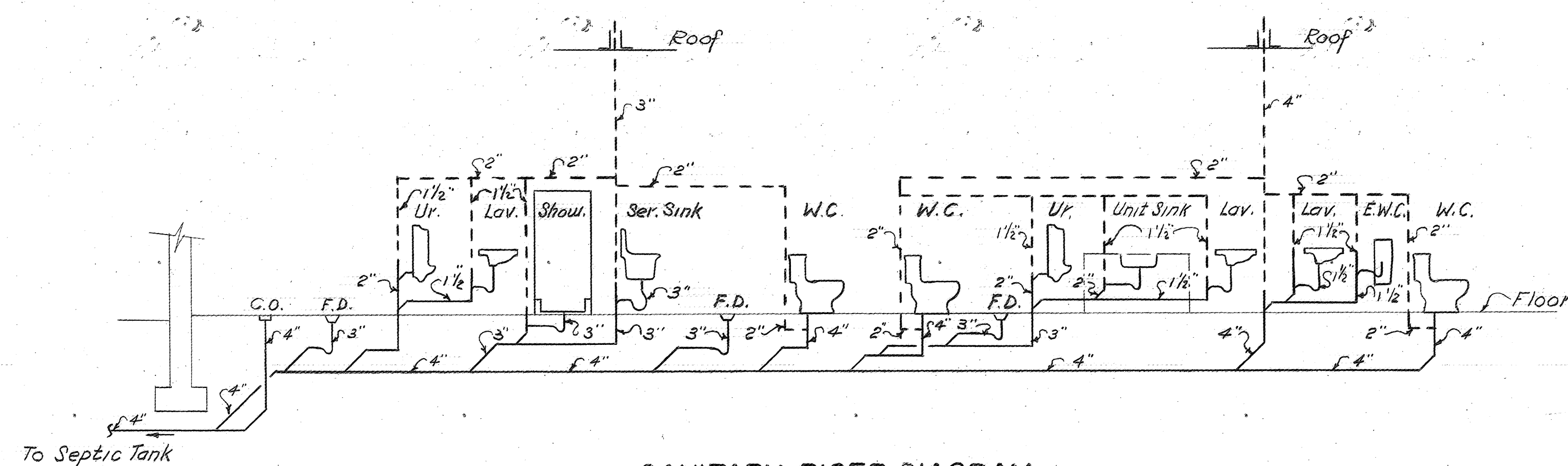
DETAIL 2
TANKLESS HEATER
PIPING DIAGRAM
 NO SCALE



SECTION A-A
 SCALE 3/4"=1'-0"



DETAIL 1
ABSORPTION FIELD PIPING
 NO SCALE



SANITARY RISER DIAGRAM
 NO SCALE



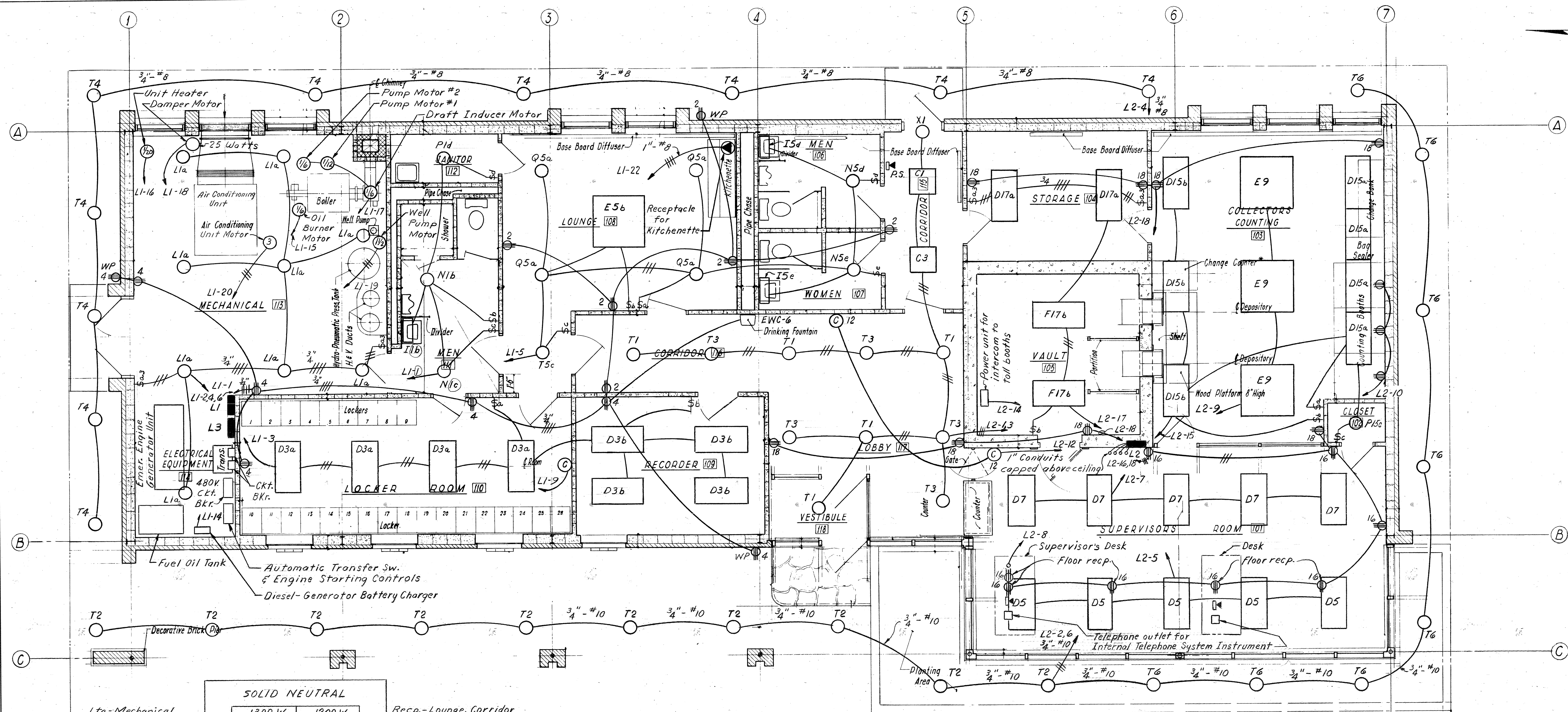
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| COMMONWEALTH OF VIRGINIA | | | |
| CHESAPEAKE BAY BRIDGE AND TUNNEL DISTRICT | | | |
| NORFOLK 1, VIRGINIA | | | |
| SVERDRUP & PARCEL, CONSULTING ENGINEERS NEW YORK, N. Y. - ST. LOUIS, MO. - NORFOLK, VA. | | | |
| CHESAPEAKE BAY BRIDGE-TUNNEL CROSSING SOUTH TOLL PLAZA TOLL OFFICE BUILDING PLUMBING DETAILS | | | |
| RECOMMENDED: | DRAWN BY: F.S. | SCALE: AS SHOWN | |
| CHECKED BY: B | DATE: 1-10-63 | | |
| DWG. NO. | 16 OF 26 | | |
| SECTION NO. T F - 3 | | | |

| APPR | NO. | REVISION |
|------|-----|--|
| MPL | 3 | 8-14-64 AS BUILT |
| F.A. | 2 | RELOCATED DISTRIBUTION BOX |
| F.A. | 1 | REVISED SEPTIC TANK-DIST. & DELETED WELL SECTION |

APPROVED:
P.E. Expland
M. Heston

AS BUILT

1555
 282N193



FLOOR PLAN

| SOLID NEUTRAL | | | |
|----------------------|-----------|--|--|
| 1300 W | 1200 W | | |
| 1 20A-1P | 2 20A-1P | | |
| 1600 W | 1400 W | | |
| 3 20A-1P | 4 20A-1P | | |
| 1270 W | 700 W | | |
| 5 20A-1P | 6 20A-1P | | |
| 848 W | | | |
| 7 20A-1P | 8 20A-1P | | |
| 9 20A-1P | 10 20A-1P | | |
| 11 20A-1P | 12 20A-1P | | |
| 13 20A-1P | 14 20A-1P | | |
| 1/4 HP | 1/2 HP | | |
| 15 20A-1P | 16 20A-1P | | |
| 1/4 HP | 25 W | | |
| 17 20A-1P | 18 20A-1P | | |
| 1 1/2 HP | 3 HP | | |
| 19 15A-3P | 20 30A-3P | | |
| 21 3P | 5260 W | | |
| | 22 30A-3P | | |
| 23 3P | 24 3P | | |
| MAIN BREAKER 225A-3P | | | |

3 PHASE, 4 WIRE, 120/208V.
MOUNTING: SURFACE
PANEL L1

Recp.-Lounge, Corridor,
& outside
Recp.-Mechanical, Locker
Recorder, & outside
1/3 HP Electric Water
Cooler - Corridor.
Spare
Spare
Spare
Diesel Generator
Battery Charger
Unit Heater
Damper Motor Operator
Air Conditioning Unit
Kitchenette
Provision for
future 100A

| SOLID NEUTRAL | | | |
|---------------|-----------|--|--|
| 980 W | 1500 W | | |
| 1 20A-1P | 2 20A-1P | | |
| 930 W | 1500 W | | |
| 3 20A-1P | 4 20A-1P | | |
| 1000 W | 1200 W | | |
| 5 20A-1P | 6 20A-1P | | |
| 1000 W | | | |
| 7 20A-1P | 8 20A-1P | | |
| 1200 W | | | |
| 9 20A-1P | 10 20A-1P | | |
| 11 20A-1P | 12 20A-1P | | |
| 13 20A-1P | 14 20A-1P | | |
| 1440 W | 1600 W | | |
| 15 20A-1P | 16 20A-1P | | |
| 800 W | 1600 W | | |
| 17 20A-1P | 18 20A-1P | | |
| 19 3P | 20 3P | | |

MAIN LUGS

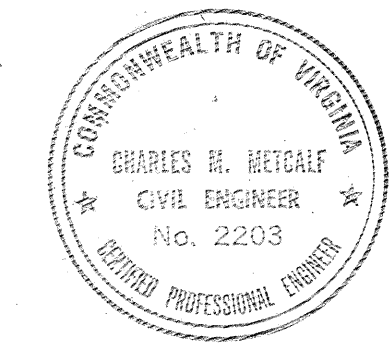
3 PHASE, 4 WIRE, 120/208V.
MOUNTING: FLUSH
PANEL L2

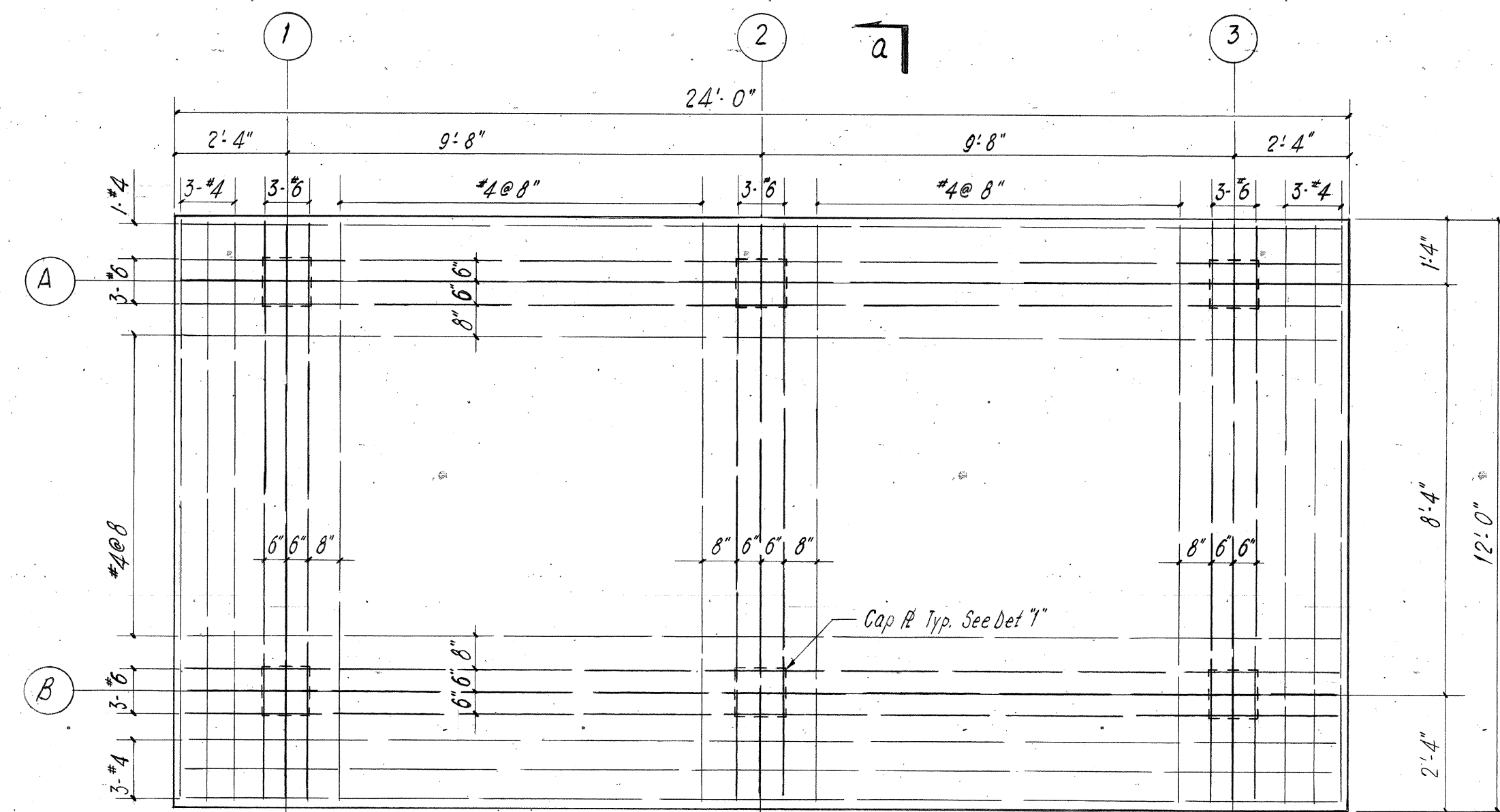
Ltg.-Lobby, Corridors,
& Vestibule
Ltg.-Lobby & Corridors
Ltg.-Supervisor's Rm.
Ltg.-Supervisor's Rm.
Ltg.-Collector's Counting
Spare
Spare
Ltg.-Collector's
Counting & Closet
Ltg.-Storage & Vault
Provision for
future 100A

Ltg.-outside W. Side
Bldg.
Ltg.-outside N & E
sides Bldg.
Ltg.-outside S & W
sides Bldg.
Overheight
Toll Totes
Recp.-
Counting booths
Clock recp.
Power Supply for
Intercom to Toll Booths
Recp.-Supervisor's Rm.
Recp.-Collector's Counting,
Storage, Vault, & Lobby
Provision for
future 100A

| | | |
|---|-------------------|-------------------|
| COMMONWEALTH OF VIRGINIA CHESAPEAKE BAY BRIDGE AND TUNNEL DISTRICT NORFOLK 1, VIRGINIA | | |
| SVERDRUP & PARCEL, CONSULTING ENGINEERS NEW YORK, N. Y.-ST. LOUIS, MO.-NORFOLK, VA. | | |
| CHESAPEAKE BAY BRIDGE-TUNNEL CROSSING SOUTH TOLL PLAZA TOLL OFFICE BUILDING ELECTRICAL | | |
| RECOMMENDED: <i>P. E. Egan</i> | DRAWN BY: J.A. | SCALE: 1/4"=1'-0" |
| CHECKED BY: <i>Opf.</i> | DATE: 1-16-63 | |
| APPROVED: <i>W. H. H. H.</i> | DWG. NO. 17 of 26 | |
| SECTION NO. TF-3 | | |

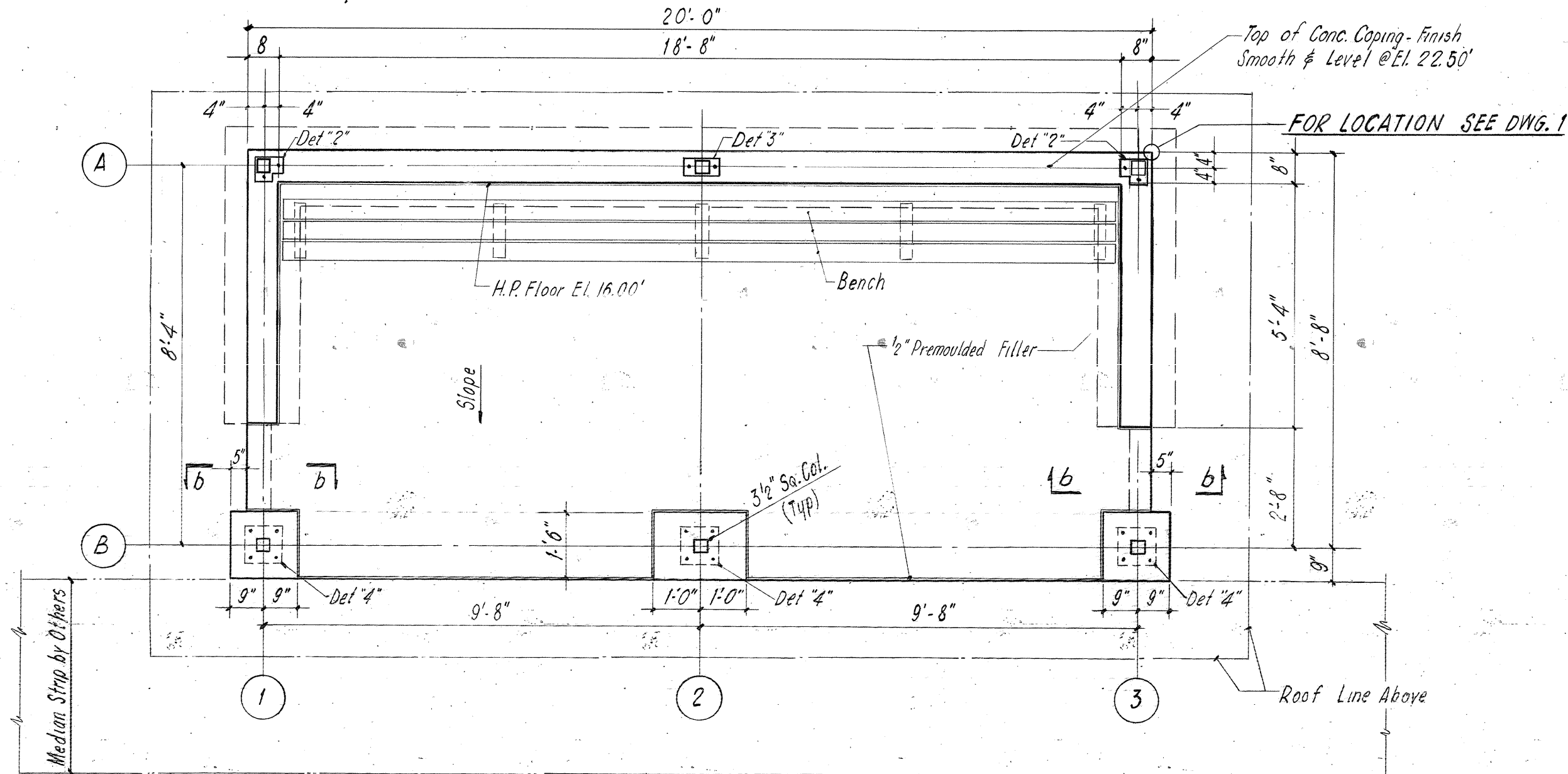
| REVISION | DATE |
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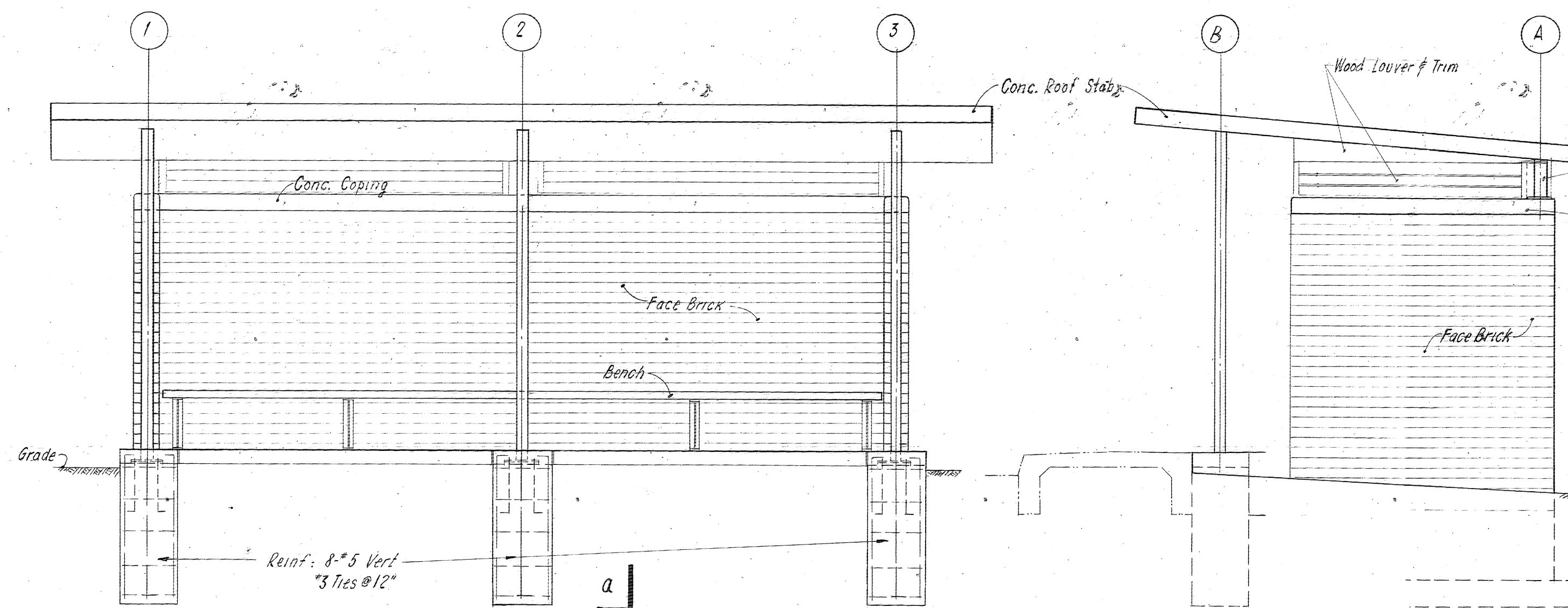


NOTE: Top Reinf. Shown on Plan
Bottom Reinf. Not Shown,
To Be #4 @ 8" Ea. Way

PLAN OF ROOF SLAB



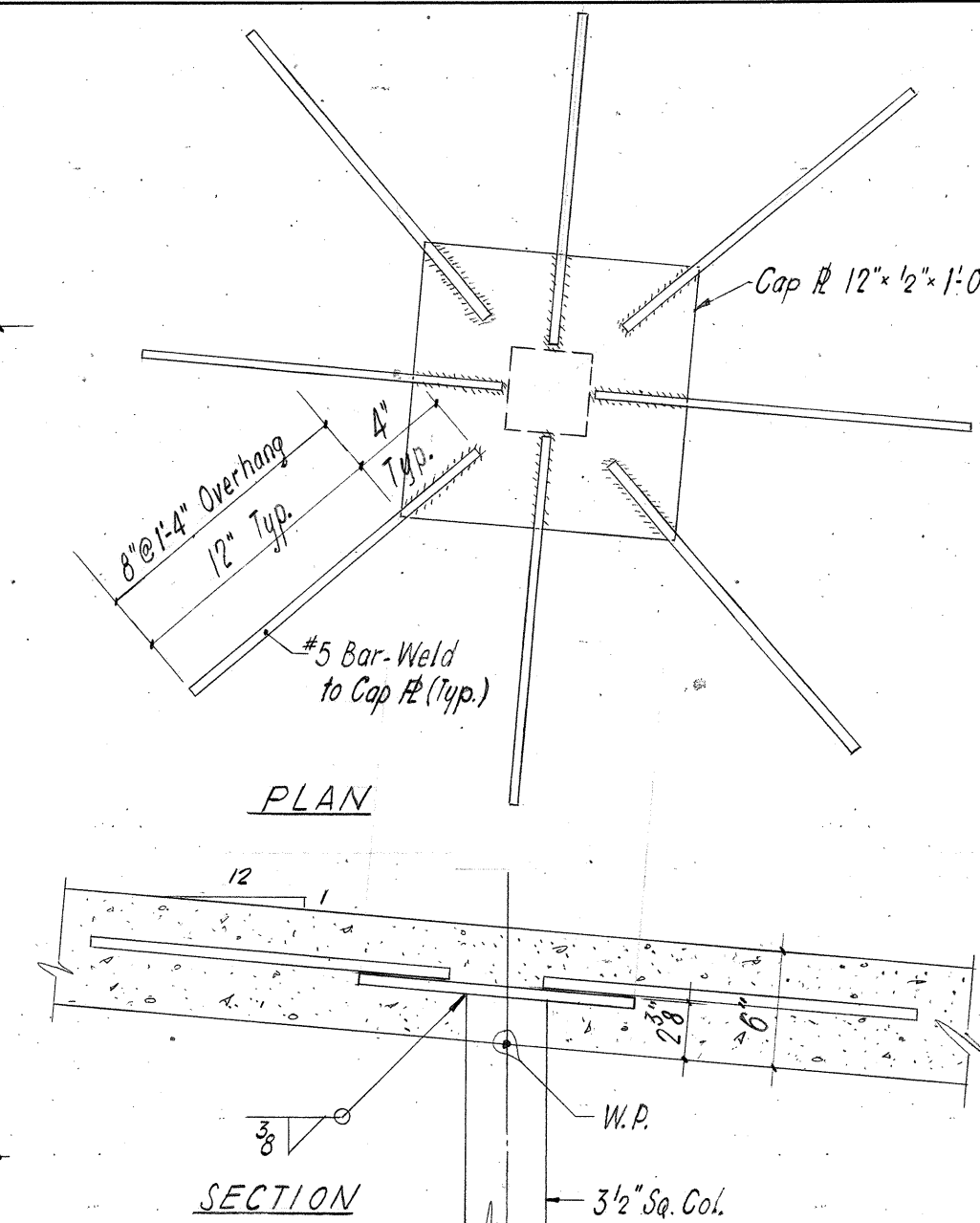
FLOOR PLAN



FRONT ELEVATION

SIDE ELEVATION

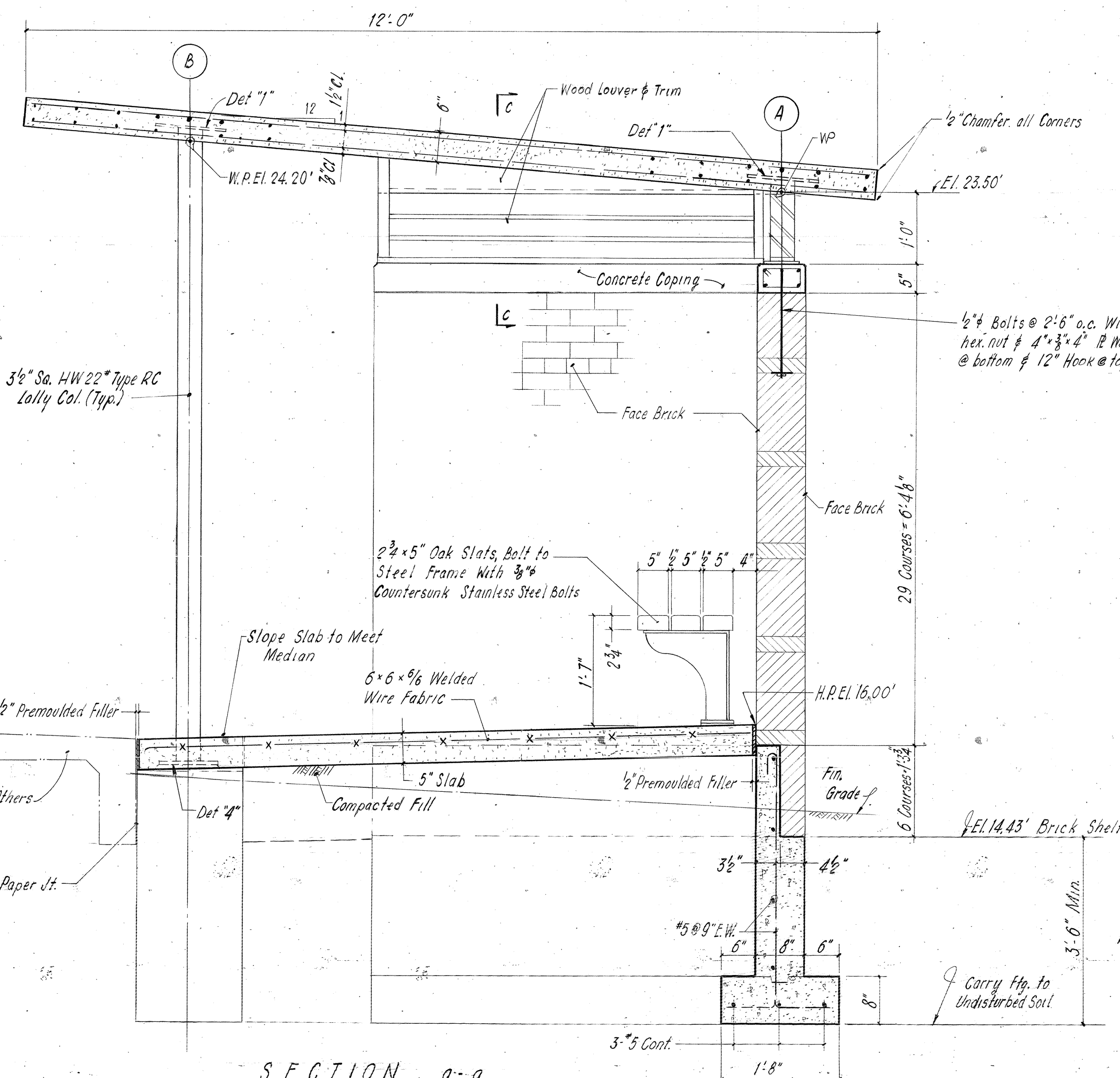
DO NOT SCALE THIS DRAWING. FOLLOW DIMENSIONS.



DETAIL '1'

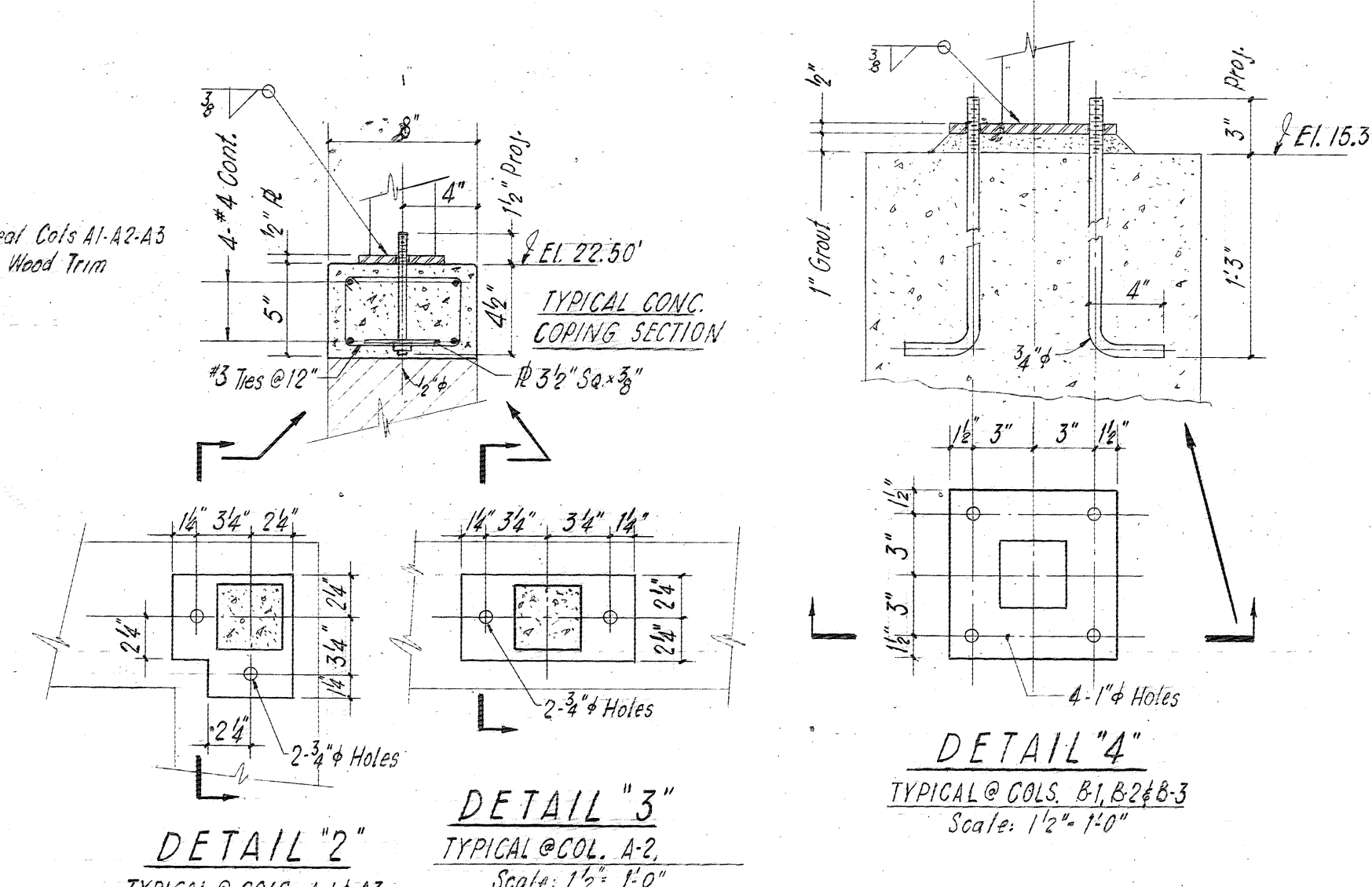
TYPICAL CAP R

Scale: 1/2" = 1'-0"



SECTION a-a

Scale: 3/4" = 1'-0"



DETAIL '2'

TYPICAL @ COLS. A-1 & A-3

Scale: 1/2" = 1'-0"

DETAIL '3'

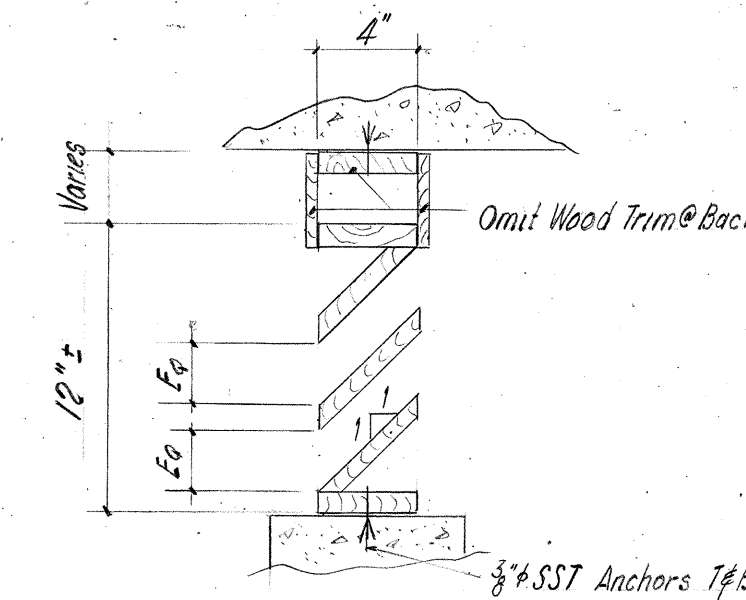
TYPICAL @ COL. A-2

Scale: 1/2" = 1'-0"

DETAIL '4'

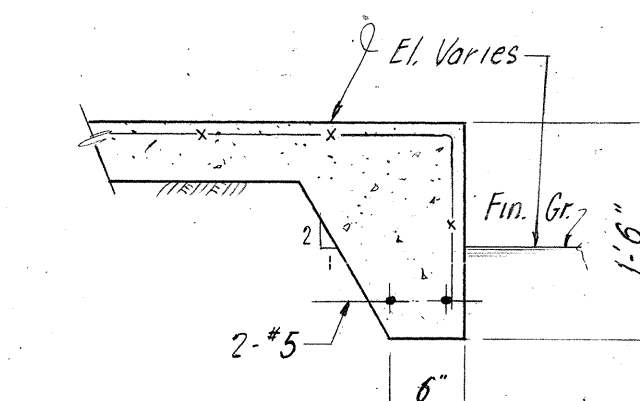
TYPICAL @ COLS. B-1, B-2 & B-3

Scale: 1/2" = 1'-0"



SECTION c-c

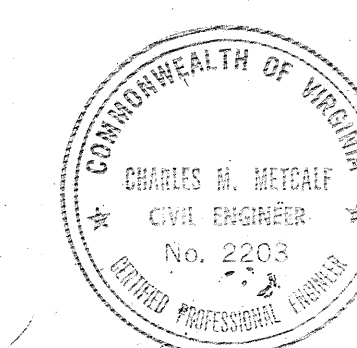
1. All wood for Louvers to be exterior grade.
2. Do not use plywood for Louver Blades.
3. Submit details of Louvers for Approval.



SECTION b-b

Scale: 3/4" = 1'-0"

NOTES:
1. For General Notes see Dwg. No. 10.



| APPR. NO. | REVISION |
|-----------|------------------|
| 1 | GENERAL REVISION |

COMMONWEALTH OF VIRGINIA
CHESAPEAKE BAY BRIDGE AND TUNNEL DISTRICT
NORFOLK 1, VIRGINIA

SVERDRUP & PARCEL, CONSULTING ENGINEERS
NEW YORK, N. Y. - ST. LOUIS, MO. - NORFOLK, VA.

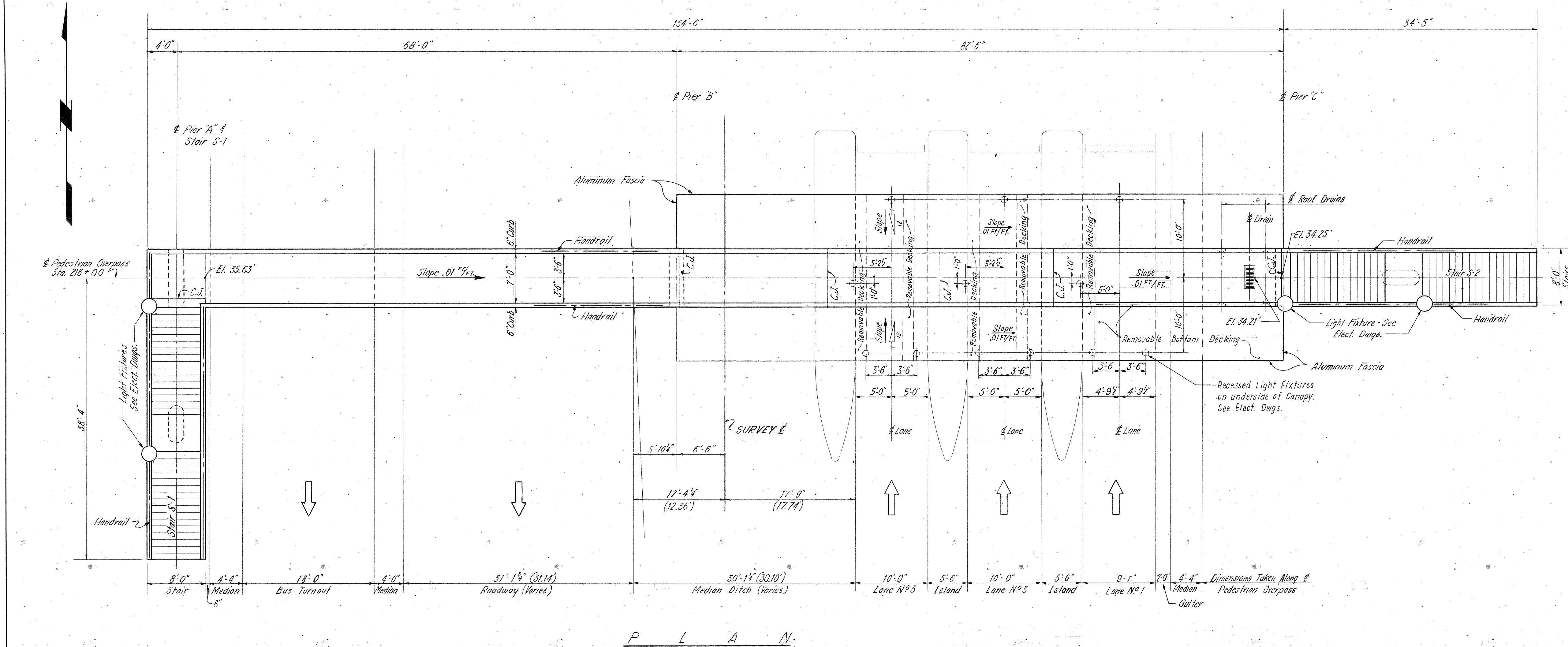
CHESAPEAKE BAY BRIDGE-TUNNEL CROSSING
SOUTH TOLL PLAZA
BUS SHELTER
PLANS & DETAILS

RECOMMENDED: *P.E. Eglund*
APPROVED: *M. Metcalf*

DRAWN BY: *Dora* SCALE: 3/4" Unless Noted
CHECKED BY: *Chen* DATE: 1-16-63
DWG. NO. 18 of 26
SECTION NO. TF-3

1565
62/105

REFERENCE NORTH



GENERAL NOTES

SPECIFICATIONS: VIRGINIA DEPARTMENT OF HIGHWAYS - ROAD AND BRIDGE SPECIFICATIONS AND SPECIAL PROVISIONS.

DESIGN LOADING: IN ACCORDANCE WITH DIVISION I OF THE AASHTO STANDARD SPECIFICATIONS 1961 EDITION.

LIVE LOAD - SIDEWALK SLAB = 85 PSF
LIVE LOAD - GIRDERS (OVER 26' SPAN) = 60 PSF
LIVE LOAD - CANOPY = 40 PSF
MAXIMUM DESIGN WIND VELOCITY = 125 MPH
DEAD LOAD = SUPPORTED BY STEEL GIRDERS ALONE
LIVE LOAD = SUPPORTED BY COMPOSITE ACTION

DESIGN UNIT STRESSES:

CONCRETE F_c = 1,200 PSI
REINFORCING STEEL F_s = 20,000 PSI
STRUCTURAL STEEL F_s = 18,000 PSI

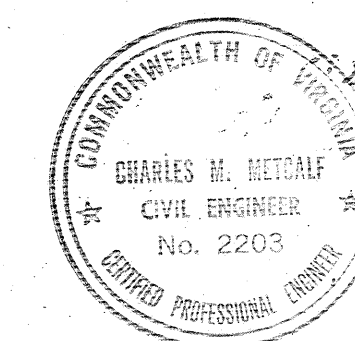
MINIMUM COVER FOR REINFORCING:

SLAB = 1" (EXCEPT AS NOTED)
PIERS = 2"
FOOTINGS = 3"

FOOTINGS: EXTEND THE BOTTOM OF ALL FOOTINGS TO UNDISTURBED SOIL IF SUCH MATERIAL IS NOT ENCOUNTERED AT THE ELEVATION INDICATED ON THESE DRAWINGS.
DESIGN SOIL PRESSURE FOR FOOTINGS IS 3,000 PSF.

NOTE

1. Removable panels are 18" wide and total width of canopy; located adjacent to toll booths.



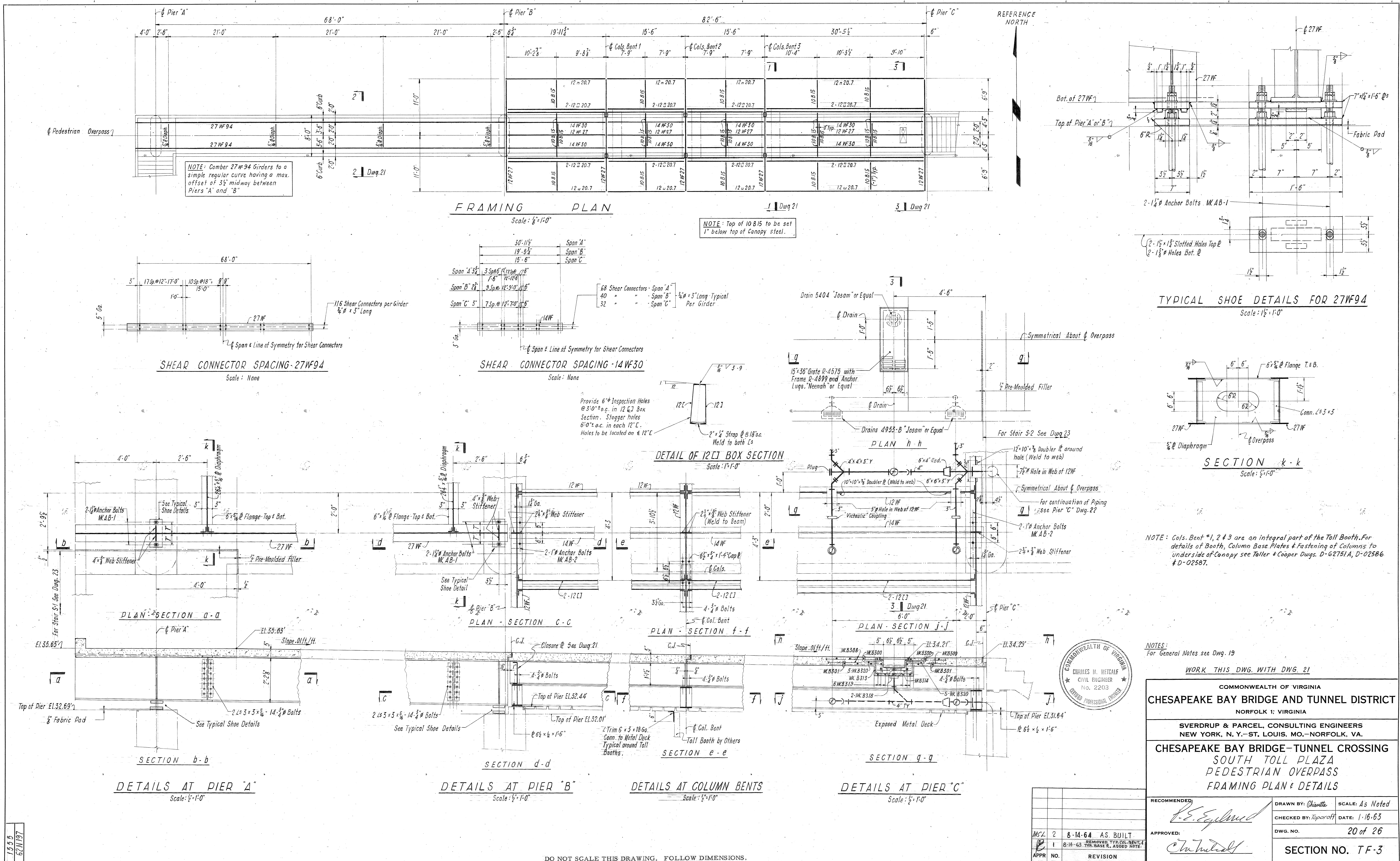
| | |
|--|-------------------------------------|
| COMMONWEALTH OF VIRGINIA | |
| CHESAPEAKE BAY BRIDGE AND TUNNEL DISTRICT | |
| NORFOLK 1, VIRGINIA | |
| SVERDRUP & PARCEL, CONSULTING ENGINEERS NEW YORK, N. Y. - ST. LOUIS, MO. - NORFOLK, VA. | |
| CHESAPEAKE BAY BRIDGE-TUNNEL CROSSING SOUTH TOLL PLAZA PEDESTRIAN OVERPASS GENERAL PLAN & ELEVATION | |
| RECOMMENDED: <i>P.E. Eglund</i> | DRAWN BY: J. Rivera SCALE: 6"=1'-0" |
| CHECKED BY: Toporoff | DATE: 1-16-63 |
| APPROVED: <i>C. Minteroff</i> | DWG. NO. 19 OF 26 |
| SECTION NO. 7F-3 | |

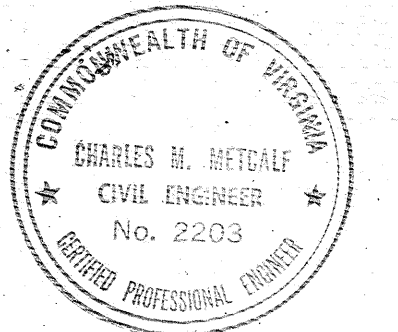
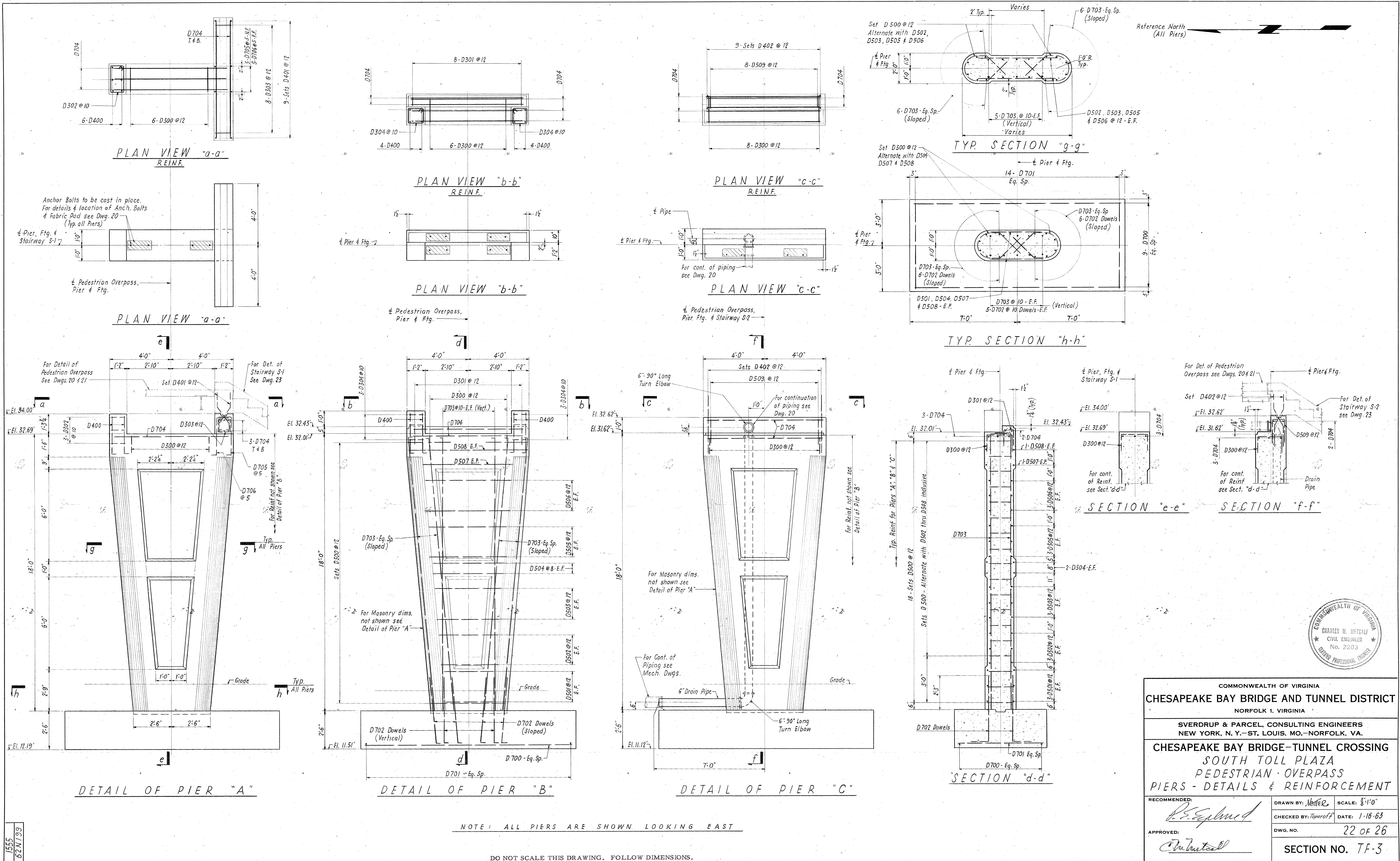
| MSL | 1 | 8-14-64 | AS BUILT |
|------|-----|----------|----------|
| APPR | NO. | REVISION | |

DO NOT SCALE THIS DRAWING. FOLLOW DIMENSIONS.

AS BUILT

1555
62 N/96





COMMONWEALTH OF VIRGINIA
CHESAPEAKE BAY BRIDGE AND TUNNEL DISTRICT
 NORFOLK 1, VIRGINIA

SVERDRUP & PARCEL, CONSULTING ENGINEERS
 NEW YORK, N. Y. - ST. LOUIS, MO. - NORFOLK, VA.

CHESAPEAKE BAY BRIDGE-TUNNEL CROSSING
SOUTH TOLL PLAZA
PEDESTRIAN OVERPASS
PIERS - DETAILS & REINFORCEMENT

RECOMMENDED: *[Signature]*
 APPROVED: *[Signature]*

DRAWN BY: *[Signature]* SCALE: 3/4"=1'-0"
 CHECKED BY: *[Signature]* DATE: 1-16-63
 DWG. NO. 22 OF 26
SECTION NO. TF-3

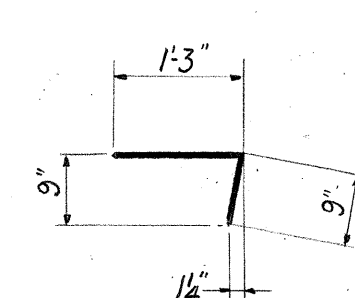
| BAR LIST - PEDESTRIAN OVERPASS SLAB | | | | | | | | | | | | | | | |
|-------------------------------------|------|---------|------|------|----------|------------------------|-------|--------|----|---|--------|----|--------|----|---|
| No. Reqd. | Size | Length | Mark | Type | Location | Dimensions for Bending | | | | | | | | | |
| | | | | | | A | B | C | D | E | F | G | H | J | K |
| 220 | #3 | 8'-7" | B300 | 1 | Slab | 5" | 7'-9" | | | | | 5" | | 3" | |
| 226 | | 9'-0" | B301 | 17 | | | 7" | 7'-10" | 7" | | | | | | |
| 16 | | 7'-10" | B302 | 17 | | | 7" | 6'-8" | 7" | | | | | | |
| 11 | | 7'-5" | B303 | 1 | | 5" | 6'-7" | | | | | 5" | | 3" | |
| 33 | | 36'-6" | B304 | Str. | | | | | | | | | | | |
| 19 | | 19'-8" | B305 | Str. | | | | | | | | | | | |
| 38 | | 15'-3" | B306 | Str. | | | | | | | | | | | |
| 14 | | 30'-10" | B307 | Str. | | | | | | | | | | | |
| 5 | | 26'-7" | B308 | Str. | | | | | | | | | | | |
| 5 | | 3'-7" | B309 | Str. | | | | | | | | | | | |
| 476 | | 1'-10" | B310 | S10 | | | 9" | 4" | 9" | | | | | | |
| 5 | | 28'-6" | B311 | Str. | | | | | | | | | | | |
| 2 | | 9'-0" | B312 | Str. | | | | | | | | | | | |
| 8 | | 2'-10" | B313 | 2 | | 7" | 1'-8" | | | | 7" | | 7 1/2" | | |
| 5 | | 1'-9" | B314 | Str. | | | | | | | | | | | |
| 6 | | 2'-5" | B315 | 16 | | — | 1'-2" | 8" | 7" | | 6 1/2" | — | 3 1/2" | | |
| 4 | | 2'-11" | B316 | 17 | | | | 2'-4" | — | | | | | | |
| 4 | | 2'-7" | B317 | 1 | | 5" | 2'-2" | | | | — | 3" | | | |
| 2 | | 2'-6" | B318 | Str. | | | | | | | | | | | |
| 6 | | 3'-10" | B319 | Str. | | | | | | | | | | | |
| 10 | | 1'-6" | B320 | 17 | | | 6" | 1'-0" | — | | | | | | |
| 2 | #3 | 2'-0" | B321 | 17 | Slab | | 1'-0" | 1'-0" | — | | | | | | |

| BAR LIST - PEDESTRIAN OVERPASS STAIRWAYS S-1 & S-2 | | | | | | | | | | | | | | | | |
|--|------|-----------|------|---------|-------------|------------------------|--------|------------|-----------|---|-----|------------|-----------|-------------|-----------|---|
| No. Reqd. | Size | Length | Mark | Type | Location | Dimensions for Bending | | | | | | | | | | |
| | | | | | | A | B | C | D | E | F | G | H | J | K | R |
| 22 | #7 | 19'-5" | C700 | 4 | Stairway | — | 18'-2" | 1'-3" | — | — | — | — | 1'-0 3/4" | — | 8" | |
| 26 | | 18'-6" | C701 | Str. | | | | | | | | | | | | |
| 11 | | 19'-4" | C702 | Special | | | | | | | | | | | | |
| 13 | | 17'-2" | C703 | Str. | | | | | | | | | | | | |
| 11 | | 18'-3" | C704 | Special | | | | | | | | | | | | |
| 13 | | 16'-2" | C705 | Str. | Stairway | | 1'-0" | 4'-0" | — | | | | | | | |
| 32 | | 5'-0" | C706 | 17 | Pier | | 1'-0" | 11'-2" | — | | | | | | | |
| 20 | | 12'-2" | C707 | 17 | | | | | | | | | | | | |
| 14 | | 9'-5" | C708 | 1 | | 10" | 7'-9" | | | | 10" | 7" | 7" | | | |
| 26 | | 7'-9" | C709 | 17 | | | 1'-0" | 6'-9" | — | | | | | | | |
| 22 | | 7'-6" | C710 | 4 | | — | 5'-3" | 2'-3" | — | — | — | 1'-2 1/2" | — | 1'-10 3/4" | | |
| 12 | #7 | 13'-6" | C711 | 17 | Pier | | 2'-3" | 11'-3" | — | | | | | | | |
| 6 | #5 | 7'-6" | C500 | Str. | Ftg. | | | | | | | | | | | |
| 16 | | 4'-6" | C501 | S10 | | | 1'-6" | 1'-6" | 1'-6" | | | | | | | |
| 12 | | 7'-8" | C502 | Str. | Stairway | | | | | | | | | | | |
| 6 | | 7'-10" | C503 | Str. | | | | | | | | | | | | |
| 26 | | 3'-11" | C504 | S10 | | | 1'-5" | 2'-6" | — | | | | | | | |
| 14 | | 10'-6" | C505 | 17 | Stairway | | 1'-4" | 7'-10" | 1'-4" | | | | | | | |
| 20 | | 4'-6" | C506 | Str. | Ftg. | | | | | | | | | | | |
| 8 | | 1'-3" | C507 | Str. | Pier | | | | | | | | | | | |
| 16 | | 2'-0" | C508 | Str. | | | | | | | | | | | | |
| 12 | | 3'-0" | C509 | Str. | | | | | | | | | | | | |
| 40 | | 6'-10" | C510 | Special | | | | | | | | | | | | |
| 8 | #5 | 4'-7" | C511 | Str. | Pier | | | | | | | | | | | |
| 32 | #4 | 4'-2" | C400 | 4 | Ftg. & Wall | — | 9 3/4" | 1'-10 3/4" | 7'-5 1/2" | — | — | — | 1'-4" | — | 1'-4 1/2" | |
| 22 | #4 | 4'-4" | C401 | Special | Stairway | | | | | | | | | | | |
| 16 | #3 | 2'-2" | C300 | S10 | Stairway | | 6" | 1'-2" | 6" | | | | | | | |
| 34 | | 10'-0" | C301 | 17 | | | 1'-11" | 7'-10" | 1'-1" | | | | | | | |
| 30 | | 8'-7" | C302 | 1 | | 5" | 7'-9" | | | | 5" | 4" | 3" | | | |
| 8 | | 22'-11" | C303 | 14 | | 1'-0" | 17'-0" | 4'-11" | | | | 9'-1 3/4" | | 14'-4" | | |
| 8 | | 2'-0" | C304 | 4 | | — | — | 1'-0" | 1'-0" | — | — | 6 1/2" | — | 10" | | |
| 2 | | 17'-8" | C305 | Str. | | | | | | | | | | | | |
| 4 | | 17'-2" | C306 | 4 | | — | 8" | 16'-6" | | — | — | 8'-10 1/4" | — | 13'-11 1/4" | | |
| 167 | | 2'-2 1/2" | C307 | S10 | | | 11" | 4 1/2" | 11" | | | | | | | |
| 67 | | 7'-10" | C308 | Str. | | | | | | | | | | | | |
| 268 | | 2'-0" | C309 | Special | | | | | | | | | | | | |
| 2 | | 18'-3" | C310 | 4 | Stairway | — | 8" | 17'-7" | — | — | — | 9'-5 3/4" | — | 14'-11" | | |
| 8 | #3 | 2'-10" | C311 | S10 | Pier | | 7" | 1'-8" | 7" | | | | | | | |

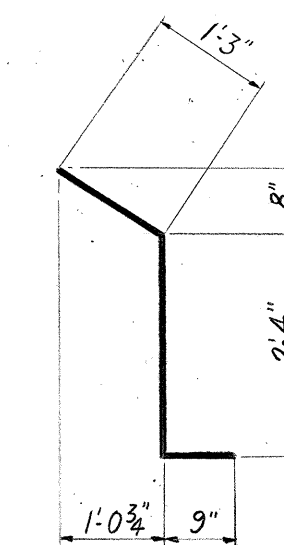
1575
6/2/2022

BAR LIST - PEDESTRIAN OVERPASS PIERS

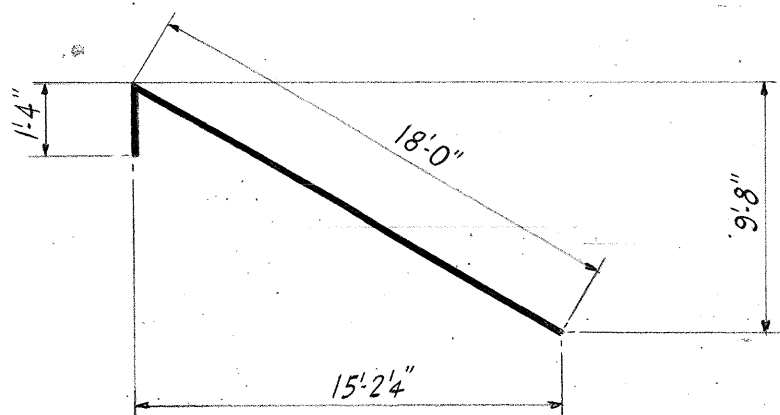
| No. Reqd. | Size | Length | Mark | Type | Location | Dimensions for Bending | | | | | | | | | |
|--------------|------|--------|------|---------|---------------------|------------------------|-------|--------|-----------|-------|---|----|-----|----|-----------|
| | | | | | | A | B | C | D | E | F | G | H | J | K |
| 27 | #7 | 13'-6" | D700 | Str. | Ftg. Piers A, B & C | | | | | | | | | | |
| 42 | #7 | 5'-6" | D701 | Str. | Ftg. Piers A, B & C | | | | | | | | | | |
| 66 | #7 | 5'-6" | D702 | 17 | Ftg. Piers A, B & C | | 1'-0" | 4'-6" | | | | | | | |
| 66 | #7 | 18'-9" | D703 | 17 | Piers A, B & C | | 1'-0" | 17'-9" | | | | | | | |
| 19 | #7 | 7'-6" | D704 | Str. | Piers A, B & C | | | | | | | | | | |
| 5 | #7 | 4'-6" | D705 | 17 | Pier A | | 1'-3" | 2'-5" | 10" | | | | | | |
| 5 | #7 | 4'-6" | D706 | 20 | Pier A | | 1'-3" | 2'-5" | 10" | | | | | | |
| 108 | #5 | 7'-8" | D500 | Special | Piers A, B & C | | | | | | | | | | |
| 18 | #5 | 3'-2" | D501 | Str. | | | | | | | | | | | |
| 18 | #5 | 3'-9" | D502 | Str. | | | | | | | | | | | |
| 18 | #5 | 4'-2" | D503 | Str. | | | | | | | | | | | |
| 12 | #5 | 4'-7" | D504 | Str. | | | | | | | | | | | |
| 18 | #5 | 4'-11" | D505 | Str. | | | | | | | | | | | |
| 18 | #5 | 5'-4" | D506 | Str. | | | | | | | | | | | |
| 6 | #5 | 5'-10" | D507 | Str. | Piers A, B & C | | | | | | | | | | |
| 6 | #5 | 7'-8" | D508 | Str. | Pier C | | | | | | | | | | |
| 8 | #5 | 4'-8" | D509 | 17 | | | 2'-0" | 8" | 2'-0" | | | | | | |
| 14 | #4 | 3'-0" | D400 | 17 | Piers A, B & C | | 6" | 2'-6" | | | | | | | |
| 18 | #4 | 3'-2" | D401 | 4 | Pier A | 3 1/2" | 5" | 1'-5" | 1'-0 1/2" | | | | 10" | 5" | 1'-1 3/4" |
| 18 | #4 | 3'-8" | D402 | 4 | Pier C | | 1'-5" | 1'-3" | 1'-0" | | | | 8" | | 1'-0 3/4" |
| 20 | #3 | 3'-8" | D300 | 17 | Piers A, B & C | | 1'-0" | 1'-8" | 1'-0" | | | | | | |
| 8 | #3 | 2'-6" | D301 | 17 | Pier B | | 1'-0" | 6" | 1'-0" | | | | | | |
| 3 | #3 | 5'-8" | D302 | 17 | Pier A | 4" | 10" | 1'-8" | 10" | 1'-8" | | 4" | | | |
| 8 | #3 | 4'-2" | D303 | 17 | Pier A | 4" | 10" | 11" | 10" | 11" | | 4" | | | |
| 6 | #3 | 4'-0" | D304 | 17 | Pier B | 4" | 10" | 10" | 10" | 10" | | 4" | | | |



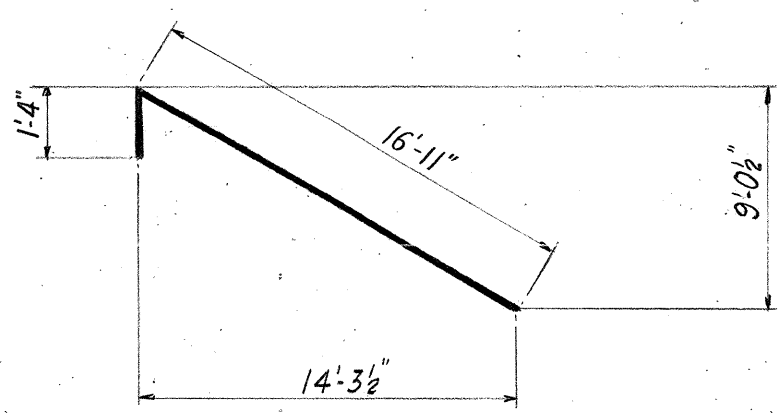
BAR MK. C309



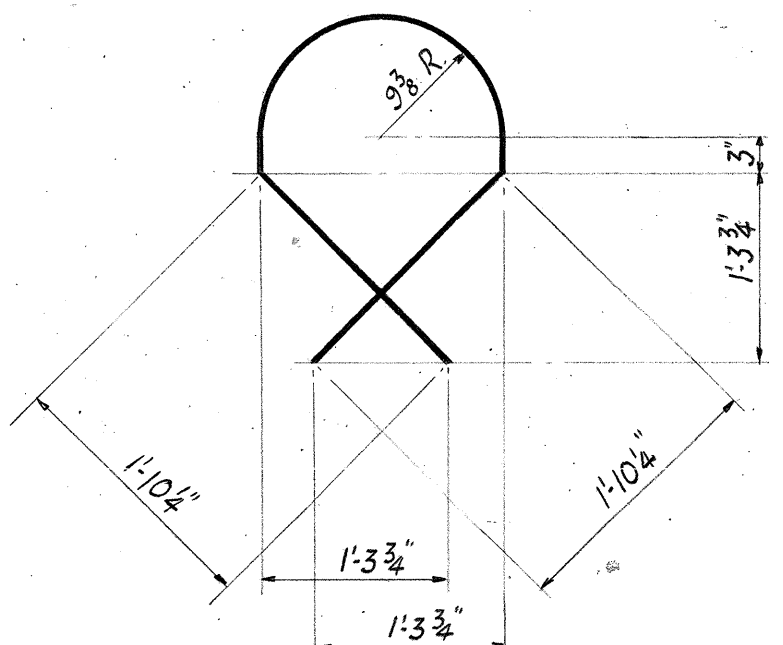
BAR MK. C401



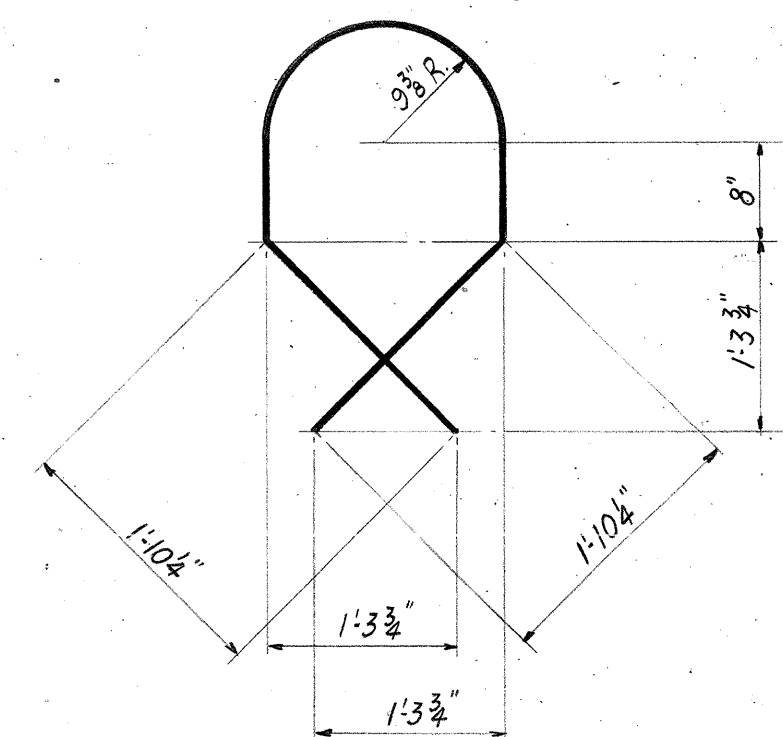
BAR MK. C702



BAR MK. C704



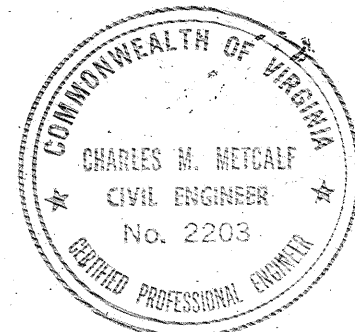
BAR MK. C510



BAR MK. D500

NOTES

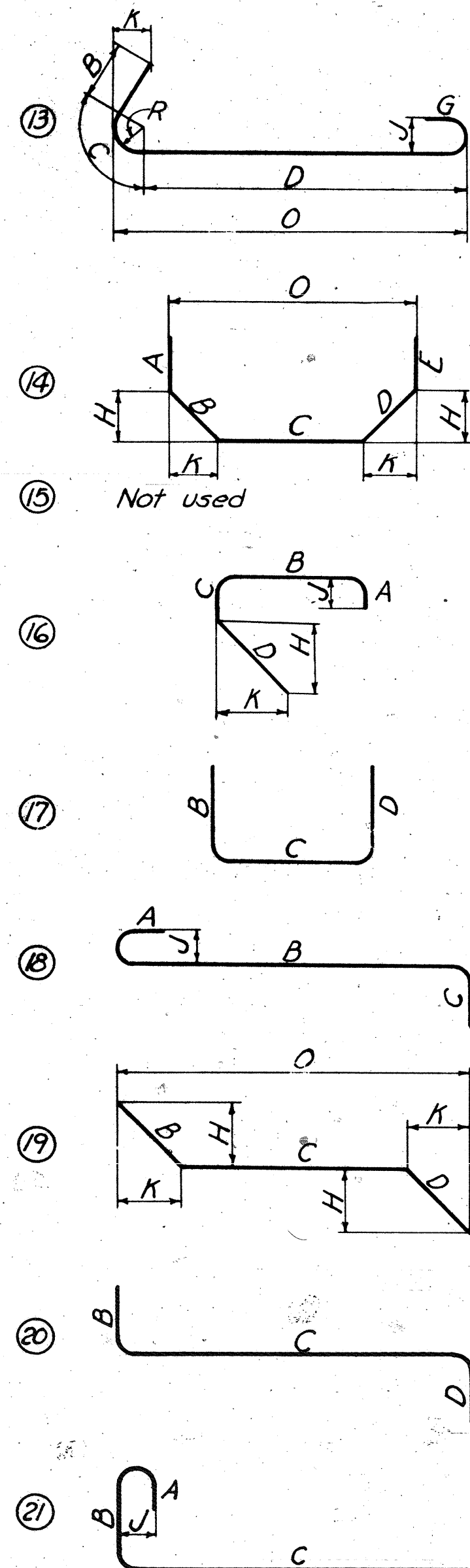
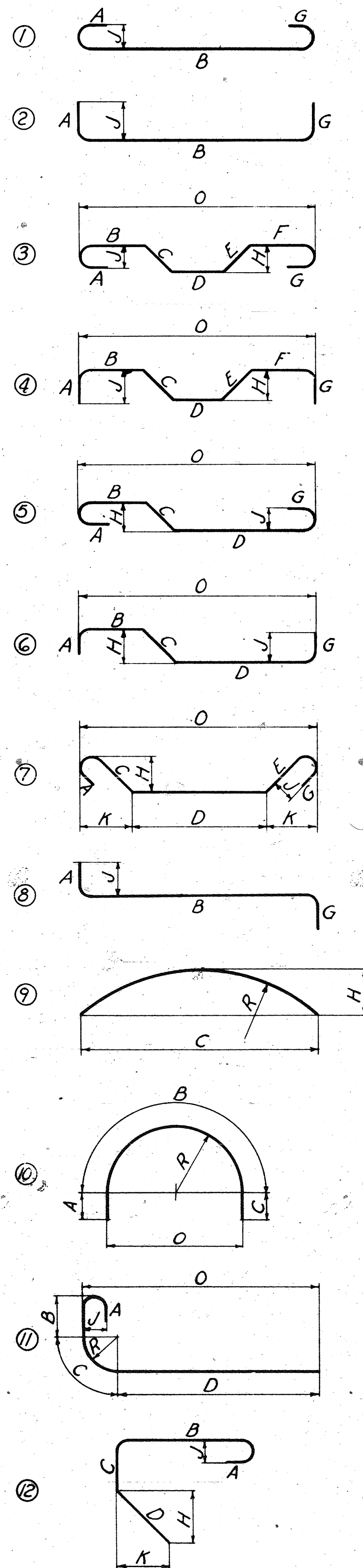
1. A dash is used in the appropriate dimension column to indicate that a hook, bend or portion of the standard bar type is to be omitted.
2. See Dwg. 26 for Typical Bar Types & Hook Dimensions.
3. Bars listed as "Special" in the Type column require special bending, see Bending Details.
4. Dimensioning, bending & hooks for Special Bending Details shall conform to the standards as noted or shown on Dwg. 26.



| | |
|--|-------------------------------------|
| COMMONWEALTH OF VIRGINIA | |
| CHESAPEAKE BAY BRIDGE AND TUNNEL DISTRICT | |
| NORFOLK 1, VIRGINIA | |
| SVERDRUP & PARCEL, CONSULTING ENGINEERS NEW YORK, N. Y. - ST. LOUIS, MO. - NORFOLK, VA. | |
| CHESAPEAKE BAY BRIDGE-TUNNEL CROSSING SOUTH TOLL PLAZA PEDESTRIAN OVERPASS BAR LIST & SPECIAL BENDING DETAILS | |
| RECOMMENDED: <i>R.E. Egle</i> | DRAWN BY: <i>Walter</i> SCALE: NONE |
| CHECKED BY: <i>Toporoff</i> | DATE: 1-16-63 |
| DWG. NO. | 25 OF 26 |
| SECTION NO. TF-3 | |

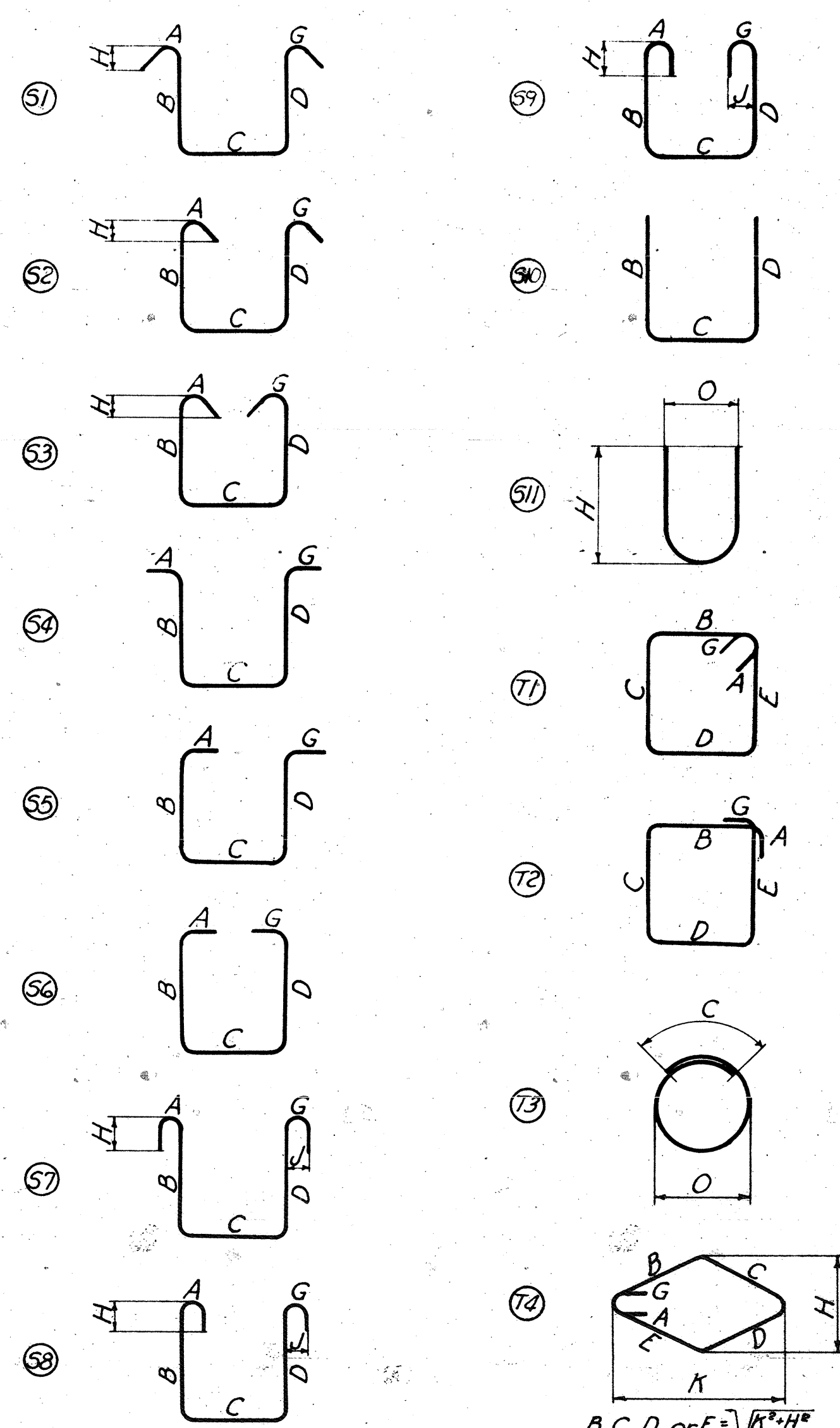
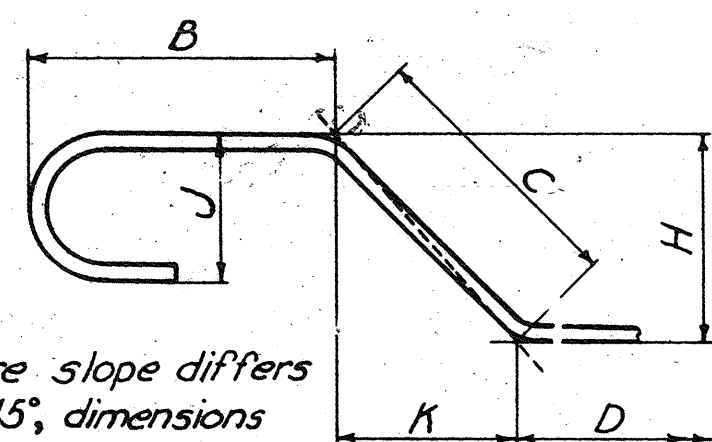
DO NOT SCALE THIS DRAWING. FOLLOW DIMENSIONS.

TYPICAL BAR TYPES



Where slope differs from 45°, dimensions "H" and "K" must be shown.

ENLARGED VIEW SHOWING BAR BENDING DETAILS

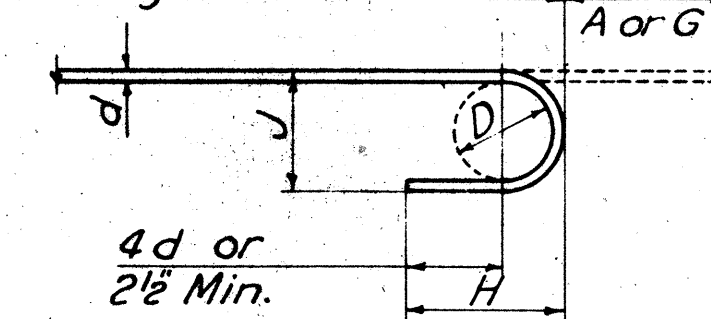


NOTES

- All dimensions are out to out, except "R" which is to inside of bend.
- "J" Dimension on 180° hooks to be shown in Bar List only where necessary to restrict hook size, otherwise standard hooks are to be used.
- Where "J" is not shown, "J" will be kept equal to or less than "H". Where "J" can exceed "H", it should be shown in Bar List.
- "H" Dimension on stirrups to be shown in Bar List where necessary to restrict hooks.
- Corrections in length, due to bending around a mandrel, will be made only when the radius "R" (as in types 11 and 13) exceeds the standard radii indicated in standard hook dimensions. However, the dimensions "A" or "G" shown for standard hooks have been corrected for curvature.
- All bends shown are bent around a standard mandrel, except where radius "R" is indicated.
- Figures in circles show bar types.
- Where "R" is shown on bar types 9, 10, 11 and 13, the length of bend shall be measured along outside of bend. The length of bar type T3 shall also be measured along outside of bar.

STANDARD HOOK DIMENSIONS

Detailing Dimension - O Hook A or G

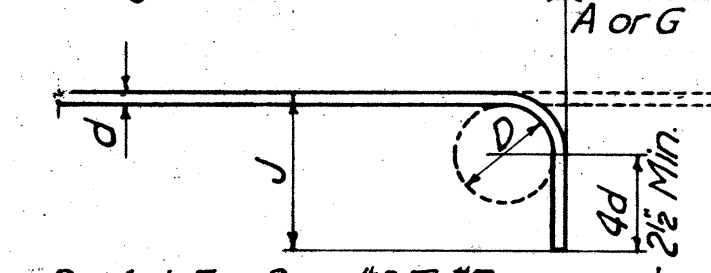


$4d$ or $2\frac{1}{2}$ Min.
 $D = 6d$ For Bars #2 To #7
 $D = 8d$ For Bars #8 To #11

| Bar Size | Hook A or G | J | Approx H |
|----------|-------------|------|----------|
| #2 | 4" | 2" | 3½" |
| #3 | 5" | 3" | 4" |
| #4 | 6" | 4" | 4½" |
| #5 | 7" | 5" | 5" |
| #6 | 8" | 6" | 6" |
| #7 | 10" | 7" | 7" |
| #8 | 11" | 10" | 9" |
| #9 | 13" | 11½" | 10½" |
| #10 | 15" | 13½" | 11½" |
| #11 | 17" | 15" | 13½" |

90° HOOK

Detailing Dimension - O Hook A or G

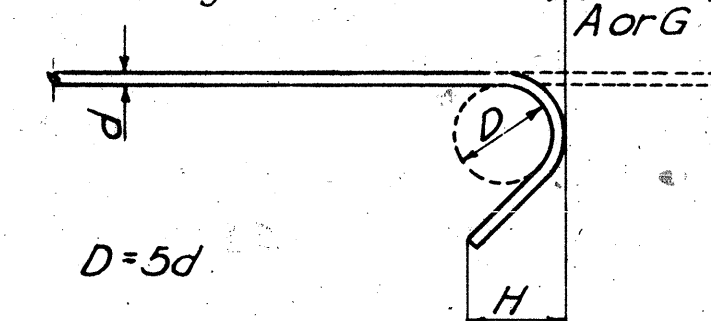


$D = 6d$ For Bars #2 To #7
 $D = 8d$ For Bars #8 To #11

| Bar Size | Hook A or G | J | Approx H |
|----------|-------------|------|----------|
| #2 | 3" | 3½" | 3½" |
| #3 | 3" | 4" | 4" |
| #4 | 3" | 4½" | 4½" |
| #5 | 4" | 5" | 5" |
| #6 | 4" | 6" | 6" |
| #7 | 5" | 7" | 7" |
| #8 | 6" | 9" | 9" |
| #9 | 7" | 10" | 10" |
| #10 | 8" | 11½" | 11½" |
| #11 | 9" | 13½" | 13½" |

135° STIRRUP HOOK

Detailing Dimension - O Hook A or G

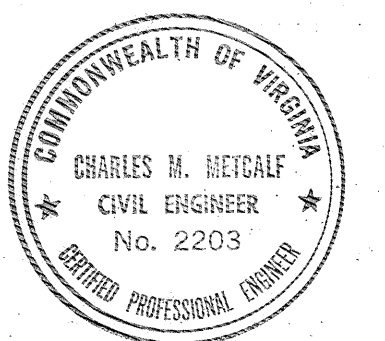


$D = 5d$

| Bar Size | Hook A or G | H |
|----------|-------------|-----|
| #2 | 3½" | 2" |
| #3 | 4" | 2½" |
| #4 | 4½" | 2½" |
| #5 | 5" | 2½" |

BAR SIZE EQUIVALENTS

| | | | |
|----|------|-----|--------|
| #2 | 6" | #7 | 7/8" |
| #3 | 3/8" | #8 | 1" |
| #4 | 1/2" | #9 | 1 1/8" |
| #5 | 5/8" | #10 | 1 3/8" |
| #6 | 3/4" | #11 | 1 5/8" |



| | |
|---|---------------------------------------|
| COMMONWEALTH OF VIRGINIA | |
| CHESAPEAKE BAY BRIDGE AND TUNNEL DISTRICT | |
| NORFOLK 1, VIRGINIA | |
| SVERDRUP & PARCEL, CONSULTING ENGINEERS NEW YORK, N. Y. - ST. LOUIS, MO. - NORFOLK, VA. | |
| CHESAPEAKE BAY BRIDGE-TUNNEL CROSSING SOUTH TOLL PLAZA PEDESTRIAN OVERPASS TYPICAL BAR TYPES AND HOOK DIMENSIONS | |
| RECOMMENDED: <i>P.E. Eggen</i> | DRAWN BY: <i>Chauntia</i> SCALE: None |
| CHECKED BY: <i>Toporoff</i> | DATE: 1-16-63 |
| APPROVED: <i>Ch. Metcalf</i> | DWG. NO. 26 of 26 |
| SECTION NO. TF-3 | |