County of Ocean

RECONSTRUCTION AND RESURFACING OF

NORTH COUNTY LINE ROAD

FROM JACKSON MILLS ROAD TO BENNETTS MILLS ROAD

TOWNSHIP OF JACKSON

OCEAN COUNTY, NEW JERSEY

UTILITIES

UTILITY POLES AND/OR UNDERGROUND CABLES;

JERSEY CENTRAL POWER AND LIGHT CO.
ATTN: HARVEY LOCKLEY
101 CRAWFORD'S CORNER ROAD
BUILDING #1, SUITE 1-511
HOLMDEL, NJ 07733
TELEPHONE: 732-212-4262

VERIZON ENGINEERING
ATTN: ARTURO CABRERA
999 WEST MAIN STREET
FREEHOLD, NJ 07728
TELEPHONE: 732-683-5124

VERIZON ENGINEERING
ATTN: BRYAN C DePAUL
999 WEST MAIN STREET, FLOOR 2
FREEHOLD, NJ 07728
TELEPHONE: 732-546-2472

VERIZON BUSINESS GROUP (MCI)
ATTN: JOHN ALESSANDRINI
630 CLARK AVE
KING OF PRUSSIA, PA 19406
TELEPHONE: 610-517-8456

VERIZON BUSINESS GROUP (MCI)
ATTN: NAHED SHAHATA
355 FOREST AVE
STATEN ISLAND, NY 10301
TELEPHONE: 646-529-0572

A.T. & T.
ATTN: LOUIS MARELO
CABLE PROTECTION CENTER
ENGINEERING INQUIRIES
40 HAMILTON AVENUE, MAIL ROOM
WHITE PLAINS, NY 10601
TELEPHONE: 914-467-1097

TELEPORT COMMUNICATIONS AMERICA, LLC (AT&T)
CABLE PROTECTION
CARLO VERDI, LOCAL ENGINEER
ENGINEERING INQUIRIES
2315 SALEM ROAD
WALL STREET - BUILDING #3
CONYERS, GA 30013
TELEPHONE: 732-610-9598

COGENT COMMUNICATIONS (SPRINT NEXTEL)
OUTSIDE PLANT ENGINEERING
ATTN: RICH CONKLIN
435 VALLEY BROOK AVENUE, BOX 270
LYNDHURST, NJ 07071
TELEPHONE: 646-208-2301

CATV;

ALTICE TECH SERVICES USA
ATTEN: JOHN MACALLUM/JEREMY SHIHADEH
40 PINE ST
TINTON FALLS, NJ 07753
TELEPHONE: 732-243-6279

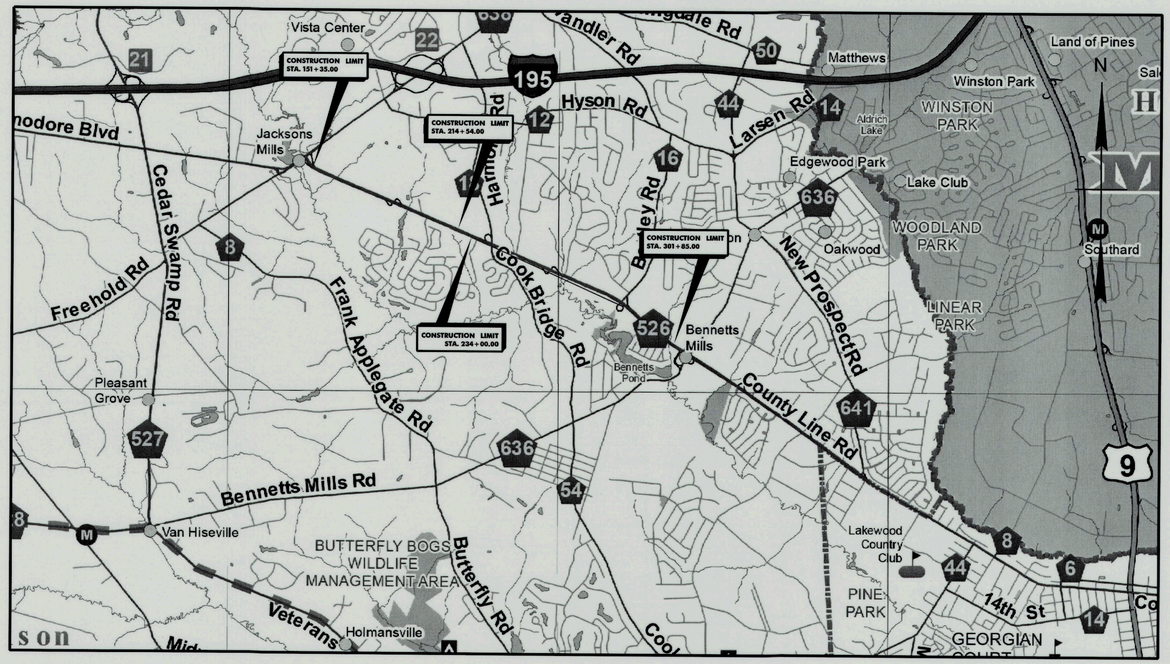
WATER AND/OR SEWER MAINS;

OCEAN COUNTY UTILITIES AUTHORITY
ATTN: ROBERT McGLAUGHLIN
501 HICKORY LANE, P.O. BOX P
BAYVILLE, NJ 08721
TELEPHONE: 732-269-4500

JACKSON TOWNSHIP M.U.A.
ATTN: MR. EARL QUIJANO
135 MANHATTAN STREET
JACKSON, NJ 08527
TELEPHONE: 732-928-2222

GAS MAINS;

NEW JERSEY NATURAL GAS CO.
ATTN: MR. KYLE RAUTH, SUPV. ENGINEER
DISTRIBUTION ENGINEERING
1415 WYCKOFF ROAD, P.O. BOX 1464
WALL, NJ 07719
TELEPHONE: 908-216-9721



KEY MAP
SCALE 1" = APPROX. 2500

2500' 0 2500' 5000'

INDEX OF SHEETS

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Board of Commissioners

Frank Sadeghi, Director
Jennifier Bacchione, Deputy Director
Robert S. Arace
Sam Ellenbogen
Ray Gormley

TOTAL LENGTH OF THE PROJECT 13,104 LINEAR FEET OR 2.48 MILES

2019 NJDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AS AMENDED BY THE NJDOT SPECIAL PROVISIONS AND THE OCEAN COUNTY SUPPLEMENTAL SPECIFICATIONS, TO GOVERN
2016 NJDOT STANDARD CONSTRUCTION DETAILS FOR ROADWAY, TRAFFIC CONTROL, AND BRIDGE BOOKLET;
AND THE
2007 NJDOT STANDARD ELECTRICAL DETAILS BOOKLET, ARE APPLICABLE TO THIS PROJECT EXCEPT FOR THOSE DETAILS CONTAINED HEREIN

DATE	REVISION	BY	CHK

Pennoni
NJ COA NO. GA28033300
BETH-ANN M. GRASSO, P.E.
STATE OF NEW JERSEY · BETH-ANN M. GRASSO · No. GE 43121 · LICENSED PROFESSIONAL ENGINEER
4/28/2026
PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

MARK F. JEHNKE
COUNTY ENGINEER
PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04670300

APPROVALS OF THE COUNTY

DIRECTOR — DATE
CLERK OF THE BOARD — DATE
COUNTY ENGINEER P.E. NO. 46703 — DATE 5/2/26

FILE NO. 12 - 009 - 0128
SHEET 1 OF 84

PAY ITEM NO.	STANDARD ITEM NO.	DESCRIPTION	UNIT	CONTRACT QUANTITY	PLAN SHEET TOTALS	IF AND WHERE DIRECTED	AS-BUILT QUANTITY	DISTRIBUTION: PLAN SHEET QUANTITY																									
								C01	C02	C03	C04	C05	C06	C07	C08	C09	C10	C11	C12	C13	C14	C15	C16	C17	C18								
1	105004M	CONSTRUCTION LAYOUT	DOLLAR	DOLLAR																													
2	151006M	PERFORMANCE BOND AND PAYMENT BOND	DOLLAR	DOLLAR																													
3	154003P	MOBILIZATION	LUMP SUM	1																													
4	159003M	BREAKAWAY BARRICADE	UNIT	60		60																											
5	159006M	DRUM	UNIT	310		310																											
6	159009M	TRAFFIC CONE	UNIT	30		30																											
7	159012M	CONSTRUCTION SIGNS	S.F.	512		512																											
8	159027M	ARROW BOARD, 4' X 8'	UNIT	4		4																											
9	159030M	PORTABLE VARIABLE MESSAGE SIGN	UNIT	4		4																											
10	159108M	TRAFFIC CONTROL TRUCK WITH MOUNTED CRASH CUSHION	UNIT	1		1																											
11	159140M	UNIFORMED TRAFFIC DIRECTOR, JACKSON TOWNSHIP	HR.	465		465																											
12	159141M	TRAFFIC DIRECTOR, FLAGGER	HR.	144		144																											
13	160004M	FUEL PRICE ADJUSTMENT	DOLLAR	DOLLAR																													
14	160007M	ASPHALT PRICE ADJUSTMENT	DOLLAR	DOLLAR																													
15	201003P	CLEARING SITE	LUMP SUM	1																													
16	202006M	EXCAVATION, TEST PIT	C.Y.	10		10																											
17	202009P	EXCAVATION, UNCLASSIFIED	C.Y.	24	4	20															4												
18	203021P	I-14 SOIL AGGREGATE	C.Y.	20		20																											
19	302036P	DENSE-GRADED AGGREGATE BASE COURSE, 6" THICK	S.Y.	61	11	50															11												
20	401027M	POLYMERIZED JOINT ADHESIVE	L.F.	123,861	113,861	10,000		6,023	7,013	7,204	7,228	8,562	7,443	7,332	4,101	5,884	9,874	6,821	7,189	772	9,220	5,018	6,813	6,444	920								
21	401030M	TACK COAT	GAL.	11,919	10,919	1,000		540	647	654	692	893	758	663	371	557	947	687	662	64	918	523	641	622	80								
22	401058M	HOT MIX ASPHALT 12.5M64 SURFACE COURSE, 2" THICK	TON	14,115	13,115	1,000		648	775	785	830	1,072	910	797	444	667	1,135	826	794	77	1,101	627	784	747	96								
23	401089M	HOT MIX ASPHALT 19M64 BASE COURSE, 4" THICK	TON	23	3	20															3												
24	401272M	HMA MILLING, 2" DEPTH	S.Y.	119,154	109,154	10,000		5,400	6,463	6,543	6,919	8,930	7,584	6,636	3,705	5,565	9,464	6,877	6,618	644	9,159	5,229	6,402	6,218	798								
25	602099M	RESET EXISTING CASTING	UNIT	5	1	4							1																				
26	602213M	CURB PIECE	UNIT	23	21	2		15	1	1		2	2																				
27	606012P	CONCRETE SIDEWALK, 4" THICK	S.Y.	114	104	10							44								38		11	11									
28	606035P	HOT MIX ASPHALT DRIVEWAY, 3" THICK	S.Y.	193	178	15			178																								
29	606051P	CONCRETE DRIVEWAY, 6" THICK	S.Y.	25	25								25																				
30	606084P	DETECTABLE WARNING SURFACE	S.Y.	20	17	3							8								6		1	2									
31	607021P	9" X 18" CONCRETE VERTICAL CURB	L.F.	209	189	20							72								103		8	6									
32	607034P	9" X 14" CONCRETE VERTICAL CURB	L.F.	1,252	1,152	100			132	128		249	185	415	43																		
33	608003P	NONVEGETATIVE SURFACE, HOT MIX ASPHALT	S.Y.	1,248	1,198	50			79	47		280	111	312	242						127												
34	609003M	BEAM GUIDE RAIL	L.F.	791	791				38	13		163	88	313	63						113												
35	609027M	TANGENT GUIDE RAIL TERMINAL	UNIT	10	10				1			3	1	1	3						1												
36	609039M	BEAM GUIDE RAIL ANCHORAGE	UNIT	6	6							2		1	2						1												
37	609059M	APPROACH GUIDE RAIL TRANSITION TL-3	UNIT	6	6				1	1		1	1	1	1																		
38	611312M	CRASH CUSHION, COMPRESSIVE BARRIER, TYPE 3, WIDTH NARROW	UNIT	2	2					1					1																		
39	804000P	TOPSOIL SPREADING, 5" THICK	S.Y.	159	129	30							47								60		11	11									
40	806010P	FERTILIZING AND SEEDING, TYPE A-5	S.Y.	159	129	30							47								60		11	11									
41	809003M	STRAW MULCHING	S.Y.	159	129	30							47								60		11	11									

OCEAN COUNTY DEPARTMENT OF ENGINEERING

ESTIMATE - DISTRIBUTION OF QUANTITIES

STATE OF NEW JERSEY
BETH-ANN M. GRASSO
No. GE 43121
LICENSED PROFESSIONAL ENGINEER
EDQ01
EDQ02

Pennoni

NJ COA NO. GA28033300
BETH-ANN M. GRASSO, P.E.
4/28/2026
PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

DATE	REVISIONS	BY	CHK.

RECONSTRUCTION OF

NORTH COUNTY LINE RD

FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

ESTIMATE & DISTRIBUTION OF QUANTITIES

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE NOT TO SCALE	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 2 OF 84

I:\Projects\2009\009-0128\009-0128 NCL Resurf & Rehab\25-00128\PLS_DISTRIBUTION.dgn
PENNONI-MAP-CONN-CO-ALL-LIB_pw.mtl,CFG
PENNONI, 10/28/19

PAY ITEM NO.	STANDARD ITEM NO.	DESCRIPTION	UNIT	CONTRACT QUANTITY	PLAN SHEET TOTALS	IF AND WHERE DIRECTED	AS-BUILT QUANTITY	DISTRIBUTION: PLAN SHEET QUANTITY																											
								TSS01	TSS02	TSS03	TSS04	TSS05	TSS06	TSS07	TSS08	TSS09	TSS10	TSS11	TSS12	TSS13	TSS14	TSS15	TSS16	TSS17	TSS18	E01	E02	E03	E04						
42	159300M	TRAFFIC STRIPES, LATEX 4"	L.F.	39,223	39,223			1,244	2,188	2,250	2,250	2,859	4,751	2,251	1,261	1,813	3,116	2,276	2,238	93	4,221	1,563	2,396	2,188	265										
43	159320M	TRAFFIC MARKING SYMBOLS, LATEX	S.F.	821	821			129					184				138			8	362														
44	610003M	TRAFFIC STRIPES, 4"	L.F.	28,179	27,179	1,000		1,785	1,775	1,801	1,801	1,760	1,622	1,802	1,008	1,451	1,821	1,821	1,791	422	1,755	1,251	1,700	1,601	212										
45	610006M	TRAFFIC STRIPES, 6"	L.F.	30,194	29,194	1,000		1,424	2,214	2,302	1,897	1,720	1,856	2,306	1,296	1,409	1,831	2,050	2,282	214	1,277	1,186	1,991	1,664	275										
46	610012M	RPM, MONO-DIRECTIONAL, WHITE LENS	UNIT	611	611			86	26	22	31	63	32	24	12	25	64	33	24	8	77	27	21	26	10										
47	610016M	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 12"	L.F.	530	490	40		112				107					89			66	80		36												
48	610018M	RPM, MONO-DIRECTIONAL, AMBER LENS	UNIT	334	334			25	22	22	22	23	18	22	12	19	24	23	22		21	16	21	19	3										
49	610021M	RPM, BI-DIRECTIONAL, AMBER LENS	UNIT	7	7															4	3														
50	610112M	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 6"	L.F.	855	785	70						99	131				167				346	42													
51	610115M	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 8"	L.F.	3,326	3,176	150		332			185	411	212	24		126	511	114	27	117	681	186	108	140	2										
52	610121M	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 24"	L.F.	1,180	1,130	50		33				103	407				126			16	366		47	32											
53	610124M	TRAFFIC MARKING, SYMBOLS, LONG LIFE THERMOPLASTIC	S.F.	871	821	50		129					184				138			8	362														
54	612003P	REGULATORY AND WARNING SIGN	S.F.	158	158			37				32	13				28			13	35														
55	612019P	MAST ARM STREET NAME SIGNS	S.F.	99	99																					21	39	21	18						
56	701021P	3" RIGID METALLIC CONDUIT	L.F.	190	190																						145		45						
57	701135M	FOUNDATION, TYPE SPF	UNIT	8	8																						5		3						
58	701144M	FOUNDATION, TYPE SFK	UNIT	1	1																						1								
59	701192P	GROUND WIRE, NO. 8 AWG	L.F.	190	190																						145		45						
60	702012M	TRAFFIC SIGNAL STANDARD, ALUMINUM	UNIT	1	1																						1								
61	702018M	PEDESTRIAN SIGNAL STANDARD	UNIT	8	8																						5		3						
62	702021M	TRAFFIC SIGNAL MAST ARM, ALUMINUM	UNIT	1	1																						1								
63	702027P	TRAFFIC SIGNAL CABLE, 2 CONDUCTOR	L.F.	2,145	2,145																						1,035		1,110						
64	702030P	TRAFFIC SIGNAL CABLE, 5 CONDUCTOR	L.F.	1,575	1,575																						1,095		480						
65	702033P	TRAFFIC SIGNAL CABLE, 10 CONDUCTOR	L.F.	345	345																						345								
66	702036M	TRAFFIC SIGNAL HEAD	UNIT	4	4																						4								
67	702038M	BACKPLATE	UNIT	31	31																					6	9	6	10						
68	702039M	PEDESTRIAN SIGNAL HEAD	UNIT	9	9																						6		3						
69	702042M	PUSH BUTTON	UNIT	12	12																						6		6						
70	702060M	CONTROLLER TURN-ON	UNIT	3	3																					1		1	1						
71	NS	REGULATORY AND WARNING SIGN, SIGNAL MOUNTED	S.F.	73	73																					19	47	3	4						
72	NS	CONTROLLER, 8 PHASE, REPLACEMENT	UNIT	3	3																					1		1	1						

OCEAN COUNTY DEPARTMENT OF ENGINEERING

ESTIMATE - DISTRIBUTION OF QUANTITIES

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

STATE OF NEW JERSEY
BETH-ANN M. GRASSO
No. GE 43121
LICENSED PROFESSIONAL ENGINEER
EDQ02

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

ESTIMATE & DISTRIBUTION OF QUANTITIES

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE NOT TO SCALE	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 3 OF 84

Z:\Accounts\OCEAN\2009\009 - Cnty Co Rd Improvements\Dwg\Estimate_QuantityDist.dwg
RS\ROW-Bh-CS\BA-CD\ALA-RLL-RP_plot\TCPS
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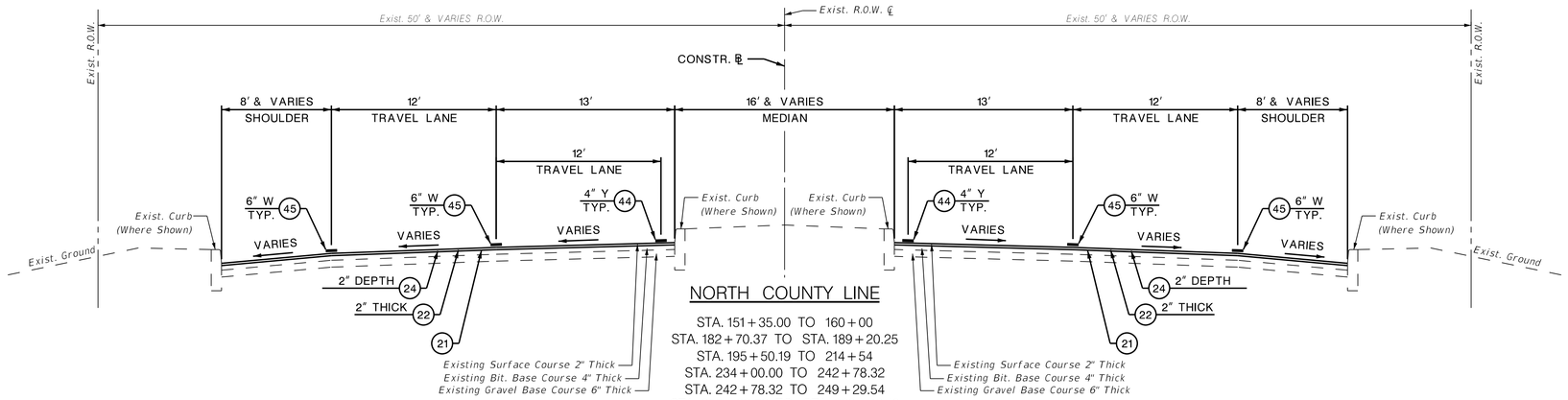

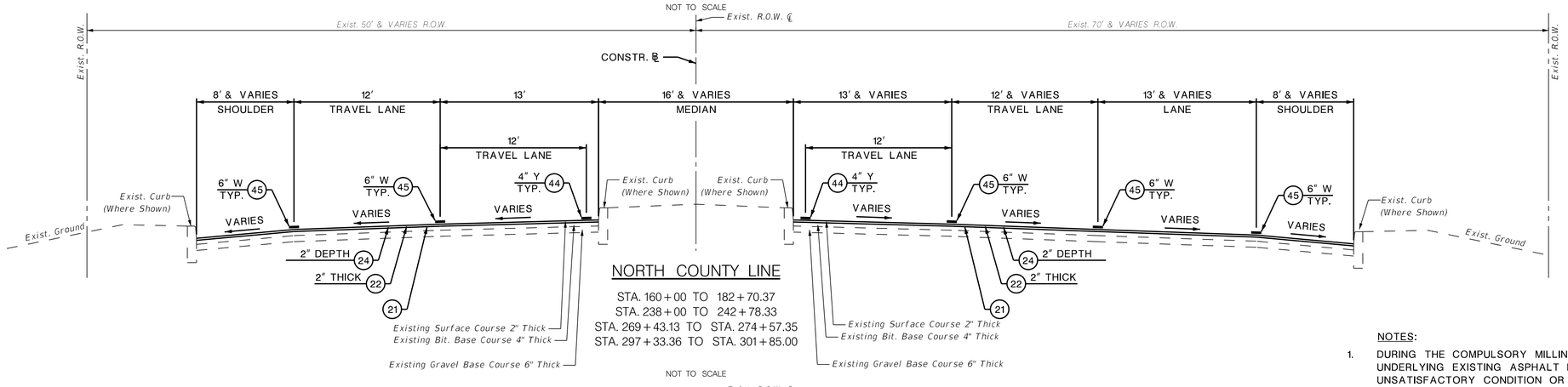

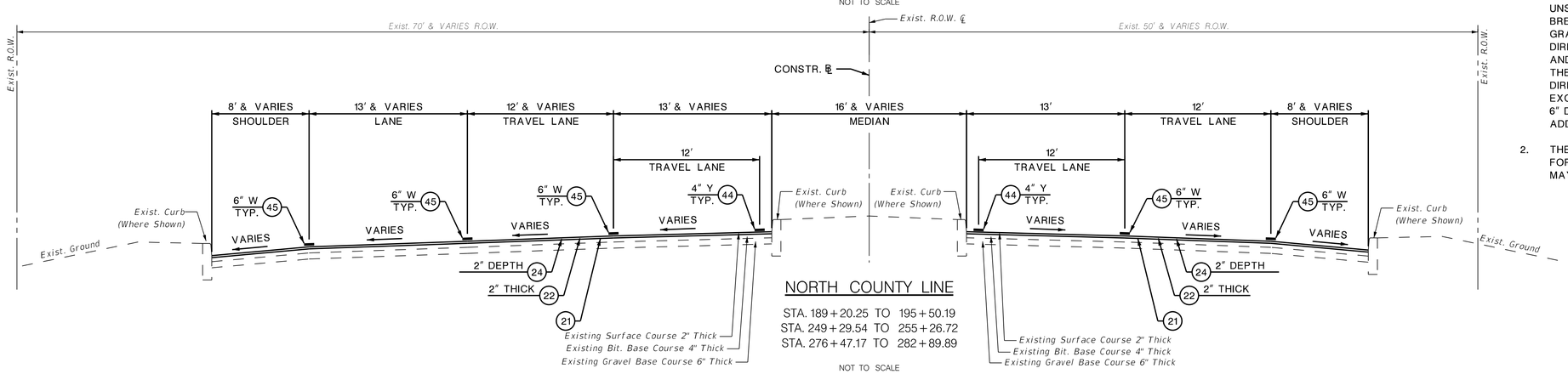

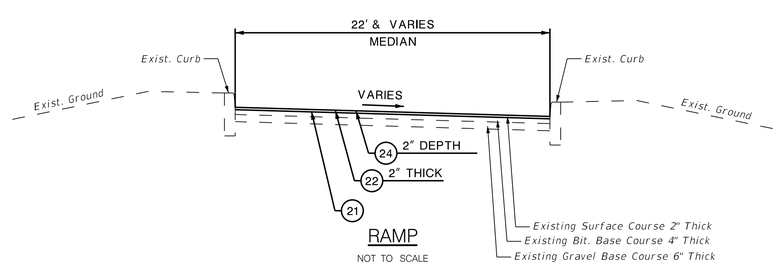

NOTES:

1. DURING THE COMPULSORY MILLING OPERATIONS, IF THE UNDERLYING EXISTING ASPHALT PAVEMENT IS OF UNSATISFACTORY CONDITION OR THE MILLING MACHINE BREAKS THROUGH THE EXISTING ASPHALT AND EXPOSES GRAVEL OR SOIL AGGREGATE, THE CONTRACTOR, AT THE DIRECTION OF THE ENGINEER, SHALL MILL AN ADDITIONAL 4" AND CONSTRUCT 4" HOT MIX ASPHALT BASE COURSE. IF THE EXPOSED SOIL OR GRAVEL IS UNACCEPTABLE, AT THE DIRECTION OF THE ENGINEER, THE CONTRACTOR SHALL EXCAVATE AN ADDITIONAL SIX (6) INCHES AND CONSTRUCT 6" DENSE GRADED AGGREGATE BASE COURSE AND THE ADDITIONAL 4" HOT MIX ASPHALT BASE COURSE.

2. THE EXISTING ASPHALT AND GRAVEL LAYERS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. ACTUAL CONDITIONS MAY VARY.

PROPOSED MATERIALS

PAY ITEM NO.	STD. ITEM NO.	DESCRIPTION
21	401030M	TACK COAT
22	401058M	HOT MIX ASPHALT 12.5M64 SURFACE COURSE, 2" THICK
24	401272M	HMA MILLING, 2" DEPTH
44	610003M	TRAFFIC STRIPES, 4"
45	610006M	TRAFFIC STRIPES, 6"

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

STATE OF NEW JERSEY — BETH-ANN M. GRASSO — No. GE 43121 — LICENSED PROFESSIONAL ENGINEER — TS01

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

TYPICAL SECTIONS

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE NOT TO SCALE	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 4 OF 84

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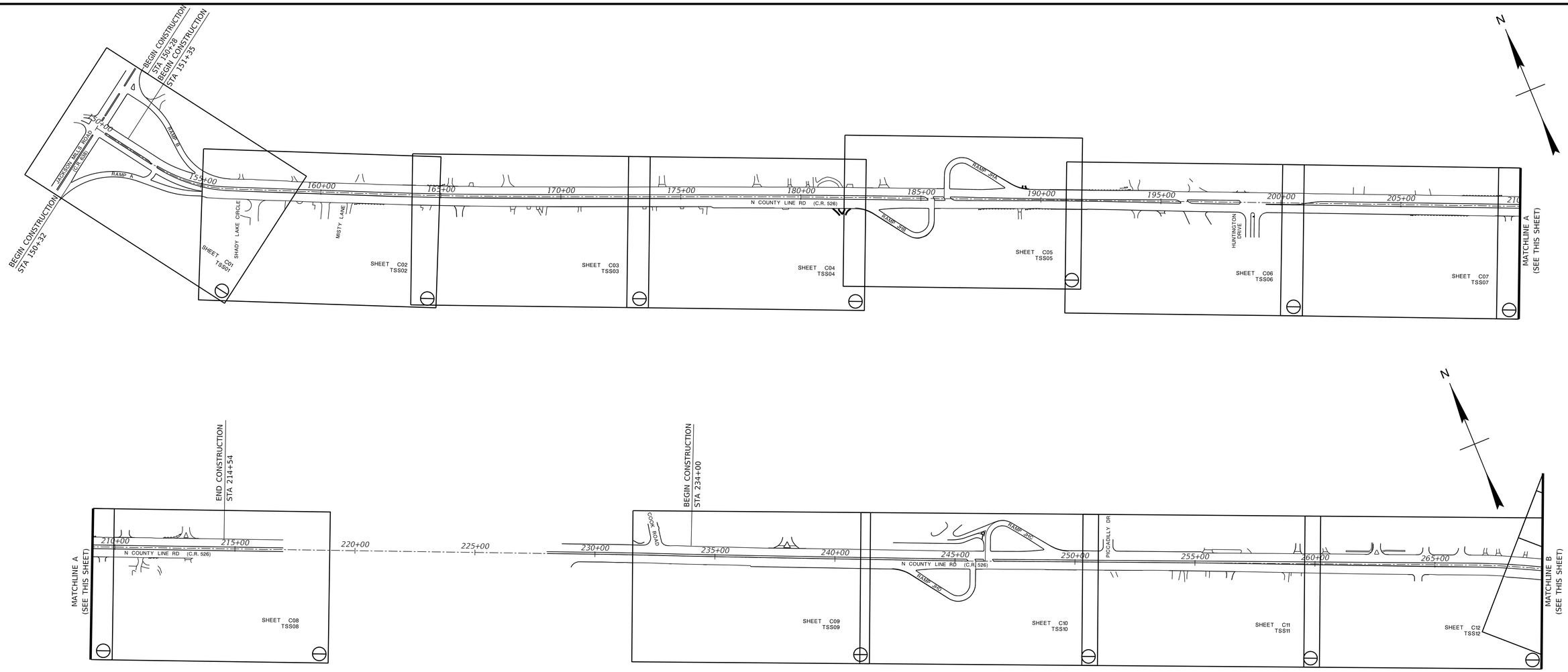
PENNONI - NEWARK, NJ



DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

STATE OF NEW JERSEY
BETH-ANN M. GRASSO
No. GE 43121
LICENSED PROFESSIONAL ENGINEER

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

PLAN SHEET INDEX

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE 1"=200'	DRAWN C.D.	APPROVED B.M.G.
FILE NO.	12 - 009 - 0128		SHEET 5 OF 84

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OCEAN COUNTY ENGINEERING DEPARTMENT

STANDARD LEGEND

LINEAR FEATURES

	Existing	PROPOSED
Base Line Stationing	5+00 ℄	℄ 10+00
Bulkhead		
Bank, Bottom of		
Bank, Top of		
Centerline of Stream		
Centerline of Road		
Contours (1,2,3)		
Contours (5,10)		
Curb (Asphalt)		
Curb (Concrete)		
Curb (Belgian Block)		
Ditch	Ditch	DITCH
Drainage Pipe (Size)	(Size & Type)	(SIZE & TYPE)
Driveway (Paved)		
Driveway (Unpaved)		
Easement Line		
Edge of Pavement		
Edge of Trail		
Edge of Water		
Fence (Type)	x x x x	
Fence (Reset)		
Floating Turbidity Barrier		
Guide Rail (Left)		
Guide Rail (Right)		
Landscape Hedges		
Limit of Disturbance		
Municipal or County Line		
Wall (Retaining, Noise)		
Property Line	℘	℘
Railroad Tracks		
Right of Way (R.O.W.)	Existing R.O.W. Line	PROPOSED R.O.W. LINE
Sidewalk (Asphalt)		
Sidewalk (Concrete)		
Silt Fence		
Slope Line	(F) (C)	(C) (F)
Swale		
Tree Line		
Utility Line Cable TV	CTV	
Utility Line Electric Conduit	E	
Utility Line Fiber Optic	FO	
Utility Line Gas (Size)	G	
Utility Line Overhead	OH	
Utility Line Sanitary Sewer		
Utility Line Telephone	T	
Utility Line Water (Size)	W	
Wetlands Limit/Points		

TOPOGRAPHICAL FEATURES

	Existing	RESET	PROPOSED
Standard Inlet			
DOT Inlet			
Flat Grate Inlet			
Manhole (Assorted Utilities)			
Cast Iron Extension (Frame or Ring) (Inlet or Manhole)			
New Manhole Casting, Square Frame, Circular Cover			
R.C. End Section or C.M. Headwall			
Headwall			
Headwall with Apron			
Gas Valve	G		G
Water Valve	W		W
Fire Hydrant			
Utility Pole with Light (Type & Number)			
Utility Pole (Type & Number)			
Light Pole			
Lamp Post			
Stop Sign	STOP		
Sign			
Street Sign			
Deciduous Tree			
Conifer Tree			
Shrubs			
Swamp			
Soil Boring			
Test Pit			
Bench Mark	BM		
Monument Found			
GPS Monument Found			
PK Nail Found			
SFT – Signalized Traffic Foundation			
SPF – Signalized Traffic Foundation			
Junction Box 18"x 36"	JB		JB
Junction Box 20" Round	JB		JB
Traffic Controller			
High Point			
Low Point			

ABBREVIATIONS USED IN THIS CONTRACT

℄, B.L.	BASELINE
B.M.	BENCH MARK
B.T.	BELL TELEPHONE
BIT., BITUM.	BITUMINOUS
BLDG.	BUILDING
℄, C.L.	CENTERLINE
C.I.P.	CAST IRON PIPE
C.M.P.	CORRUGATED METAL PIPE
CONC.	CONCRETE
C.Y.	CUBIC YARD
CULV.	CULVERT
D, DIA.	DIAMETER
DE	DITCH EXCAVATION
DEP., DP	DEPRESSED CURB
DWY	DRIVEWAY
E.B., W.B.	EASTBOUND, WESTBOUND
N.B., S.B.	NORTHBOUND, SOUTHBOUND
EL., ELEV.	ELEVATION
E.O.P.	EDGE OF PAVEMENT
EXIST.	EXISTING
F.C.	FLUSH CURB
GAL.	GALLON
GR., GRT.	GRATE
G	GUTTER
H.D.P.E.P.	HIGH DENSITY POLYETHYLENE PIPE
H.M.A.	HOT MIX ASPHALT
HT.	HEIGHT
H.W.	HEADWALL
HYD.	HYDRANT
INV.	INVERT
IP	IRON PIN
J.B.	JUNCTION BOX
L.F.	LINEAR FOOT, LINEAR FEET
L.P.	LIGHT POLE
L.O.D.	LIMIT OF DISTURBANCE
L.O.M.	LIMIT OF MILLING
L.O.P.	LIMIT OF PAVEMENT (PAVING)
LT., RT.	LEFT, RIGHT
M.B.	MAILBOX
MAX.	MAXIMUM
MIN.	MINIMUM
NO.	NUMBER
N.T.S.	NOT TO SCALE
PAV'T.	PAVEMENT
PERF.	PERFORATED
℘, P.L.	PROPERTY LINE, PROFILE LINE
PK	PARKER KAYLON MASONRY NAIL
POC, P.O.C.	POINT ON CURVE
POL, P.O.L.	POINT ON LINE
POT, P.O.T.	POINT ON TANGENT
PRC, P.R.C.	POINT OF REVERSE CURVE
PROP.	PROPOSED
PT, P.T.	POINT OF TANGENCY
PVCP, P.V.C.P.	POLYVINYL CHLORIDE PIPE,
PVC, P.V.C.	POINT OF VERTICAL CURVATURE
PVI, P.V.I.	POINT OF VERTICAL INTERSECTION
PVT, P.V.T.	POINT OF VERTICAL TANGENCY, PAVEMENT
R, RAD.	RADIUS
RCCP, R.C.C.P.	REINFORCED CONCRETE CULVERT PIPE
RCP, R.C.P.	REINFORCED CONCRETE PIPE
RMC, R.M.C.	RIGID METALLIC CONDUIT
RNMC, R.N.M.C.	RIGID NON-METALLIC CONDUIT
ROW, R.O.W.	RIGHT OF WAY
R.R.	RAILROAD
RTE., RT.	ROUTE
SAN. SEW.	SANITARY SEWER
SW.	SIDEWALK
SHLD.	SHOULDER
S.F.	SQUARE FOOT, SQUARE FEET
S.Y.	SQUARE YARD
S.H.D.	STATE HIGHWAY DEPARTMENT
S.O.D.	SUBBASE OUTLET DRAIN
℄, S.L.	SURVEY LINE
T	TANGENT
TBA	TO BE ABANDONED
TBR	TO BE REMOVED
TC	TOP OF CURB
TCD	TOP OF DEPRESSED CURB
TN.	TON
TEL.	TELEPHONE
TEMP.	TEMPORARY
THK., TH.	THICK
TYP.	TYPICAL
U.D.	UNDERDRAIN
UN.	UNIT
UP, U.P.	UTILITY POLE
VAR.	VARIABLE, VARIES
W.C.V.C.	WHITE CONCRETE VERTICAL CURB
W.V.	WATER VALVE
X-SECT	CROSS SECTION

MISCELLANEOUS SYMBOLS

N.T.S. — North Arrow To Be Used On Plan Sheets

Pay Items Used On Plan Sheets

Roadway Excavation

Area Of Reconstruction (base, surface or intermediate course)

Concrete Driveway

Asphalt Driveway

Concrete Sidewalk, 4" Thick

Turf Repair (topsoil, fertilizing, seeding and mulch)

Proposed Bridge

Bridge Approach Slabs and Transition Slabs

DOUBLE REFERENCE CODES

TS01 / TS21 — CODES BELOW LOCATED IN BOX ON TITLE BLOCKS

EQ	- ESTIMATE & DISTRIBUTION OF QUANTITIES (ROADWAY)
TS	- TYPICAL SECTIONS
PSI	- PLAN SHEET INDEX
SL	- STANDARD LEGEND
GN	- GENERAL NOTES & S.C.D. NOTES
C	- CONSTRUCTION PLANS (PROFILES MAY BE INCLUDED)
ECS	- EROSION & SEDIMENT CONTROL NOTES & DETAILS
TIE	- TIES
DU	- DRAINAGE, GRADING, AND SEDIMENT & EROSION CONTROL PLANS
G	- GRADES
ET	- ENTIRE TRACT MAP
GP	- GENERAL PARCEL MAP
TCP	- TRAFFIC CONTROL DETAILS (AND STAGING PLANS)
TSS	- TRAFFIC SIGNING & STRIPING PLANS
E	- ELECTRICAL PLANS
TSP	- TRAFFIC SIGNAL PLANS
X	- CROSS SECTIONS
CD	- CONSTRUCTION DETAILS
ADA	- ADA CURB RAMP DETAILS
ED	- ELECTRICAL DETAILS

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

STATE OF NEW JERSEY — BETH-ANN M. GRASSO — No. GE 43121 — LICENSED PROFESSIONAL ENGINEER

SL01

RECONSTRUCTION OF

NORTH COUNTY LINE RD

FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

STANDARD LEGEND

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE NOT TO SCALE	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 6 OF 84

OCEAN COUNTY ENGINEERING DEPARTMENT

GENERAL CONSTRUCTION NOTES

1. Prior to beginning construction the Contractor, in conjunction with the Engineer and the Inspector, shall document the existing on site and adjoining off site conditions, by means of date stamped Digital Video supplemented with date stamped Digital Photographs and dated written notes. Areas of special concern may be documented during the course of the contract, by approval of the Engineer, and only if the documentation is made in conjunction with the Engineer and the Inspector prior to commencement of any construction in the area concerned. Copies of all digital and written material shall be provided to the County prior to commencement of work, Documentation of the existing on site and adjoining off site conditions shall be used in the event of any dispute arising during the course of construction. The County will reject all claims which are not verified by this prior documentation.

2. The contractor shall prepare and submit all documents to obtain all permits and approvals necessary for this work. The charges to prepare the documents and the fees required for all permits, approvals, and registrations shall be paid by the contractor in accordance with Subsection 107.05.

3. It shall be the Contractor's responsibility to visit the site before bidding on the project to determine if there are existing above ground utilities that may influence the planned construction activity. It shall also be the Contractor's responsibility to contact any of the utilities noticed in the field or shown on the construction plans to acquire information regarding said utility. It is then the Contractor's responsibility to account in the bid for any non typical construction technique necessary to complete the project based on the above findings.

4. It shall be the Contractor's responsibility to verify that no utility conflicts exist prior to ordering precast units. Any units requiring replacement due to utility conflicts shall be done so at the Contractor's expense and at no additional expense to the County.

5. It shall be the Contractor's responsibility, regardless of specific direction or no direction from the Engineer, to verify that there will not be any utility conflicts prior to the commencement of drainage pipe installation. In addition to direct conflicts, it is also the Contractor's responsibility to verify if appropriate clearances will be provided between utility and drainage pipe or structure. There shall be no additional cost to the County as a result of the Contractor not performing the necessary field investigations.

6. It shall be the Contractor's responsibility to verify the invert elevation, pipe sizes, number of pipe penetrations, and angle of penetration of any existing drainage directly connecting to precast units prior to ordering the units.
Any units requiring replacement due to invert elevation; pipe sizes or other inconsistencies shall be done so at the Contractor's expense and at no additional cost to the County.

7. Existing pipe which is to remain in areas of excavation may be under conditions of minimal cover. Care should be taken not to damage existing pipe and structures in areas where they are to remain. Existing pipe and structures in areas where they are to remain, damaged during excavation or construction, shall be repaired or replaced as instructed by the Engineer, at the expense of the Contractor and at no additional cost to the County.

8. The Contractor shall be responsible for all aspects of construction dewatering if required to render and maintain excavations in a dewatered and hydrostatically relieved condition. The Contractor shall design, furnish, install, operate, monitor, maintain and remove the dewatering system. Prior to beginning any work on the dewatering system, the Contractor shall submit a detailed dewatering plan, signed and sealed by a N.J. Professional Engineer, to the Ocean County Engineering Department and the Ocean County Soil Conservation District for approval. The Contractor shall be responsible for obtaining all approvals and permits necessary for the temporary dewatering. The cost of designing, furnishing, installing, operating, monitoring, maintaining and removing a dewatering system and any materials, equipment and labor necessary for and incidental to shall be included in the price bid for the various items requiring such work. Any temporary dewatering required for the test pits shall be the responsibility of the Contractor and shall be paid for under the price bid for the various items requiring such work.

9. The edges of any area of excavation, adjacent to or crossing, any paved or concrete surface, shall be saw cut to the full depth of the paving or concrete. Sawcutting will not be measured separately. The cost of this work shall be included in the price bid for the various pay items requiring sawcutting.

10. At the Preconstruction Conference or prior to the start of the applicable construction, the Contractor shall submit for acceptance his schedules for accomplishment of temporary and permanent erosion control work, as are applicable for clearing and grubbing; grading; construction; and paving. He shall also submit for acceptance his proposed method of erosion control on haul roads and borrow pits and his plan for disposal of waste materials. No work shall be started until the erosion control schedules and methods of operations have been accepted by the Engineer. Various temporary soil erosion and sediment control devices will not be measured for payment. Separate payment will not be made for temporary soil erosion and sediment control devices but the cost will be included in the various items in the proposal.

11. Using methods and practices, set forth by the New Jersey State Soil Conservation Committee's Standards for Soil Erosion and Sediment Control in New Jersey, the Contractor shall maintain temporary stabilization, dust control, daily road cleaning sweeping, daily road opening stabilization and seeding.

12. The item of reset heads shall include raising or lowering the head castings of inlets and catch basins, or the raising of manhole head castings, for a maximum of 12". All other changes in position of head castings shall be considered as reconstructed manholes or reconstructed inlets or catch basins.

13. When curb piece height specified is greater than curb face height, depress the gutter of grate so that the top of the curb piece is at the same elevation as the top of curb (see detail).

14. There is not any implied assurance that all utility valve boxes or utility manholes have been indicated on the plans.

15. Relocation/resetting of utility valve boxes or utility manholes whether indicated on the plan or not, shall be relocated at no cost to the County of Ocean.

16. Construction practices shall be in accordance with the New Jersey Department of Transportation 2019 Standard Specifications for Road and Bridge Construction unless modified by the Supplementary Specifications specific to this construction contract.

17. It shall be the Contractor's responsibility, regardless of specific direction or no direction from the Engineer, to verify that there will not be any utility conflicts prior to the commencement of guide rail installation. In addition to direct conflicts, it is also the Contractor's responsibility to verify if appropriate clearances will be provided between utility and guide rail. All costs associated with utility conflict investigations including but not limited to test pits shall be included in the unit prices bid for the various guide rail pay items.

18. Existing Pylons to be modified under a separate contract.

SURVEY NOTES:

1. This survey was prepared without the benefit of a title report and is subject to any easement, restriction and/ or covenants that a current report of title, or complete search of the public record, may disclose.

2. This survey was prepared in accordance with the following sources of information:

   - An actual field survey preformed by Pennoni Associates

3. Planimetric & topographic features shown are taken from an actual field survey preformed by Pennoni Associates on 11/7/24.

4. This survey is not intended to guarantee ownership.

5. Survey Based on N.J.S.P.C.S. NAD '83 (2011). Vertical Datum: NAVD 1988

6. Right of Way was determined from following sources of information:

   - 1996 As-Built Plans of County Line Road provided by Ocean County.

   - Township of Jackson, Ocean County, New Jersey Deeds for Lot 60 & 61 Block 5601

   - Township of Jackson, Ocean County, New Jersey Subdivision Map for:
     - Lots 18.03, 22, 23, & 24.03 Block 128.01 - 1988
     - Lots 18, & 18B Block 117 - 1977
     - South Knolls Planned Retirement Community - 2000

   - Official tax maps of the Township of Jackson, Ocean County, New Jersey, Plates 21, 43, 46-53, 56, 65, 83, & 84

7. It is beyond the scope of this survey to determine the existence or non-existence of any regulated environmental condition on or near the subject parcel(s). Underground explorations were not conducted or utilized during the preparation of this survey. Concerned parties should pursue any environmental matters separate and apart from this survey.

8. It should not be assumed that any copy of this drawing without a raised impression of the surveyor's seal is a true copy of the original as issued by the surveyor.

9. The location of underground utilities are approximate. All locations illustrated are based upon above ground structures that were visible & accessible in the field. The State of New Jersey requires notification prior to any excavation by utilizing the New Jersey One-Call System (1-800-272-1000).

DATE	REVISIONS	BY	CHK

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

STATE OF NEW JERSEY · BETH-ANN M. GRASSO · No. GE 43121 · LICENSED · PROFESSIONAL ENGINEER

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

RECONSTRUCTION OF

NORTH COUNTY LINE RD

FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

GENERAL NOTES

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE	DRAWN C.D.	APPROVED B.M.G.
FILE NO.	12 - 009 - 0128		SHEET 7 OF 84

GN02

OCEAN COUNTY ENGINEERING DEPARTMENT

SOIL CONSERVATION DISTRICT NOTES

CONSTRUCTION MATERIALS

A. The Contractor shall test the soil and incorporate limestone, as needed, with the fertilizer per The Standards for Soil Erosion and Sediment Control in N.J. 7th Edition, January 2014, revised July 2017.
   1. Grade as needed and feasible to permit the use of conventional equipment for seedbed preparation, seeding, mulch application, and mulch anchoring. All grading should be done in accordance with Standard for Land Grading.
   2. Immediately prior to seeding and topsoil application, the subsoil shall be evaluated for compaction in accordance with the Standard for Land Grading.
   3. Install needed erosion control practices or facilities such as diversions, grade-stabilization structures, channel stabilization measures, sediment basins, and waterways.
   4. For permanent vegetative cover, apply topsoil to a uniform depth of 5 inches (unsettled). Topsoil should be handled only when it is dry enough to work without damaging soil structure. Topsoil shall be amended with organic matter, as needed, in accordance with the Standard for Topsoiling.
   5. The Contractor shall test the soil and incorporate limestone, as needed, with the fertilizer per the Standards for Soil Erosion and Sediment Control N.J. 7th Edition, January 2014, revised July 2017.
      a. Fertilizer, 10-10-10, apply at a rate of 11 lbs/1000 square feet or 500 lbs/acre for permanent stabilization.
      b. Fertilizer, 10-20-10, apply at a rate of 11 lbs/100 square feet or 500 lbs/acre for temporary stabilization. See "SCD 20 Notes" Note #6, for Mulch Only Requirements.
   6. Work lime and fertilizer into the topsoil as nearly as practical to a depth of 4 inches with disc, spring-tooth harrow, or other suitable equipment. The final operations should be on the general contour and continued until a uniform seedbed is prepared.
   7. Inspect site just before seeding. If traffic has left the soil compacted, the area must be retilled and firmed in accordance with the Standards for Soil Erosion and Sediment Control in New Jersey.
   8. (Conventional Seeding) Apply seed uniformly by hand, cyclone (centrifugal) seeder, drop seeder, drill or cultipacker seeder. Except for drilled, hydroseeded or cultipacked seedings, seed shall be incorporated into the soil, to a depth of ¼ to ½ inch, by raking or dragging. Depth of seed placement may by ¼ inch deeper on coarse textured soil.
   9. After seeding, liming the soul with a corrugated roller will assure good seed-to-soil contact, restore capillary, and improve seeding emergence. This is the preferred method. When performed on the contour, sheet erosion will be minimized and water conservation on site will be maximized.

B. Straw mulch shall be uniformly spread at a rate of 70 to 90 lbs/1,000 sq. ft. for both permanent and temporary stabilization. Mulch is applied at the rate of 1 to 2 Tons per Acre.

C. Mulching Anchoring - for both permanent and temporary stabilization - should be accomplished immediately after placement of hay or straw mulch to minimize loss by wind and water. This may be done by one of the following methods, depending upon the size of the area and steepness of slopes.
   1. Peg and Twine - Drive 8 to 10 inch wooden pegs to within 2 to 3 inches of the soil surface every 4 feet in all directions. Stakes may be driven before or after applying mulch. Secure mulch to soil surface by stretching twine between pegs in a crisscross and a square pattern. Secure twine around each peg with two or more round turns.
   2. Mulch Nettings - Staple paper, cotton or plastic netting over mulch. Use a degradable netting in areas to be mowed. Netting is usually available in rolls 4 feet wide and up to 300 feet long.
   3. Crimper Mulch Anchoring Coulter Tool - A tractor -drawn implement especially designed to punch and anchor mulch into the soil surface. This practice affords maximum erosion control, but its use is limited to those slopes upon which the tractor can operate safely. Soil penetration should be about 3 to 4 inches. On sloping land, the operation should be on the contour.
   4. Liquid Mulch-Binders
      a. Applications should be heavier at edges where wind catches the mulch, in valleys and at crests of banks. Remainder of area should be uniform in appearance.
      b. Use one of the following:
      1. Organic and Vegetable Based Binders - Naturally occurring, powder based, hydrophilic materials that mixed with water formulates a gel and when applied to mulch under satisfactory curing conditions will form membraned networks of insoluble polymers. The vegetable gel shall be physiologically harmless and not result in a phytotoxic effect or impede growth of turf grass. Vegetable based gels shall be applied at rates and weather conditions recommended by the manufacturer.
      2. Synthetic Binders - High polymer synthetic emulsion, miscible with water when diluted and following application to mulch, drying and curing shall no longer be soluble or dispersible in water. It shall be applied at rate and weather conditions recommended by the manufacturer and remain tacky until germination of grass.
      c. If construction occurs during the non-growing season, refer to the mulch only Note #6 of the Construction Notes.

D. TYPE A-5 SEED MIXTURE, apply at a rate of 305 lbs./acre.

KIND OF SEED	POUNDS PER ACRE	POUNDS PER 1,000 SQ. FT.
Tall Fescue	265	6
KY Bluegrass (blend)	20	0.5
Perennial Ryegrass (blend)	20	0.5

Rye or Oat grain shall be sown with Type A-5 Seed Mixture at the rate of 10 lbs. per acre.
Recommended seeding period for Zone 6b is 8/15 - 10/15 and for Zones 7a and 7b is 8/15 - 10/30.
For additional dates and seed mixtures refer to section 4 of "The Standards for Soil Erosion and Sediment Control in New Jersey, January 2014 7th Edition, revised February 2017".

E. Temporary seeding mix to be perennial rye grass. Application rate shall be 1.0 pound/1000 square feet or 100 pounds/acre. Irrigation is provided to ensure successful germination.
   a.) Temporary Seeding Rates

TEMPORARY VEGETATIVE STABILIZATION GRASSES, SEEDING RATES, DATES AND DEPTH						
SEED SELECTIONS	SEEDING RATE (pounds)		OPTIMUM SEEDING DATE Based on Plant Hardiness Zone			OPTIMUM SEED DEPTH (inches)
	Per Acre	Per 1000 Sq. Ft.	ZONE 5b, 6s	ZONE 6b	ZONE 7a, b	
COOL SEASON GRASSES						
1. Perennial Ryegrass	100	1.0	3/15-6/1 8/1-9/15	3/1-5/15 8/15-10/1	2/15-5/1 8/15-10/15	0.5 1" IF SOILS ARE SANDY

F. Mulching is required on all seeding. Spread mulch uniformly by hand or mechanically at a rate of 70 to 90 lbs/1000 square feet for both permanent and temporary stabilization. Unrotted small grain straw, hay free of seeds, applied at the rate of 1-1/2 to 2 tons per acre except that where a crimper is used instead of liquid mulch-binder (lackillying or adhesive agent), the rate of application is 3 tons/acre. Mulch chopper-blowers must not grind the mulch. Hay mulch is not recommended for establishing fine turf or lawns due to the presence of weed seeds.

G. Stone for Wire-enclosed Riprap. Wire-enclosed riprap consists of mats or baskets fabricated from wire mesh, filled with 4" stone, connected together and anchored to the ground. A plastic filter cloth shall be placed on the prepared area where gabions are in contact with soil. Stone used for wire-enclosed riprap shall meet the requirements of dumped riprap except for size and gradation of stone. Stone used shall be well graded within the sizes available and 70 percent, by weight, and smallest stone shall exceed the wire mesh opening. The maximum size of stone, measured normal to the slope, shall not exceed the mat thickness.

H. Wire for Wire-enclosed Riprap. All wire gauges are subject to a tolerance of +/- 2.50% of the manufacturer's stated specifications, the thickness of P.V.C. coating excluded.

I. Geotextile roadway underlayment shall be woven or non-woven polypropylene or polyester fabric (Bidim C28, Mirafi 140, Supac 5-P, Staff M-1195 Permealiner or equivalent) meeting the following specifications: A.S.T.M.D. 1910, A.S.T.M.D. 1777, A.S.T.M.D. 1682, A.S.T.M.D. 2263 and A.S.T.M.D. 774-46 and having a minimum grab tensile strength of 130 pounds and a water permeability rate of at least 0.02 cm/sec.

J. A 5" Average Depth of Topsoil is Required, Firmed in Place.

K. Established Permanent Vegetation Means 80% Vegetative Coverage with the Specified Mixture for Seeded Area and Mowed Once.

SOIL COMPACTION MITIGATION NOTES

1. Procedures shall be used to mitigate excessive soil compaction prior to placement of topsoil and establishment of permanent vegetative cover.
2. Restoration of compacted soils shall be through deep scarification/tillage (6" minimum depth) where there is no danger to underground utilities (cables, irrigation systems, etc.). In the alternative, another method as specified by a New Jersey Licensed Professional Engineer may be substituted subject to District approval.
3. Soil compaction testing is not required if/when subsoil compaction remediation (scarification/tillage 6" minimum depth) is proposed as part of the sequence of construction.

TOPSOILING NOTES

1. Topsoil should be handled only when it is dry enough to work without damaging soil structure.
2. A uniform application to an average depth of 5" (minimum 4") firmed in place is required.
3. Pursuant to the requirements in Section 7 of the Standard for Permanent Vegetative Stabilization, the contractor is responsible to ensure that permanent vegetative cover becomes established on at least 80% of the soils to be stabilized with vegetation. Failure to achieve the minimum coverage may require additional work to be performed.
4. Topsoil shall be amended with organic matter, as needed, in accordance with Standards for Topsoiling.

SCD 20 NOTES

1. The Ocean County Soil Conservation District shall be notified forty-eight (48) hours in advance of any land disturbance.
2. All work is to be done in accordance with the State Standards for Soil Erosion and Sediment control in New Jersey.
3. All Soil Erosion and Sediment Control practices are to be installed prior to any major soil disturbance, or in their proper sequence, and maintained until permanent protection is established.
4. Any changes to the Certified Soil Erosion and Sediment Control Plans will require the submission of revised Soil Erosion and Sediment Control Plans to the District. The revised plans must meet all current "The Standards for Soil Erosion and Sediment Control in New Jersey, 7th Edition, January 2014, Revised July 2017. Link to 2014 Standards: http://www.state.nj.us/agriculture/divisions/anr/nrc/njerosion.html.
5. N.J.S.A. 4:24-39 et. seq. requires that no Certificates of Occupancy be issued before there has been compliance with provisions of a certified plan for permanent measures. All site work, and all work around individual lots in subdivisions, must be completed prior to the District issuing a Report of Compliance for the issuance of a Certificate of Occupancy by the Municipality.
6. Any disturbed area that will be left exposed for more than sixty (60) days, and not subject to construction traffic, will immediately receive a temporary seeding. If the season prevents the establishment of temporary cover, the disturbed areas will be mulched with straw, or equivalent material, within 14 days at a rate of 2 to 2 tons per acre, according to State Standard for Stabilization with Mulch Only.
7. Immediately following initial disturbance or rough grading, all critical areas subject to erosion (i.e. steep slopes and roadway embankments) will receive temporary seeding in combination with straw mulch or a suitable equivalent, at a rate of 1 to 2 tons per acre, according to State Standards.
8. A sub-base course will be applied immediately following rough grading and installation of improvements to stabilize streets, roads, driveways, and parking areas. In areas where no utilities are present, the sub-base shall be installed within fifteen (15) days of the preliminary grading.
9. Any steep slopes (3:1 or greater) or any existing roadways receiving pipeline installation will be backfilled and stabilized daily, as the installation continues.
10. The Standard for Stabilized Construction Access requires the installation of a stone pad using clean crushed angular stone (ASTM C-33, size No. 2 or 3) at all construction driveways where vehicles will access paved roadways from unpaved areas of the site.
11. All sediment washed, dropped, spilled, or tracked onto roadways (public or private) or other impervious surfaces will be removed immediately.
12. Permanent vegetation is to be seeded or sodded on all exposed areas within ten (10) days after final grading. At the time of the final inspection, the Contractor shall be required to provide confirmation that the proper type and amount of seed, lime, and fertilizer have been used for permanent stabilization work. Straw mulch is required on all seeding.
13. At the time that site preparation for permanent vegetative stabilization is going to be accomplished, any soil that will not provide a suitable environment to support adequate vegetative ground cover shall be removed or treated in such a way that it will permanently adjust the soil conditions and render it suitable for vegetative ground cover. If the removal or treatment of the soil will not provide suitable conditions, non-vegetative means of permanent ground stabilization will have to be employed.
14. In accordance with the Standard for Management of High Acid Producing Soils, any soil having a pH of 4 or less or containing iron sulfides shall be covered with a minimum of twelve (12) inches of soil having a Ph of 5 or more prior to seedbed preparation. Areas where trees or shrubs are to be planted shall be covered with a minimum of twenty-four (24) inches of soil having a pH of 5 or more.
15. Conduit Outlet Protection must be installed at all required outfalls prior to the drainage system becoming operational. Conduit outlet protection is not required in basins acting as sediment basins during construction.
16. Unfiltered dewatering is not permitted. Necessary precautions must be taken during all dewatering operations to minimize sediment transfer. Any dewatering methods used must be in accordance with the Standards for Dewatering.
17. Should the control of dust at the site be necessary, the site will be sprinkled until the surface is wet, temporary vegetative cover shall be established or mulch shall be applied as required by the Standard for Dust Control.
18. Stockpile and staging locations established in the field shall be placed within the limit of disturbance according to the certified plan. Staging and stockpiles not located within the limit of disturbance will require certification of a revised Soil Erosion and Sediment Control Plan. The District reserves the right to determine when certification of a new and separate Soil Erosion and Sediment Control Plan will be required for these activities.
19. All soil stockpiles are to be temporarily stabilized in accordance with Soil Erosion and Sediment Control note #6. Stockpiles should be situated so as to not obstruct natural drainage or cause off-site environmental damage.
20. The Contractor shall be responsible for any erosion or sedimentation that may occur below stormwater outfalls or offsite as a result of construction of the project.

CONSTRUCTION NOTES

1. During the construction contract period, the Contractor, under the direction of the Ocean County Engineer, shall be responsible for the construction and maintenance of temporary and permanent soil erosion and sedimentation control devices, as required by the Standards for Soil Erosion and Sediment Control in N.J. 7th Edition, January 2014, revised July 2017.
2. All disturbed areas resulting from soil stockpiles and construction yards shall be permanently stabilized prior to completing the project. The contractor shall comply with all soil conservation district standards.
3. Immediately prior to topsoiling, the surface should be scarified 6" to 12" where there has been soil compaction. This will help ensure a good bond between the topsoil and subsoil.
4. The dry method (conventional seeding) and not the hydraulic method (hydroseeding) shall be the ONLY acceptable method to place seed and fertilizer due to the sandy nature of the subsoils in this project.
5. Inspect site just before seeding. If traffic has left the soil compacted, the area must be retilled and firmed in accordance with the Standards for Soil Erosion and Sediment Control in New Jersey.
6. All seed must be drilled or raked to a depth of " to " below the surface of the soil. Depth of seed placement may be 1/4" deeper on course-textured soil.
7. To prevent compaction of the stormwater basins subsoil which will reduce its infiltration capacity, basins should be excavated with light earth moving equipment, preferably with tracks or over-sized tires rather than the normal rubber tires. Once the final construction phase is reached, the floor of the basin shall be deeply tilled with a rotary tiller or disc harrow and smoothed over with a leveling drag or equivalent grading equipment.
8. Basin must be properly constructed and permanently stabilized, and conduit outlet protection installed, prior to the drainage system becoming operational.
9. The Standards for Soil Erosion and Sediment Control in New Jersey have specific requirements for topsoiling, the installation of sod, temporary and/or permanent vegetative cover, land grading, and construction of infiltration structures. The text found on pages 4-2 (sec. 2 C), 6-2 (Sec. 2 D), 7-1 (sec 1 C), 8-2 (sec. 3 D), 19-2 (last paragraph), 38-8 (4th paragraph), and 38-12 (1st paragraph) serve to help minimize soil compaction and reduce maintenance needs.
10. Additional measures will be required if erosion develops.
11. The Contractor shall be responsible for placing silt fence in areas that may be subject to sediment build up from construction, i.e. along swales, streams, wetlands, open bodies of water, etc. in accordance with the State Standards for Soil Erosion and Sediment Control in New Jersey.
12. If acid producing soils are encountered during construction, refer to Standard for Management of High Acid-Producing Soils Chapter 1-1, of the Standards for Soil Erosion and Sediment Control in N.J. 7th Edition, January 2014, revised July 2017.
13. The Ocean County Soil Conservation District may require additional soil erosion measures to be installed as directed by the district inspector, in accordance with the State Standards for Soil Erosion and Sediment Control in New Jersey.

DUST CONTROL NOTES

Definition: the control of dust on construction sites and roads.

Purpose: to prevent blowing and movement of dust from exposed soil surfaces, reduced on-site and off-site damage and health hazards, and improve traffic safety.

Condition where practice applies: this practice is applicable to areas subject to dust blowing and movement where on-site and off-site damage is likely without treatment. consult with local municipal ordinances on any restrictions.

Water quality enhancement: sediments deposited as "dust" are often fine colloidal material which is extremely difficult to remove from water once it becomes suspended. use of this standard will help to control the generation of dust from construction sites and subsequent blowing and deposition into local surface water resources.

Planning criteria:
the following methods should be considered for controlling dust:

Mulches: see the Current Standards for Soil Erosion and Sediment Control in New Jersey (standards) for stabilization with mulches only, pg. 5-1.

Vegetative cover: see Standards for Temporary Vegetative Cover, Pg. 7-1, Permanant Vegetative Cover for Soil Stabilization, Pg. 4-1, and Permanent Stabilization with Sod, Pg. 6-1.

Spray on adhesives: on mineral soils (not effective on muck soils). keep traffic off these areas dust.

	ITEM	CONSTRUCTION SEQUENCE	DURATION (Working Days)
1.	INSTALLATION OF TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES MUST BE INSTALLED AT THE INITIATION OF LAND DISTURBANCE ACTIVITIES. ALL TEMPORARY SOIL EROSION MEASURES MUST BE INCLUDED.		2 Days
2.	SET UP TRAFFIC CONTROL DEVICES.		7 Days
3.	CLEAR SITE & PERFORM TEST PITS.		5 Days
4.	CONSTRUCT TRAFFIC SIGNAL(S) AND ACTIVATE.		30 Days
5.	REMOVE EXISTING TRAFFIC SIGNAL(S).		10 Days
6.	REMOVE/INSTALL GRADING CURB/SIDEWALK, AND ROADWAY CONSTRUCTION.		14 Days
7.	FINAL MILL/PAVE, INSTALL SIGNING AND STRIPING.		10 Days
8.	APPLY TOP SOIL TO AN AVERAGE DEPTH OF 5" (4" MINIMUM) LIMITED IN PLACE (SEE STANDARD "TOPSOILING" PG 8-2).		8 Days
9.	INSTALL TEMPORARY AND/OR PERMANENT SEEDING, FERTILIZER, LIME, AND MULCH.		5 Days
10.	REMOVAL OF TRAFFIC CONTROL DEVICES.		4 Days
11.	REMOVAL OF SEDIMENT CONTROL DEVICES UPON PERMANENT STABILITY OF DISTURBED SURFACES.		2 Days

NOTE:

1. SEDIMENT CONTROL DEVICES AND TEMPORARY GROUND COVER SHALL BE MAINTAINED THROUGHOUT THE PROJECT DURING CONSTRUCTION. THESE CONTROL DEVICES SHALL BE REMOVED ONLY WHEN UPSTREAM CONTRIBUTING CONSTRUCTION AREA HAVE BEEN STABILIZED.
2. DAILY SWEEPING AND STREET MAINTENANCE/DAILY BACKFILLING OF TRENCHES WITH IMMEDIATE STABILIZATION SHALL BE PERFORMED.

VEGETATION MAINTENANCE NOTES

1. Mowing is a recurring practice and its intensity depends upon the function of the ground cover. On high to moderate (A to B) maintenance areas, such as lawns, certain recreation fields, and picnic areas, mowing will be frequent (2 to 7 day intervals) and typically at a height of 2.5 to 3 inches. Return clippings from mowing (mulching mower) to the turf to reduce the amount of fertilizer needed to maintain the turf by as much as 50%. Some turf mixtures can be managed as naturalized stands requiring only one (cool season mixtures) or two (warm season mixtures) mowings per year. Mowing of naturalized areas is typically done at heights no less than 4 inches and should not be done between April 1st and July 15th to avoid disturbing ground nesting birds. The large amount of clipping debris generated by mowing naturalized areas will need to be removed and/or dispersed so the vegetation is not smothered. Burning of naturalized areas is another procedure used to manage naturalized turfs. Low maintenance (D) areas may be left unmowed to permit natural succession. See pg. 4-13 footnote #4, Maintenance Levels A, B, C and D in the Standard for Permanent Vegetative Cover, Table 4-3.
2. Incorporation of organic matter (for example, mature compost) into the soil will substantially reduce the need for fertilizer and irrigation inputs.
3. Fertilizer and lime should be applied as needed to maintain a dense stand of desirable species. Frequently mowed areas and those on sandy soils will require more frequent fertilization but at lower nutrient rates per application.
4. Lime requirement should be determined by soil testing every 2 or 3 years. Fertilization may increase the need for liming. Contact the local county extension office for details on soil testing and fertilization and pest control recommendations online at http://njaes.rutgers.edu/county/.
5. Fertilization and additions of other soil amendments are not recommended for managing native vegetation such as in the Pinelands National Reserve. See the Standard for Permanent Vegetative Stabilization for specific requirements in the PNR
6. Weed invasion may result from abusive mowing and from inadequate fertilizing and liming. Many newly established grasses will not survive if mowed at heights below 2.5 inches and at intervals greater than 7 days. Brush invasion is a common consequence of lack of mowing. The amount of weeds or brush that can be tolerated in any vegetated area depends upon the intended use of the land. Drainage ways are subject to rapid infestation by weed and woody plants. These should be controlled, since they often reduce drainage way efficiency. Control of weeds or brush is accomplished by using herbicides or mechanical methods.
7. Fire hazard is greater where dry vegetation has accumulated. The taller the vegetation, the greater the hazard.
8. Prune trees and shrubs to remove dead or damaged branches. Remove undesirable or invasive plants to maintain integrity of the landscape and enhance quality of permanent vegetative cover.

2014 Errata

1. Pages 3-1, 4-2 (3x), 4-4, 4-5 and 4-11 refer to "Table 4-3" which should be "Table 4-2".
2. Page 4-6, Title of table is missing. It should be "Table 4-3"/
3. Page 6-1 item 6.1B shows a topsoil depth of 6" required. This should be consistent with topsoil depth of 5" recommended as noted in the Standard of Topsoiling, 1999.
4. Page 6-2. Table 6-1 should be ignored and liming rates should be established via soil testing.
5. Page 7-1, 2.A. Liming rates shall be established via soil testing. The default rate given in Section 2A, 4th sentence regarding liming a 2 TPA should be ignored.
6. Page 12-2 The equation for apron length for tailwater conditions greater than ½Do shoes an extra Do term in the numerator on the original release of the 2014 Standards. It has been corrected as of 3/4/2014.
7. Page 26-4 Engineering-Grading Section. Slope Protection Standards should be page 25-1 not 27-1.
8. Page 27-3 Figure 27-1 refers to table 29-1 when it should refer to table 27-1.

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

STATE OF NEW JERSEY — BETH-ANN M. GRASSO — No. GE 43121 — LICENSED PROFESSIONAL ENGINEER — GN02

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

S.E.&S.C. NOTES

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE NOT TO SCALE	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 8 OF 84

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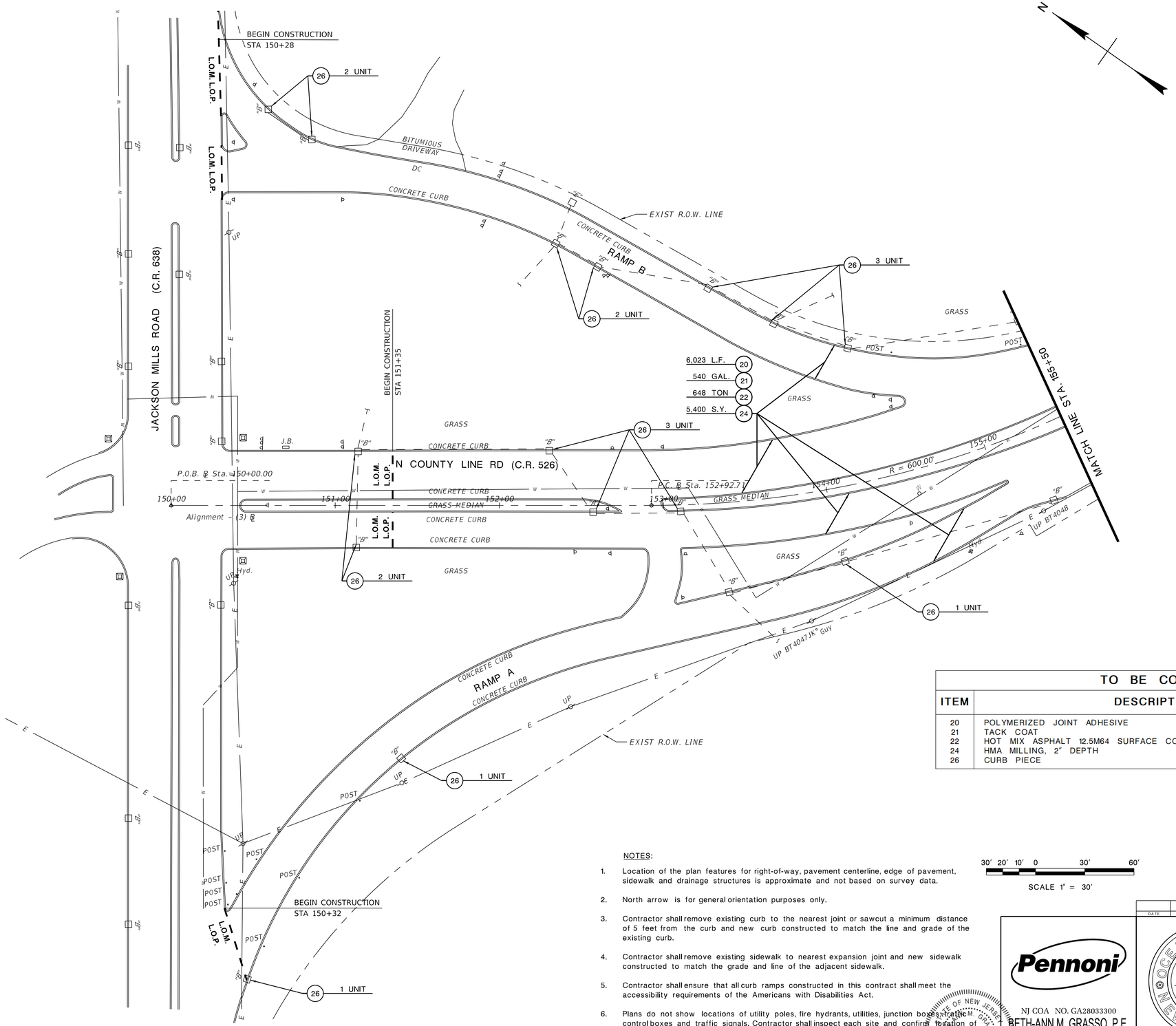

TO BE CONSTRUCTED

ITEM	DESCRIPTION	QUANTITIES	
20	POLYMERIZED JOINT ADHESIVE	6,023	L.F.
21	TACK COAT	540	GAL.
22	HOT MIX ASPHALT 12.5M64 SURFACE COURSE, 2" THICK	648	TON
24	HMA MILLING, 2" DEPTH	5,400	S.Y.
26	CURB PIECE	15	UN.

NOTE:

1. RIGHT OF WAY PLOTTED FROM RECORD PLANS AND TAX MAPS.

NOTES:

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6. Plans do not show locations of utility poles, fire hydrants, utilities, junction boxes, traffic control boxes and traffic signals. Contractor shall inspect each site and confirm location of possible obstructions to ADA curb ramp and sidewalks.
7. Paving limit is approximate. Paving limit shall be field verified to match existing pavement joint.

30' 20' 10' 0 30' 60'

SCALE 1" = 30'

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

STATE OF NEW JERSEY BETH-ANN M. GRASSO No. GE 43121 PROFESSIONAL ENGINEER

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

CONSTRUCTION PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE 1"=30'	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 9 OF 84

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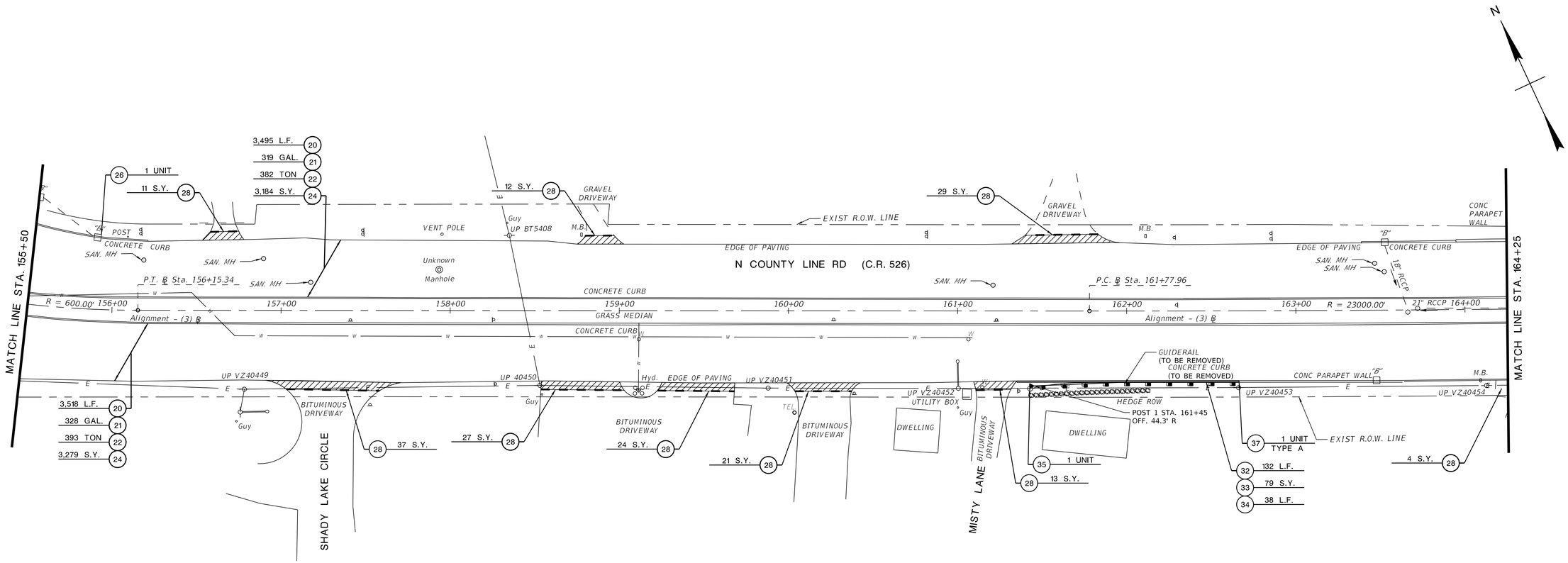

TO BE CONSTRUCTED			
ITEM	DESCRIPTION	QUANTITIES	
20	POLYMERIZED JOINT ADHESIVE	7,013	L.F.
21	TACK COAT	647	GAL.
22	HOT MIX ASPHALT 12.5M64 SURFACE COURSE, 2" THICK	775	TON
24	HMA MILLING, 2" DEPTH	6,463	S.Y.
26	CURB PIECE	1	UN.
28	HOT MIX ASPHALT DRIVEWAY, 3" THICK	178	S.Y.
32	9" X 14" CONCRETE VERTICAL CURB	132	L.F.
33	NONVEGETATIVE SURFACE, HOT MIX ASPHALT	79	S.Y.
34	BEAM GUIDE RAIL	38	L.F.
35	TANGENT GUIDE RAIL TERMINAL	1	UN.
37	APPROACH GUIDE RAIL TRANSITION TL-3	1	UN.

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SCALE 1" = 30'

STATE OF NEW JERSEY
BETH-ANN M. GRASSO
GE 43121
LICENSED PROFESSIONAL ENGINEER

C18

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

DATE	REVISIONS	BY	CHK.

RECONSTRUCTION OF

NORTH COUNTY LINE RD

FROM JACKSON MILLS RD TO BENNETTS MILLS RD

JACKSON TOWNSHIP

CONSTRUCTION PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE 1"=30'	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 10 OF 84

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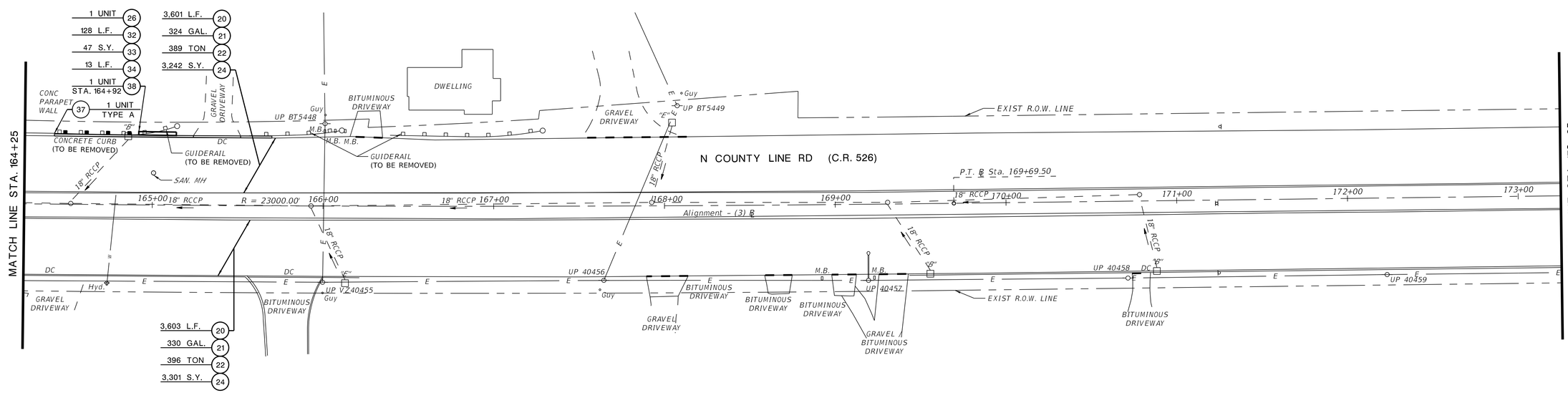

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TO BE CONSTRUCTED

ITEM	DESCRIPTION	QUANTITIES	
20	POLYMERIZED JOINT ADHESIVE	7,204	L.F.
21	TACK COAT	654	GAL.
22	HOT MIX ASPHALT 12.5M64 SURFACE COURSE, 2" THICK	785	TON
24	HMA MILLING, 2" DEPTH	6,543	S.Y.
26	CURB PIECE	1	UN.
32	9" X 14" CONCRETE VERTICAL CURB	128	L.F.
33	NONVEGETATIVE SURFACE, HOT MIX ASPHALT	47	S.Y.
34	BEAM GUIDE RAIL	13	L.F.
37	APPROACH GUIDE RAIL TRANSITION TL-3	1	UN.
38	CRASH CUSHION, COMPRESSIVE BARRIER, TYPE 3, WIDTH NARROW	1	UN.

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

STATE OF NEW JERSEY BETH-ANN M. GRASSO No. GE 43121 LICENSED PROFESSIONAL ENGINEER

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

CONSTRUCTION PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE 1"=30'	DRAWN C.D.	APPROVED B.M.G.
FILE NO.	12 - 009 - 0128		SHEET 11 OF 84

C18

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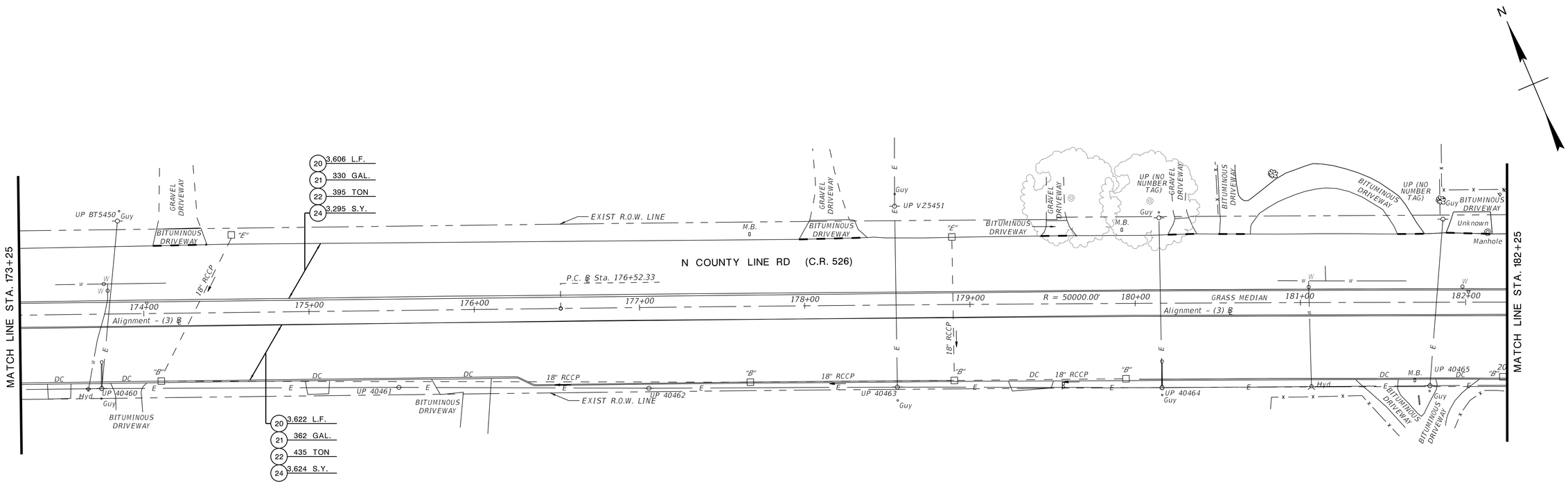

TO BE CONSTRUCTED		
ITEM	DESCRIPTION	QUANTITIES
20	POLYMERIZED JOINT ADHESIVE	7,228 L.F.
21	TACK COAT	692 GAL.
22	HOT MIX ASPHALT 12.5M64 SURFACE COURSE, 2" THICK	830 TON
24	HMA MILLING, 2" DEPTH	6,919 S.Y.

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NOTE:

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STATE OF NEW JERSEY
BETH-ANN M. GRASSO
No. GE 43121
LICENSED PROFESSIONAL ENGINEER

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

4/28/2026

PROFESSIONAL ENGINEER
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OCEAN COUNTY NEW JERSEY

DATE	REVISIONS	BY	CHK.

RECONSTRUCTION OF

NORTH COUNTY LINE RD

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TO BENNETTS MILLS RD

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CONSTRUCTION PLAN

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FILE NO.	12 - 009 - 0128		SHEET 12 OF 84

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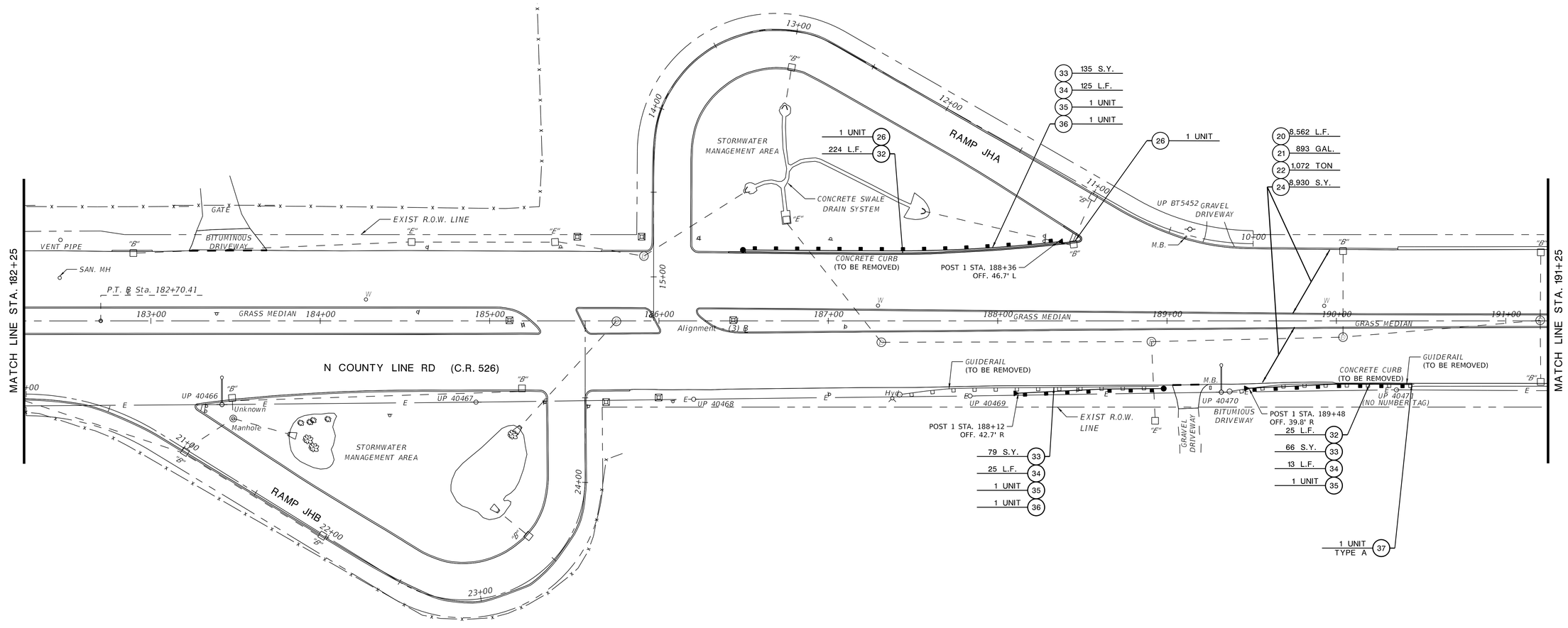

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30' 20' 10' 0 30' 60'

SCALE 1" = 30'

TO BE CONSTRUCTED

ITEM	DESCRIPTION	QUANTITIES	
20	POLYMERIZED JOINT ADHESIVE	8,562	L.F.
21	TACK COAT	893	GAL.
22	HOT MIX ASPHALT 12.5M64 SURFACE COURSE, 2" THICK	1,072	TON
24	HMA MILLING, 2" DEPTH	8,930	S.Y.
26	CURB PIECE	2	UN.
32	9" X 14" CONCRETE VERTICAL CURB	249	L.F.
33	NONVEGETATIVE SURFACE, HOT MIX ASPHALT	280	S.Y.
34	BEAM GUIDE RAIL	163	L.F.
35	TANGENT GUIDE RAIL TERMINAL	3	UN.
36	BEAM GUIDE RAIL ANCHORAGE	2	UN.
37	APPROACH GUIDE RAIL TRANSITION TL-3	1	UN.

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

STATE OF NEW JERSEY BETH-ANN M. GRASSO No. GE 43121 LICENSED PROFESSIONAL ENGINEER

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

C18

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

CONSTRUCTION PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE 1"=30'	DRAWN C.D.	APPROVED B.M.G.
FILE NO.	12 - 009 - 0128		SHEET 13 OF 84

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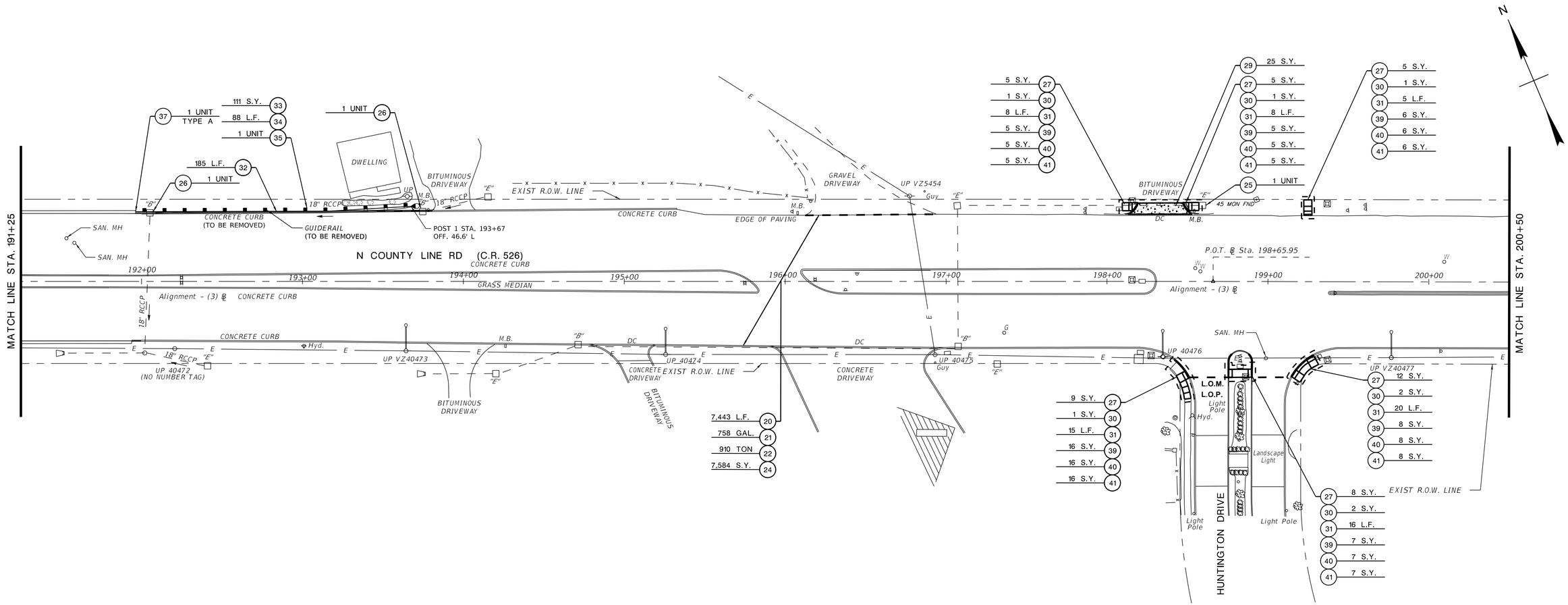

TO BE CONSTRUCTED			
ITEM	DESCRIPTION	QUANTITIES	
20	POLYMERIZED JOINT ADHESIVE	7,443	L.F.
21	TACK COAT	758	GAL.
22	HOT MIX ASPHALT 12.5M64 SURFACE COURSE, 2" THICK	910	TON
24	HMA MILLING, 2" DEPTH	7,584	S.Y.
25	RESET EXISTING CASTING	1	UN.
26	CURB PIECE	2	UN.
27	CONCRETE SIDEWALK, 4" THICK	44	S.Y.
29	CONCRETE DRIVEWAY, 6" THICK	25	S.Y.
30	DETECTABLE WARNING SURFACE	8	S.Y.
31	9" X 18" CONCRETE VERTICAL CURB	72	L.F.
32	9" X 14" CONCRETE VERTICAL CURB	185	L.F.
33	NONVEGETATIVE SURFACE, HOT MIX ASPHALT	111	S.Y.
34	BEAM GUIDE RAIL	88	L.F.
35	TANGENT GUIDE RAIL TERMINAL	1	UN.
37	APPROACH GUIDE RAIL TRANSITION TL-3	1	UN.
39	TOPSOIL SPREADING, 5" THICK	47	S.Y.
40	FERTILIZING AND SEEDING, TYPE A-5	47	S.Y.
41	STRAW MULCHING	47	S.Y.

NOTES:

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NOTE:

1. RIGHT OF WAY PLOTTED FROM RECORD PLANS AND TAX MAPS.

30' 20' 10' 0 30' 60'

SCALE 1" = 30'

STATE OF NEW JERSEY
BETH-ANN M. GRASSO
No. GE 43121
LICENSED PROFESSIONAL ENGINEER
C18

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

DATE	REVISIONS	BY	CHK.

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

CONSTRUCTION PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE 1"=30'	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 14 OF 84

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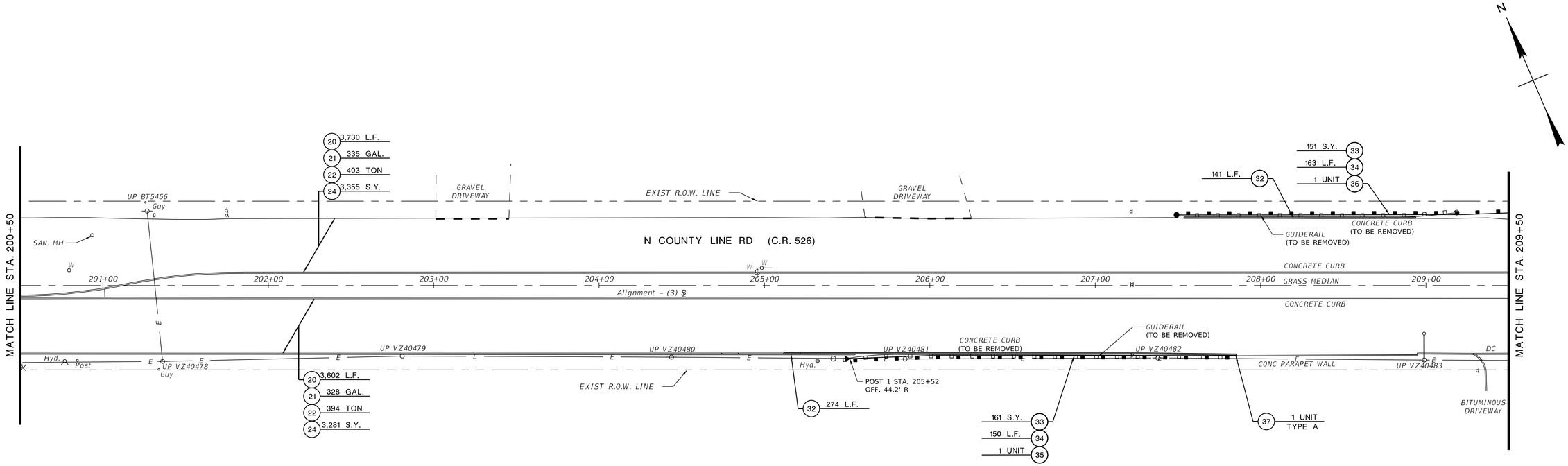

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30' 20' 10' 0 30' 60'

SCALE 1" = 30'

TO BE CONSTRUCTED

ITEM	DESCRIPTION	QUANTITIES	
20	POLYMERIZED JOINT ADHESIVE	7,332	L.F.
21	TACK COAT	663	GAL.
22	HOT MIX ASPHALT 12.5M64 SURFACE COURSE, 2" THICK	797	TON
24	HMA MILLING, 2" DEPTH	6,636	S.Y.
32	9" X 14" CONCRETE VERTICAL CURB	415	L.F.
33	NONVEGETATIVE SURFACE, HOT MIX ASPHALT	312	S.Y.
34	BEAM GUIDE RAIL	313	L.F.
35	TANGENT GUIDE RAIL TERMINAL	1	UN.
36	BEAM GUIDE RAIL ANCHORAGE	1	UN.
37	APPROACH GUIDE RAIL TRANSITION TL-3	1	UN.

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

STATE OF NEW JERSEY
BETH-ANN M. GRASSO
No. GE 43121
LICENSED PROFESSIONAL ENGINEER

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

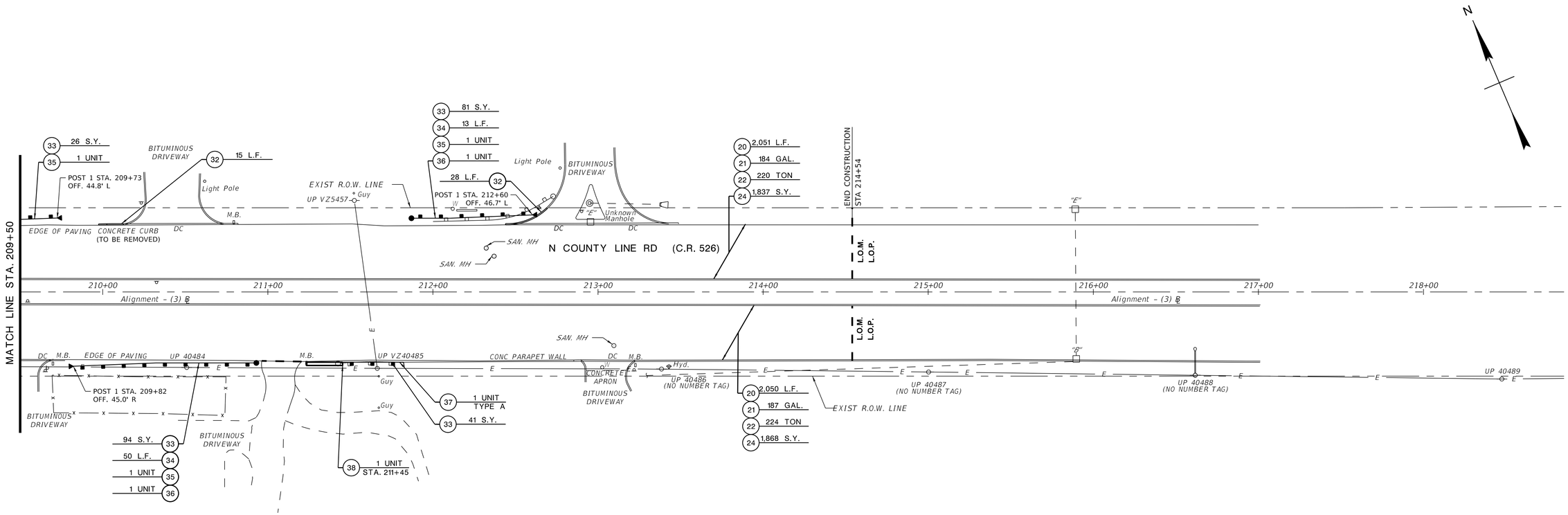
FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

CONSTRUCTION PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE 1"=30'	DRAWN C.D.	APPROVED B.M.G.
FILE NO.	12 - 009 - 0128		SHEET 15 OF 84

C18



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TO BE CONSTRUCTED			
ITEM	DESCRIPTION	QUANTITIES	
20	POLYMERIZED JOINT ADHESIVE	4,101	L.F.
21	TACK COAT	371	GAL.
22	HOT MIX ASPHALT 12.5M64 SURFACE COURSE, 2" THICK	444	TON
24	HMA MILLING, 2" DEPTH	3,705	S.Y.
32	9" X 14" CONCRETE VERTICAL CURB	43	L.F.
33	NONVEGETATIVE SURFACE, HOT MIX ASPHALT	242	S.Y.
34	BEAM GUIDE RAIL	63	L.F.
35	TANGENT GUIDE RAIL TERMINAL	3	UN.
36	BEAM GUIDE RAIL ANCHORAGE	2	UN.
37	APPROACH GUIDE RAIL TRANSITION TL-3	1	UN.
38	CRASH CUSHION, COMPRESSIVE BARRIER, TYPE 3, WIDTH NARROW	1	UN.

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STATE OF NEW JERSEY
BETH-ANN M. GRASSO
No. GE 43121
LICENSED PROFESSIONAL ENGINEER

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

DATE	REVISIONS	BY	CHK.

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

CONSTRUCTION PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE 1"=30'	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 16 OF 84

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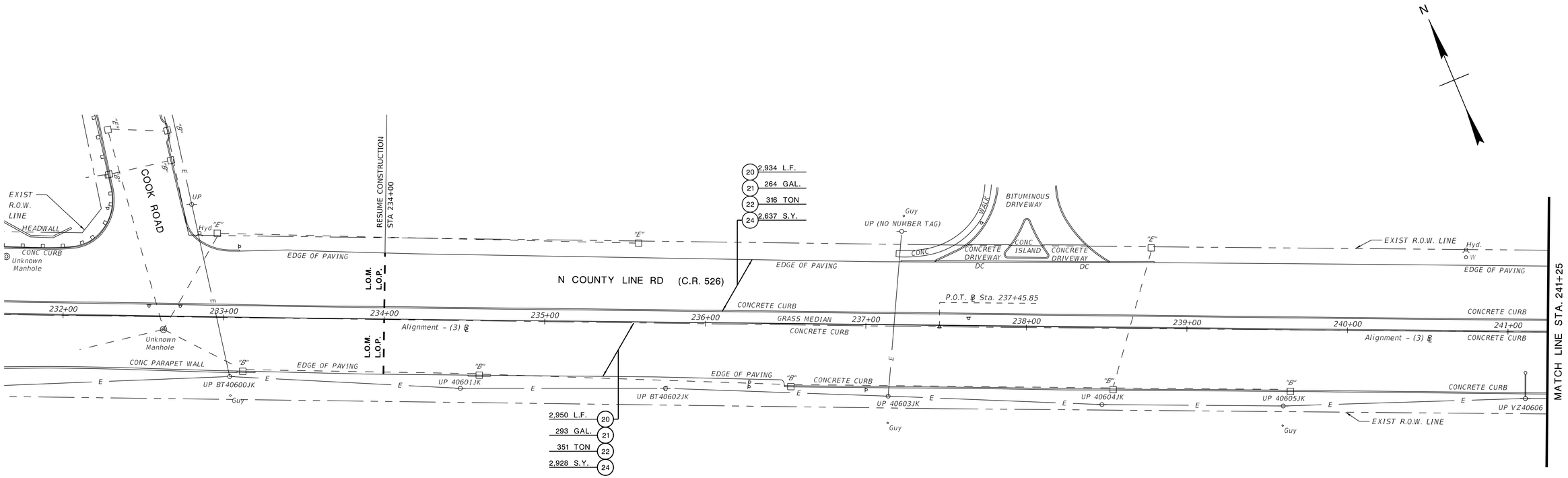

TO BE CONSTRUCTED		
ITEM	DESCRIPTION	QUANTITIES
20	POLYMERIZED JOINT ADHESIVE	5,884 L.F.
21	TACK COAT	557 GAL.
22	HOT MIX ASPHALT 12.5M64 SURFACE COURSE, 2" THICK	667 TON
24	HMA MILLING, 2" DEPTH	5,565 S.Y.

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4/28/2026

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OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

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JACKSON TOWNSHIP

CONSTRUCTION PLAN

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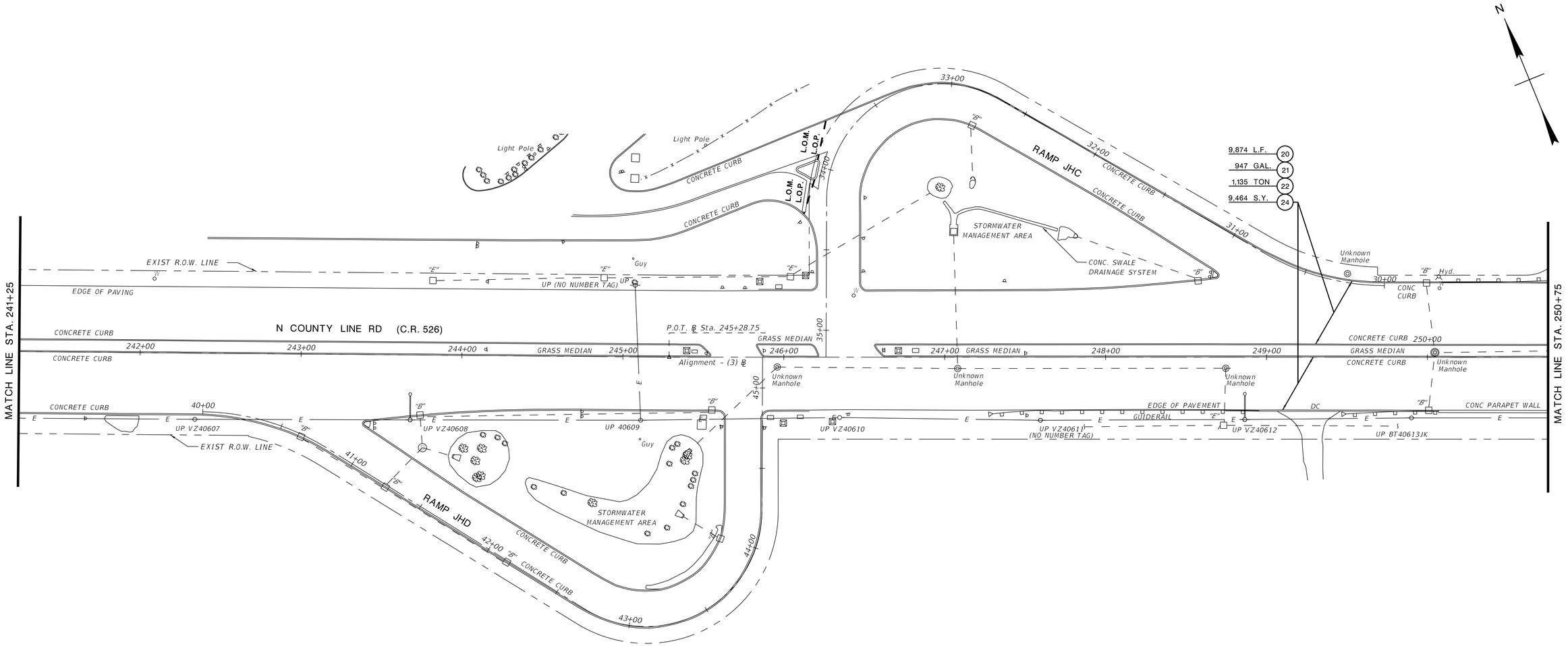

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30′ 20′ 10′ 0 30′ 60′

SCALE 1″ = 30′

TO BE CONSTRUCTED			
ITEM	DESCRIPTION	QUANTITIES	
20	POLYMERIZED JOINT ADHESIVE	9,874	L.F.
21	TACK COAT	947	GAL.
22	HOT MIX ASPHALT 12.5M64 SURFACE COURSE, 2″ THICK	1,135	TON
24	HMA MILLING, 2″ DEPTH	9,464	S.Y.

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

4/28/2026

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OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

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JACKSON TOWNSHIP

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FILE NO. 12 - 009 - 0128			SHEET 18 OF 84

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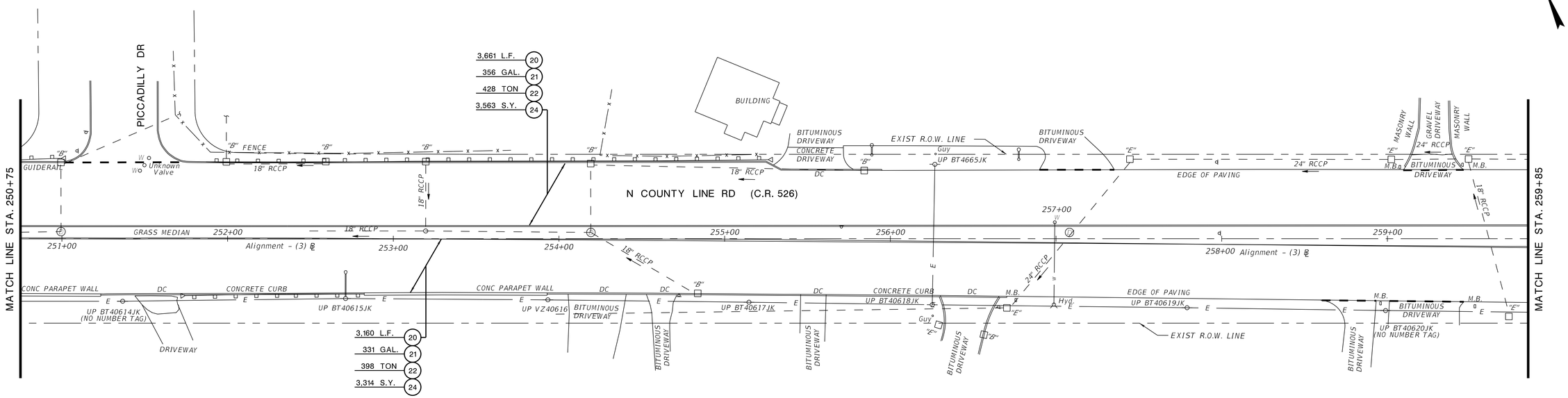

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TO BE CONSTRUCTED		
ITEM	DESCRIPTION	QUANTITIES
20	POLYMERIZED JOINT ADHESIVE	6,821 L.F.
21	TACK COAT	687 GAL.
22	HOT MIX ASPHALT 12.5M64 SURFACE COURSE, 2" THICK	826 TON
24	HMA MILLING, 2" DEPTH	6,877 S.Y.

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

STATE OF NEW JERSEY BETH-ANN M. GRASSO No. GE 43121 LICENSED PROFESSIONAL ENGINEER

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

CONSTRUCTION PLAN

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FILE NO.	12 - 009 - 0128		SHEET 19 OF 84

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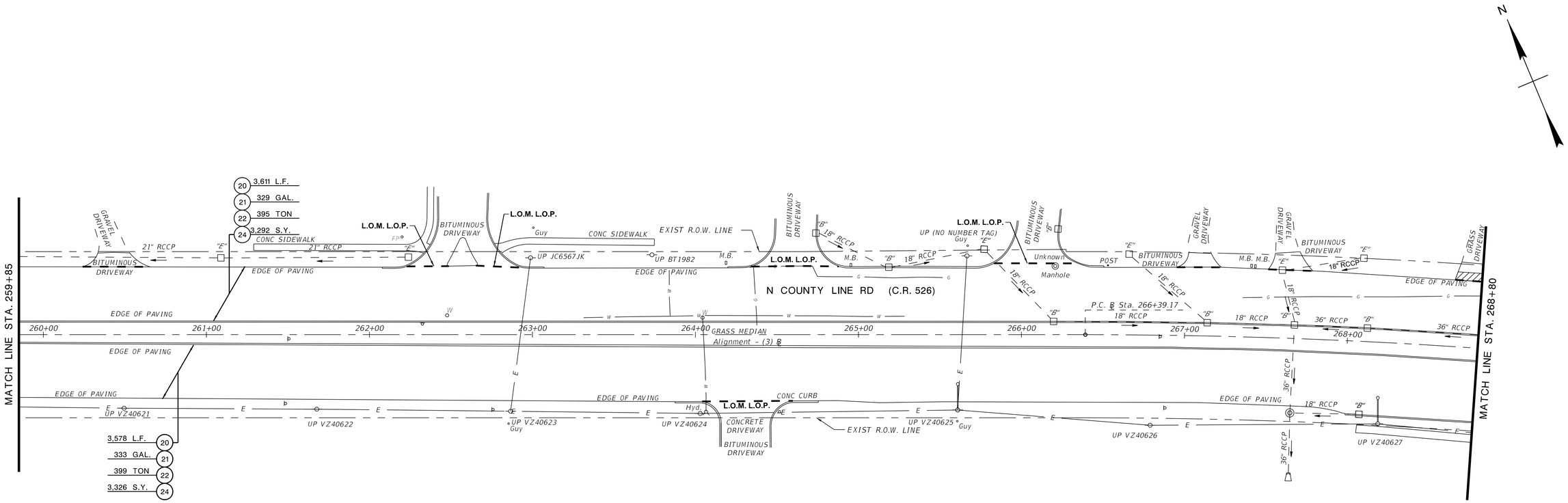

TO BE CONSTRUCTED		
ITEM	DESCRIPTION	QUANTITIES
20	POLYMERIZED JOINT ADHESIVE	7,189 L.F.
21	TACK COAT	662 GAL.
22	HOT MIX ASPHALT 12.5M64 SURFACE COURSE, 2" THICK	794 TON
24	HMA MILLING, 2" DEPTH	6,618 S.Y.

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PROFESSIONAL ENGINEER

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OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

FROM JACKSON MILLS RD
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JACKSON TOWNSHIP

CONSTRUCTION PLAN

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	SCALE 1"=30'	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 20 OF 84

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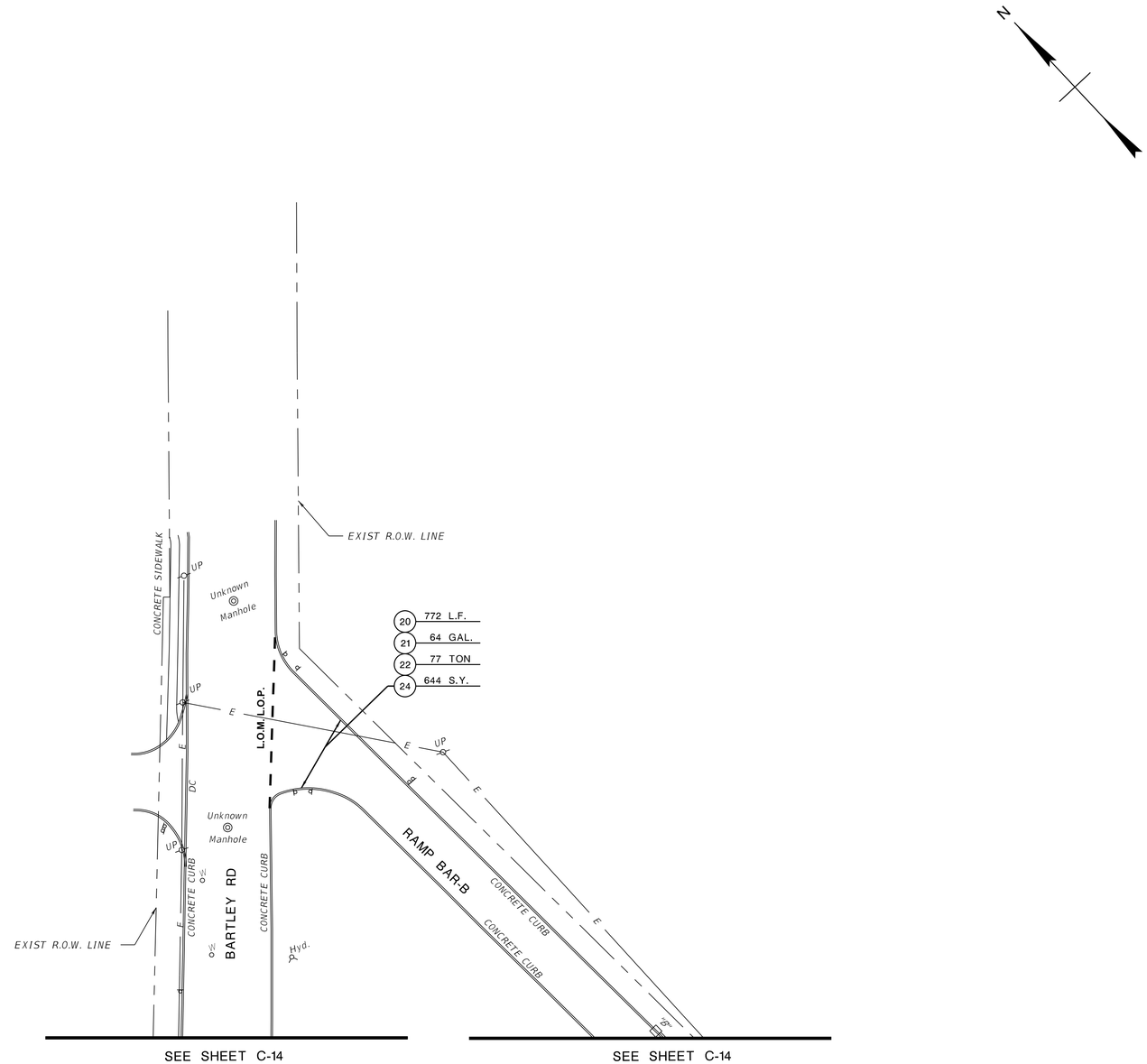

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TO BE CONSTRUCTED			
ITEM	DESCRIPTION	QUANTITIES	
20	POLYMERIZED JOINT ADHESIVE	772	L.F.
21	TACK COAT	64	GAL.
22	HOT MIX ASPHALT 12.5M64 SURFACE COURSE, 2" THICK	77	TON
24	HMA MILLING, 2" DEPTH	644	S.Y.

STATE OF NEW JERSEY
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C18

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4/28/2026

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DATE	REVISIONS	BY	CHK.

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

FROM JACKSON MILLS RD
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CONSTRUCTION PLAN

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FILE NO. 12 - 009 - 0128			SHEET 21 OF 84

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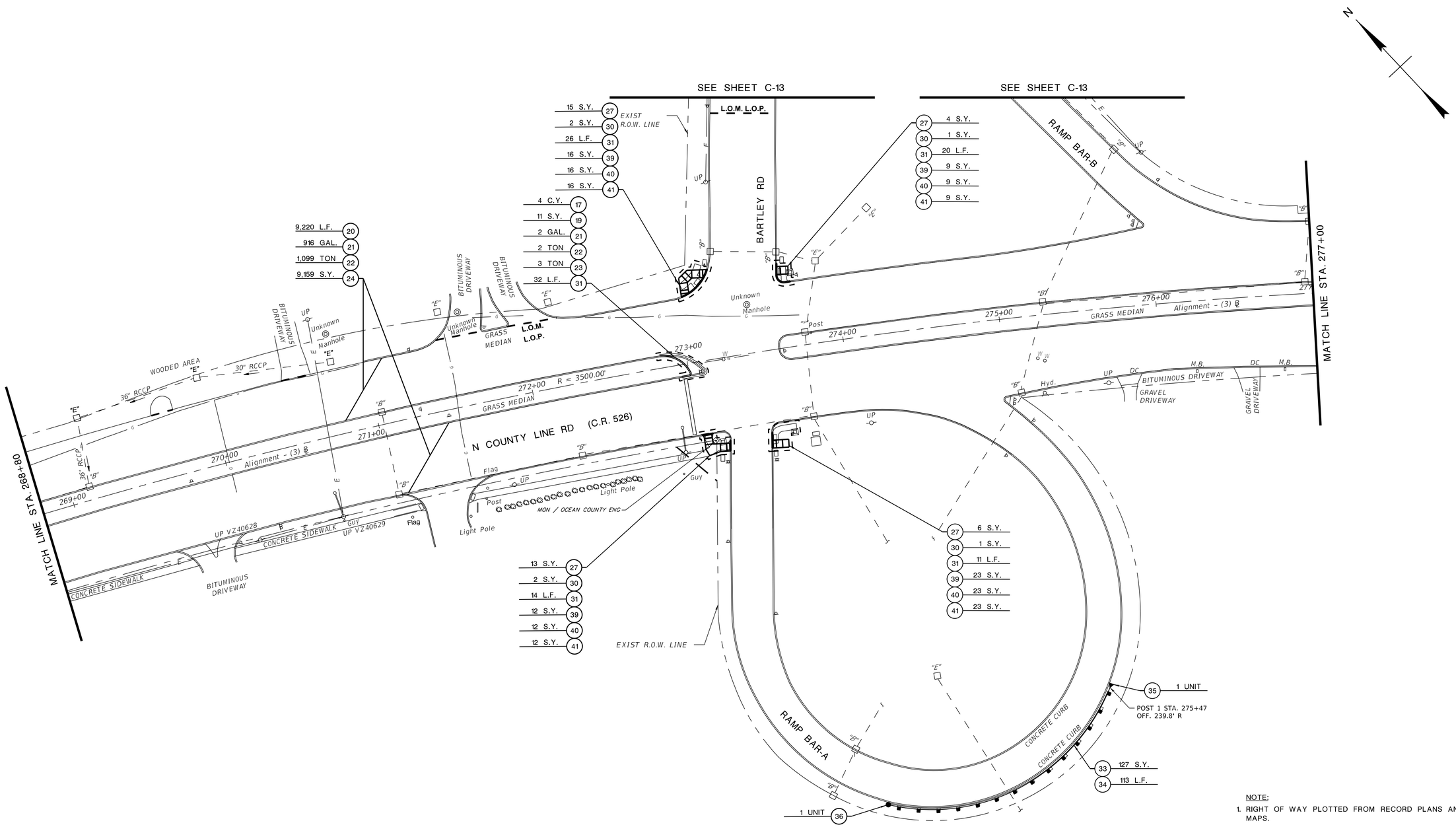

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TO BE CONSTRUCTED

ITEM	DESCRIPTION	QUANTITIES	
17	EXCAVATION, UNCLASSIFIED	4	C.Y.
19	DENSE-GRADED AGGREGATE BASE COURSE, 6" THICK	11	S.Y.
20	POLYMERIZED JOINT ADHESIVE	9,220	L.F.
21	TACK COAT	918	GAL.
22	HOT MIX ASPHALT 12.5M64 SURFACE COURSE, 2" THICK	1,101	TON
23	HOT MIX ASPHALT 19M64 BASE COURSE, 4" THICK	3	TON
24	HMA MILLING, 2" DEPTH	9,159	S.Y.
27	CONCRETE SIDEWALK, 4" THICK	38	S.Y.
30	DETECTABLE WARNING SURFACE	6	S.Y.
31	9" X 18" CONCRETE VERTICAL CURB	103	L.F.
33	NONVEGETATIVE SURFACE, HOT MIX ASPHALT	127	S.Y.
34	BEAM GUIDE RAIL	113	L.F.
35	TANGENT GUIDE RAIL TERMINAL	1	UN.
36	BEAM GUIDE RAIL ANCHORAGE	1	UN.
39	TOPSOIL SPREADING, 5" THICK	60	S.Y.
40	FERTILIZING AND SEEDING, TYPE A-5	60	S.Y.
41	STRAW MULCHING	60	S.Y.

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DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

STATE OF NEW JERSEY BETH-ANN M. GRASSO No. GE 43121 LICENSED PROFESSIONAL ENGINEER

4/28/2026

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NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

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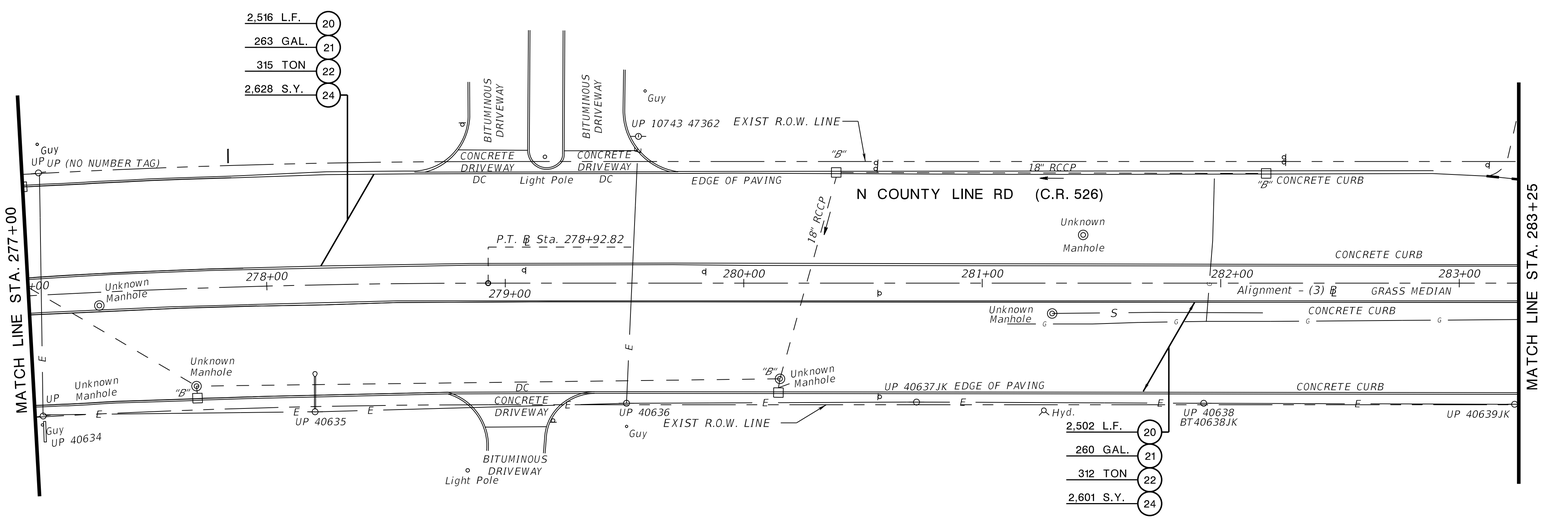
FILE NO.	12 - 009 - 0128		SHEET 22 OF 84

C18



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6. Plans do not show locations of utility poles, fire hydrants, utilities, junction boxes, traffic control boxes and traffic signals. Contractor shall inspect each site and confirm location of possible obstructions to ADA curb ramp and sidewalks.
7. Paving limit is approximate. Paving limit shall be field verified to match existing pavement joint.

NOTE:

1. RIGHT OF WAY PLOTTED FROM RECORD PLANS AND TAX MAPS.

30' 20' 10' 0 30' 60'

SCALE 1" = 30'

TO BE CONSTRUCTED		
ITEM	DESCRIPTION	QUANTITIES
20	POLYMERIZED JOINT ADHESIVE	5,018 L.F.
21	TACK COAT	523 GAL.
22	HOT MIX ASPHALT 12.5M64 SURFACE COURSE, 2" THICK	627 TON
24	HMA MILLING, 2" DEPTH	5,229 S.Y.

STATE OF NEW JERSEY BETH-ANN M. GRASSO No. GE 43121 LICENSED PROFESSIONAL ENGINEER

C18

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

DATE	REVISIONS	BY	CHK.

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

CONSTRUCTION PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE 1"=30'	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 23 OF 84

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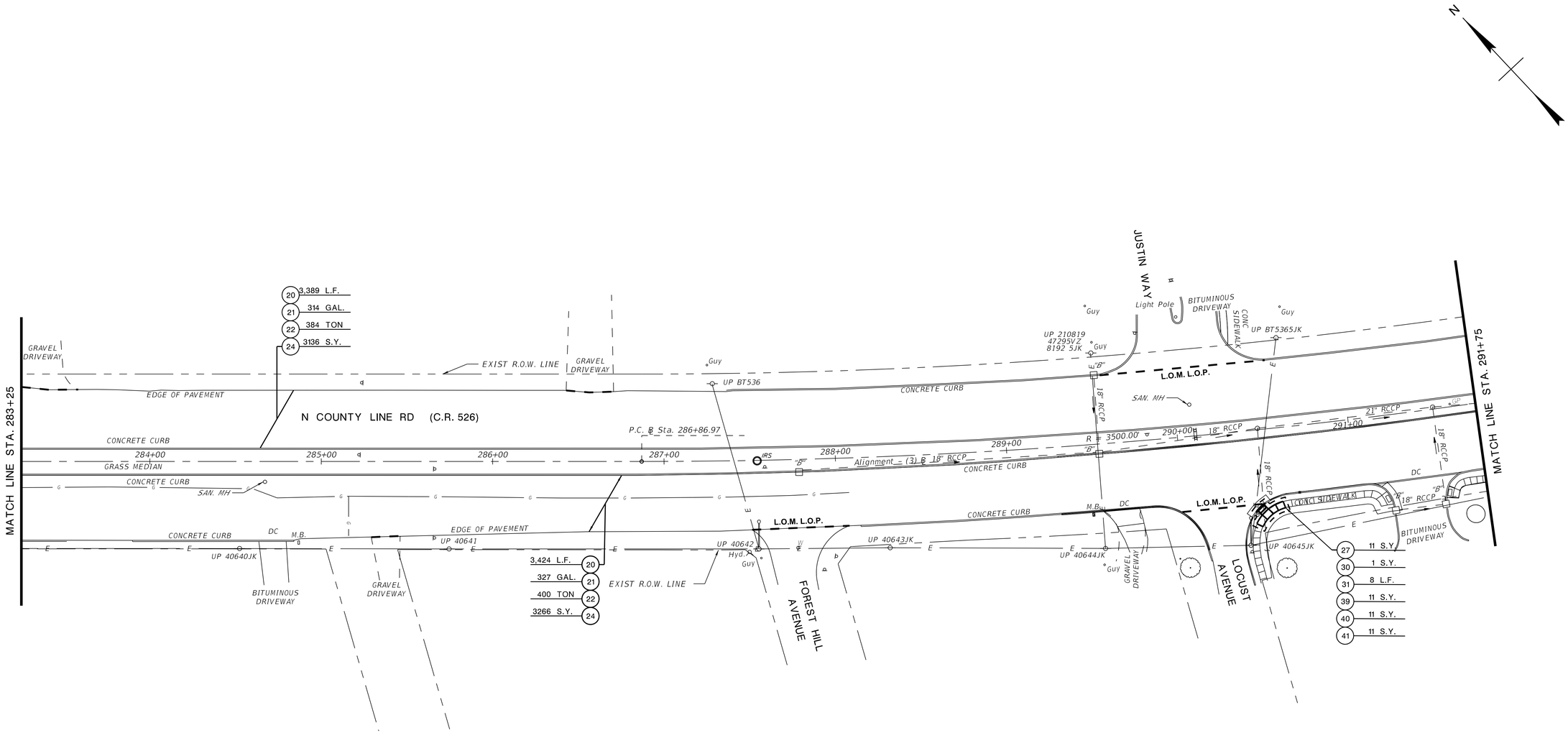
Plotted by: CPSEC on Apr 27, 2026 - 10:08am



NOTE:

1. RIGHT OF WAY PLOTTED FROM RECORD PLANS AND TAX MAPS.

NOTES:

1. Location of the plan features for right-of-way, pavement centerline, edge of pavement, sidewalk and drainage structures is approximate and not based on survey data.
2. North arrow is for general orientation purposes only.
3. Contractor shall remove existing curb to the nearest joint or sawcut a minimum distance of 5 feet from the curb and new curb constructed to match the line and grade of the existing curb.
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30' 20' 10' 0 30' 60'

SCALE 1" = 30'

TO BE CONSTRUCTED

ITEM	DESCRIPTION	QUANTITIES	
20	POLYMERIZED JOINT ADHESIVE	6,813	L.F.
21	TACK COAT	641	GAL.
22	HOT MIX ASPHALT 12.5M64 SURFACE COURSE, 2" THICK	784	TON
24	HMA MILLING, 2" DEPTH	6,402	S.Y.
27	CONCRETE SIDEWALK, 4" THICK	11	S.Y.
30	DETECTABLE WARNING SURFACE	1	S.Y.
31	9" X 18" CONCRETE VERTICAL CURB	8	L.F.
39	TOPSOIL SPREADING, 5" THICK	11	S.Y.
40	FERTILIZING AND SEEDING, TYPE A-5	11	S.Y.
41	STRAW MULCHING	11	S.Y.

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

STATE OF NEW JERSEY
BETH-ANN M. GRASSO
No. GE 43121
LICENSED PROFESSIONAL ENGINEER

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

CONSTRUCTION PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE 1"=30'	DRAWN C.D.	APPROVED B.M.G.
FILE NO.	12 - 009 - 0128		SHEET 24 OF 84

C18

I:\Accounts\OCEAN\12-009\01 - CONSTRUCTION\Phase 1\Phase 1 CD SET\PLANS_SHEETS\C-18.dgn
PENNONI-GRASSOBA-CO-ALL-LPF_ms.PLTCFG
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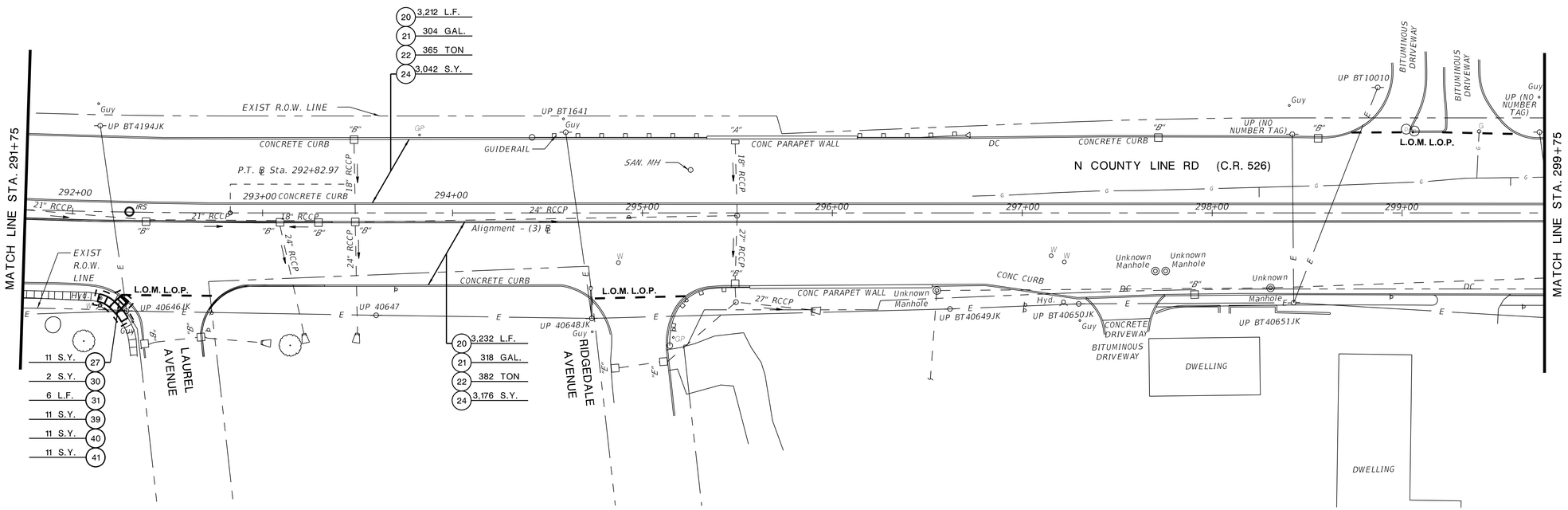

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30' 20' 10' 0 30' 60'

SCALE 1" = 30'

TO BE CONSTRUCTED

ITEM	DESCRIPTION	QUANTITIES	
20	POLYMERIZED JOINT ADHESIVE	6,444	L.F.
21	TACK COAT	622	GAL.
22	HOT MIX ASPHALT 12.5M64 SURFACE COURSE, 2" THICK	747	TON
24	HMA MILLING, 2" DEPTH	6,218	S.Y.
27	CONCRETE SIDEWALK, 4" THICK	11	S.Y.
30	DETECTABLE WARNING SURFACE	2	S.Y.
31	9" X 18" CONCRETE VERTICAL CURB	6	L.F.
39	TOPSOIL SPREADING, 5" THICK	11	S.Y.
40	FERTILIZING AND SEEDING, TYPE A-5	11	S.Y.
41	STRAW MULCHING	11	S.Y.

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

STATE OF NEW JERSEY BETH-ANN M. GRASSO No. GE 43121 LICENSED PROFESSIONAL ENGINEER

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

CONSTRUCTION PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE 1"=30'	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 25 OF 84

C:\Users\CDEL\OCEANCO - OCEAN NS Bennett & Bayville SIDEWALK_SHEETS.dwg
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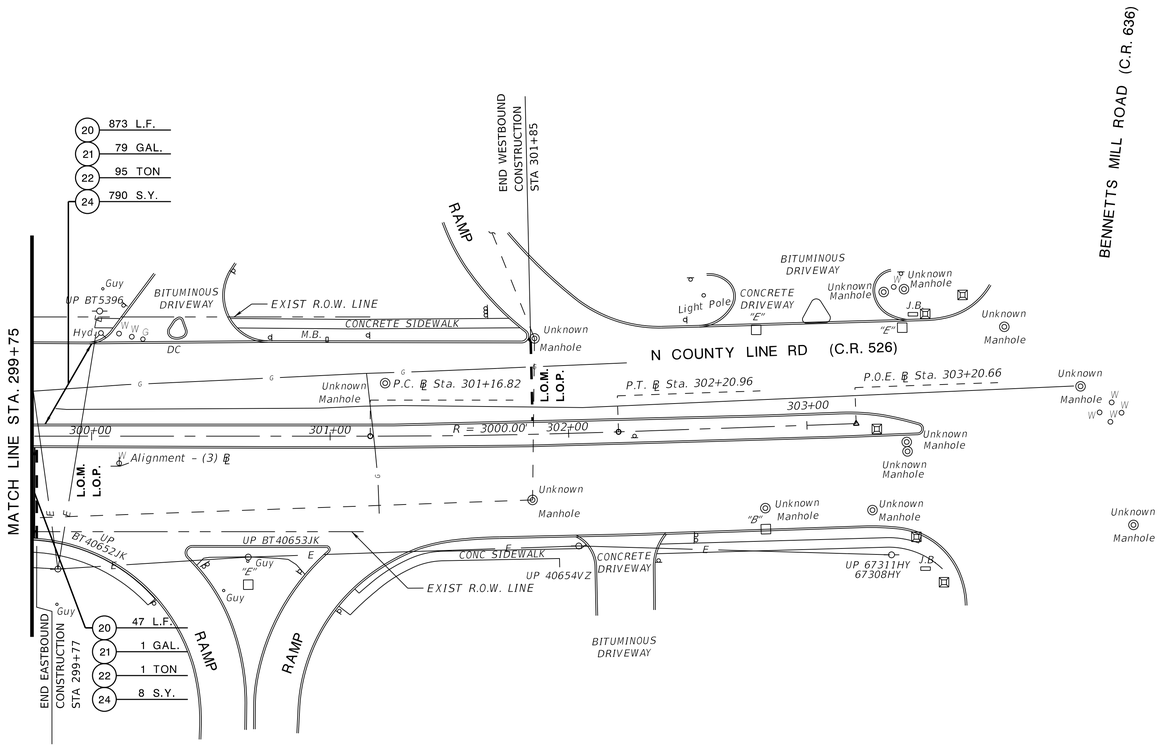
Plotted: 5/28/2026



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NOTE:

1. RIGHT OF WAY PLOTTED FROM RECORD PLANS AND TAX MAPS.

30' 20' 10' 0 30' 60'

SCALE 1" = 30'

TO BE CONSTRUCTED

ITEM	DESCRIPTION	QUANTITIES
20	POLYMERIZED JOINT ADHESIVE	920 L.F.
21	TACK COAT	80 GAL.
22	HOT MIX ASPHALT 12.5M64 SURFACE COURSE, 2" THICK	96 TON
24	HMA MILLING, 2" DEPTH	798 S.Y.

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

STATE OF NEW JERSEY BETH-ANN M. GRASSO No. GE 43121 LICENSED PROFESSIONAL ENGINEER

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

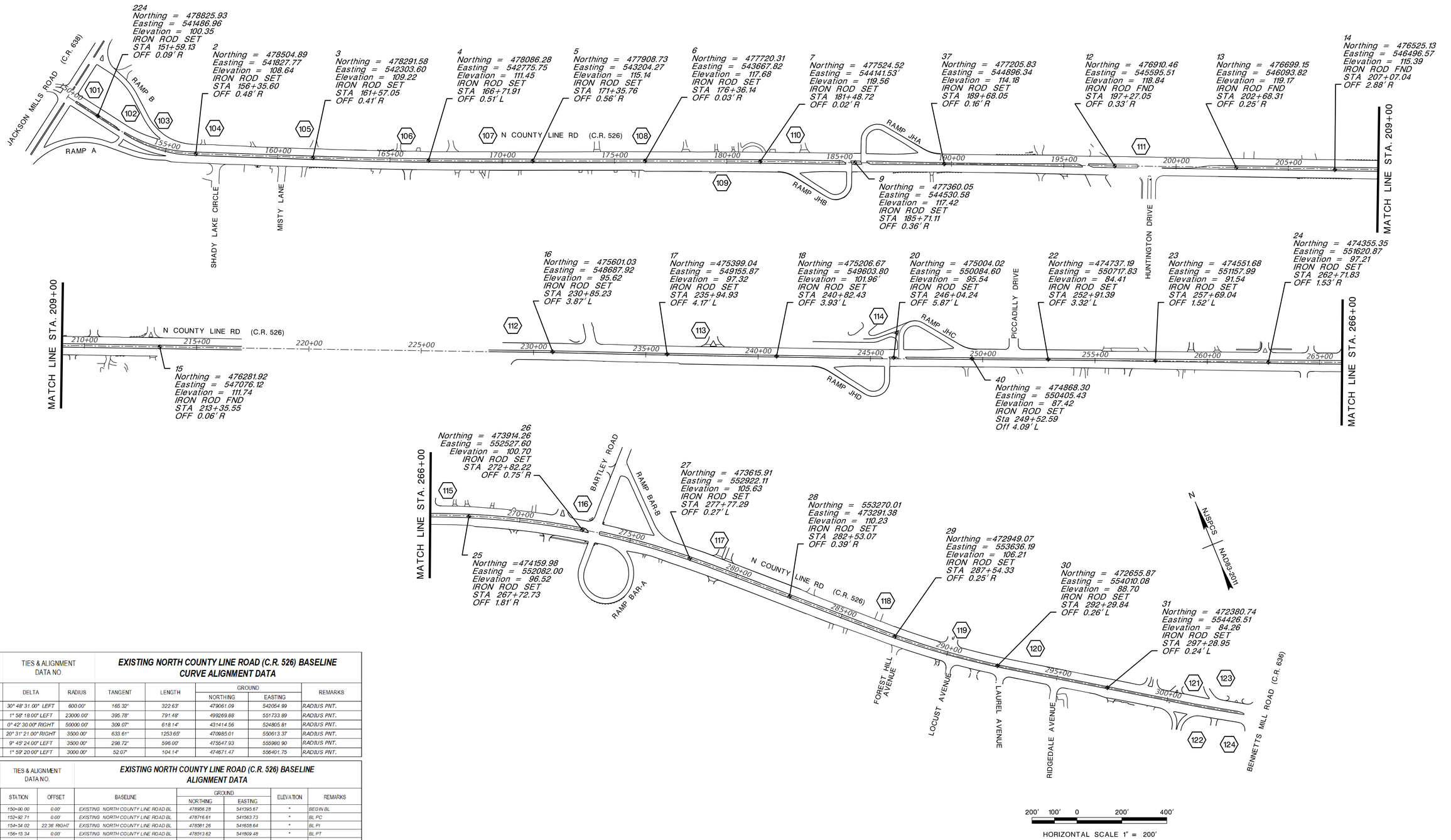
FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

CONSTRUCTION PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE 1"=30'	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 26 OF 84

C18



*	TIES & ALIGNMENT DATA NO.		EXISTING NORTH COUNTY LINE ROAD (C.R. 526) BASELINE CURVE ALIGNMENT DATA				
NO.	DELTA	RADIUS	TANGENT	LENGTH	GROUND NORTHING	GROUND EASTING	REMARKS
C1	30° 48' 31.00" LEFT	600.00'	165.32'	322.63'	479061.09	542054.99	RADIUS PNT.
C2	1° 58' 18.00" LEFT	23000.00'	395.78'	791.48'	499269.88	551733.89	RADIUS PNT.
C3	0° 42' 30.00" RIGHT	50000.00'	309.07'	618.14'	431414.56	524805.81	RADIUS PNT.
C4	20° 31' 21.00" RIGHT	3500.00'	633.61'	1253.65'	470985.01	550613.37	RADIUS PNT.
C5	9° 45' 24.00" LEFT	3500.00'	298.72'	596.00'	475547.93	555980.90	RADIUS PNT.
C6	1° 59' 20.00" LEFT	3000.00'	52.07'	104.14'	474671.47	556401.75	RADIUS PNT.

*	TIES & ALIGNMENT DATA NO.		EXISTING NORTH COUNTY LINE ROAD (C.R. 526) BASELINE ALIGNMENT DATA				
NO.	STATION	OFFSET	BASELINE	GROUND NORTHING	GROUND EASTING	ELEVATION	REMARKS
1	150+00.00	0.00'	EXISTING NORTH COUNTY LINE ROAD BL	478956.28	541395.67	*	BEGIN BL
2	152+92.71	0.00'	EXISTING NORTH COUNTY LINE ROAD BL	478716.61	541563.73	*	BL PC
3	154+54.02	22.36' RIGHT	EXISTING NORTH COUNTY LINE ROAD BL	478581.26	541658.64	*	BL PI
4	156+15.34	0.00'	EXISTING NORTH COUNTY LINE ROAD BL	478513.62	541809.48	*	BL PT
5	161+77.96	0.00'	EXISTING NORTH COUNTY LINE ROAD BL	478283.40	542322.85	*	BL PC
6	165+73.70	3.40' RIGHT	EXISTING NORTH COUNTY LINE ROAD BL	478121.46	542683.98	*	BL PI
7	169+69.44	0.00'	EXISTING NORTH COUNTY LINE ROAD BL	477972.04	543050.47	*	BL PT
8	176+52.33	0.00'	EXISTING NORTH COUNTY LINE ROAD BL	477714.23	543682.82	*	BL PC
9	179+61.40	0.96' LEFT	EXISTING NORTH COUNTY LINE ROAD BL	477597.53	543969.02	*	BL PI
10	182+70.47	0.00'	EXISTING NORTH COUNTY LINE ROAD BL	477477.32	544253.75	*	BL PT
11	198+63.93	0.00'	EXISTING NORTH COUNTY LINE ROAD BL	476856.74	545723.60	*	BL PI
12	230+31.88	0.00'	EXISTING NORTH COUNTY LINE ROAD BL	475610.72	548655.76	*	BL PI
13	237+45.85	0.00'	EXISTING NORTH COUNTY LINE ROAD BL	475335.32	549292.75	*	BL PI
14	245+28.75	0.00'	EXISTING NORTH COUNTY LINE ROAD BL	475027.66	550012.67	*	BL PI
15	266+39.17	0.00'	EXISTING NORTH COUNTY LINE ROAD BL	474215.37	551960.50	*	BL PC
16	272+65.99	56.89' LEFT	EXISTING NORTH COUNTY LINE ROAD BL	473971.50	552545.30	*	BL PI
17	278+92.82	0.00'	EXISTING NORTH COUNTY LINE ROAD BL	473738.09	553007.49	*	BL PT
18	286+86.97	0.00'	EXISTING NORTH COUNTY LINE ROAD BL	472994.86	553586.78	*	BL PC
19	289+84.97	12.72' RIGHT	EXISTING NORTH COUNTY LINE ROAD BL	472790.52	553804.69	*	BL PI
20	292+82.97	0.00'	EXISTING NORTH COUNTY LINE ROAD BL	472626.07	554054.06	*	BL PT
21	301+16.82	0.00'	EXISTING NORTH COUNTY LINE ROAD BL	472167.01	554750.18	*	BL PC
22	301+68.89	0.45' RIGHT	EXISTING NORTH COUNTY LINE ROAD BL	472138.34	554793.65	*	BL PI
23	302+20.96	0.00'	EXISTING NORTH COUNTY LINE ROAD BL	472111.20	554838.09	*	BL PT
24	303+20.66	0.00'	EXISTING NORTH COUNTY LINE ROAD BL	472059.23	554923.18	*	END BL

DATE	REVISIONS	BY	CHK.

Pennoni
NJ COA NO. GA28033300

RECONSTRUCTION OF
NORTH COUNTY LINE RD, C.R. 526
FROM JACKSON MILLS RD, C.R. 638
TO BENNETTS MILLS RD, C.R. 636
JACKSON TOWNSHIP, OCEAN COUNTY, NJ

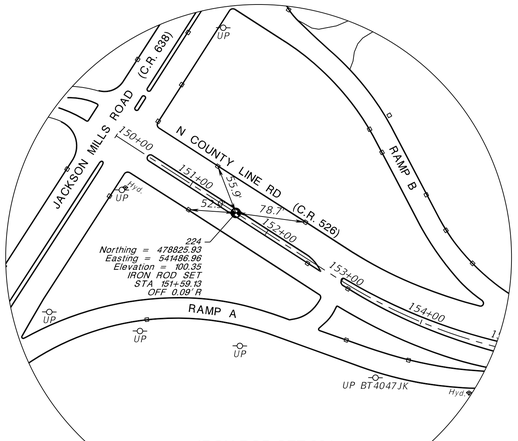
TIES

FLD.BK.	DATE MARCH 6, 2026	DESIGNED M.D.D.	CHECKED K.M.L.
	SCALE 1" = 200'	DRAWN M.D.D.	APPROVED K.M.L.
FILE NO. 12 - 009 - 0128			SHEET 27 OF 83

TIE01 / TIE04

KEITH M. LUDWIG, P.L.S.
PROFESSIONAL LAND SURVEYOR
NJ LICENSE NO. 24GS04324400

U:\Accurist\CORP\CORP\CORP GP2023-SB Round 6 Rout\CR 526\DESIGN_SHEETS\TIE-01.dgn
Jackson Twp
Ocean County
NJ



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EASTING = 541486.96
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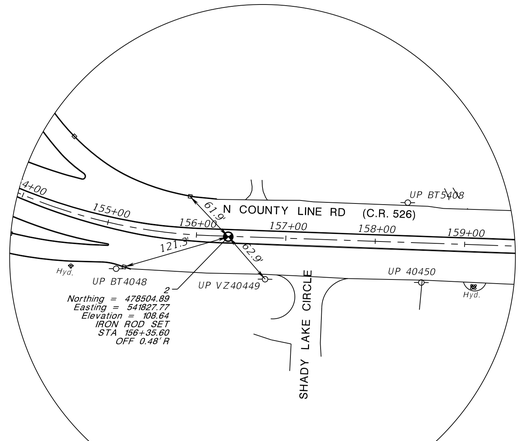

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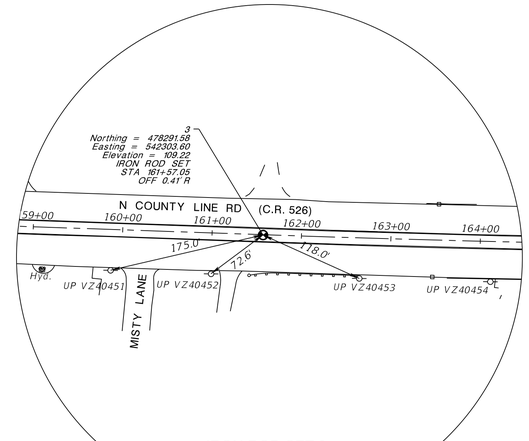

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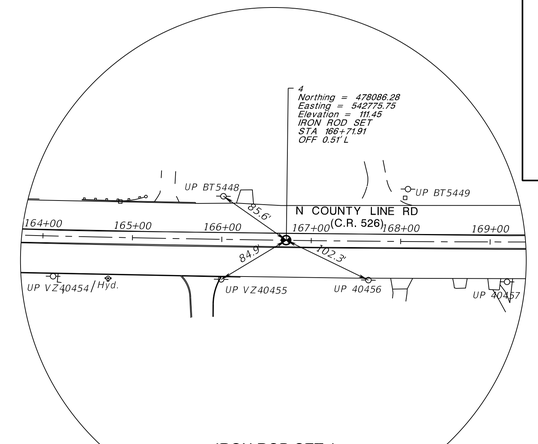

IRON ROD SET 4
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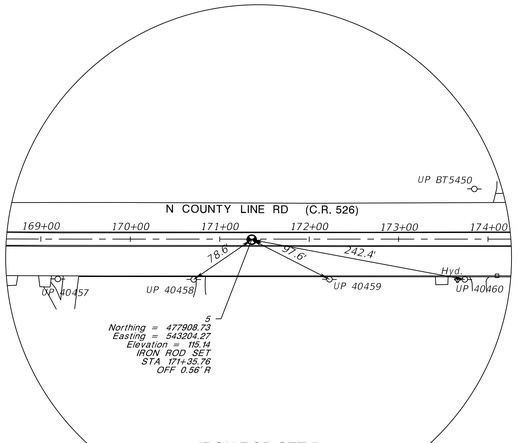

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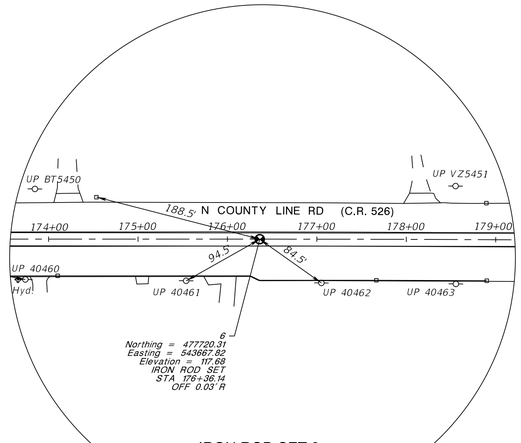

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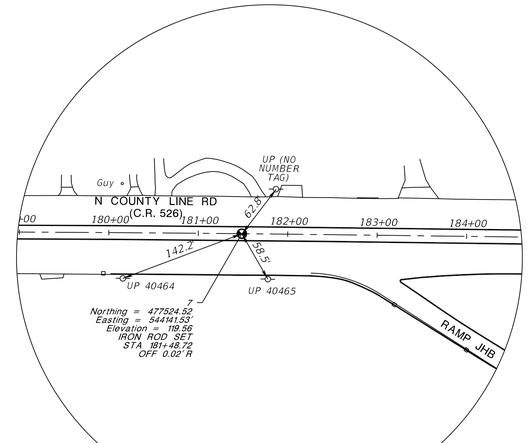

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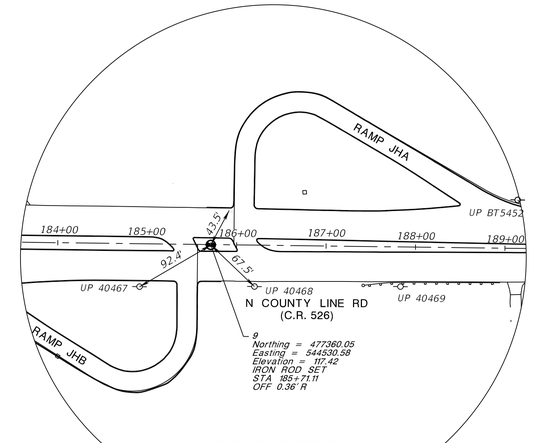

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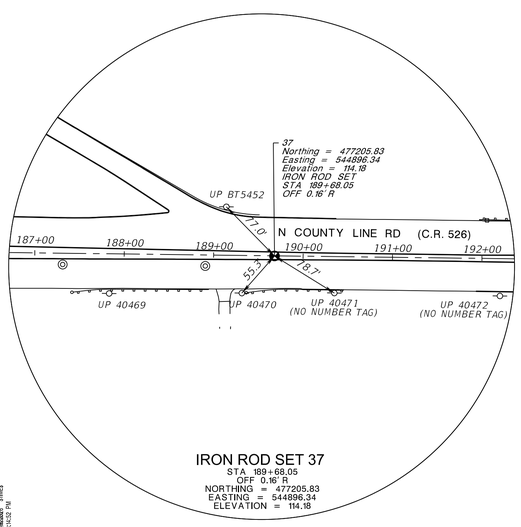

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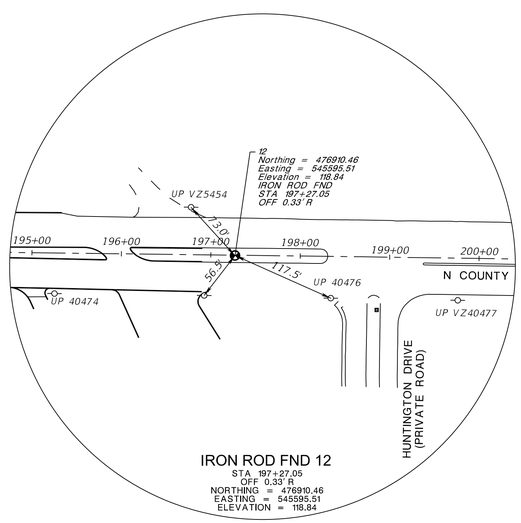

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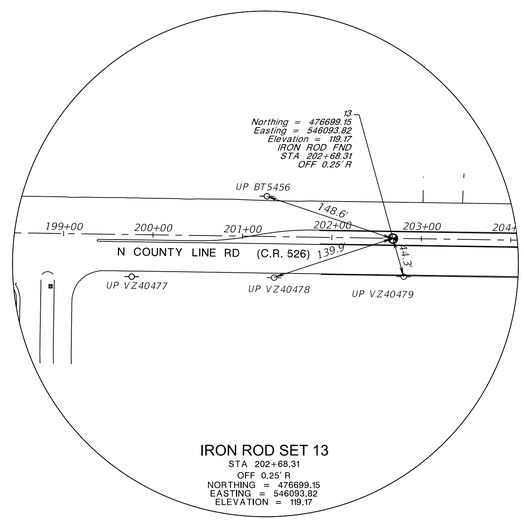

IRON ROD SET 13
STA 202+68.31
OFF 0.25' R
NORTHING = 476699.15
EASTING = 546093.82
ELEVATION = 119.17

N
NJSPCS
NAD83-2011

DATE	REVISIONS	BY	CHK.

Pennoni
NJ COA NO. GA28033300

RECONSTRUCTION OF
NORTH COUNTY LINE RD, C.R. 526
FROM JACKSON MILLS RD, C.R. 638
TO BENNETTS MILLS RD, C.R. 636
JACKSON TOWNSHIP, OCEAN COUNTY, NJ

TIES

FLD.BK.	DATE MARCH 6, 2026	DESIGNED M.D.D.	CHECKED K.M.L.
	SCALE NOT TO SCALE	DRAWN M.D.D.	APPROVED K.M.L.
FILE NO. 12 - 009 - 0128			SHEET 28 OF 83

TIE02
TIE04

KEITH M. LUDWIG, P.L.S.
PROFESSIONAL LAND SURVEYOR
NJ LICENSE NO. 24GS04324400

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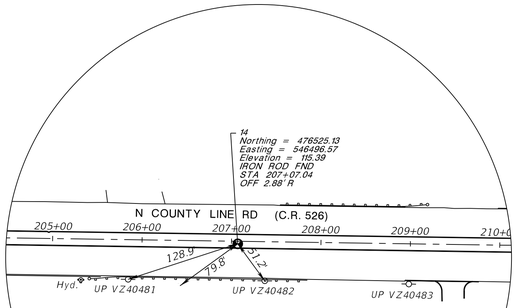
mludwig



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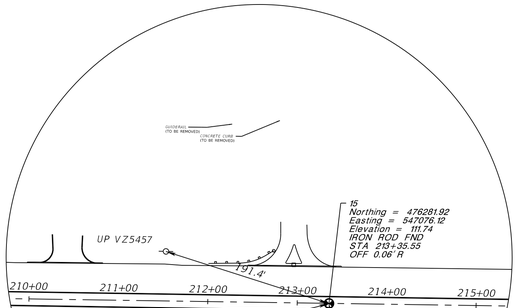
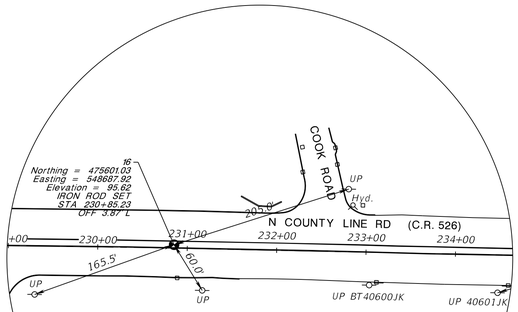
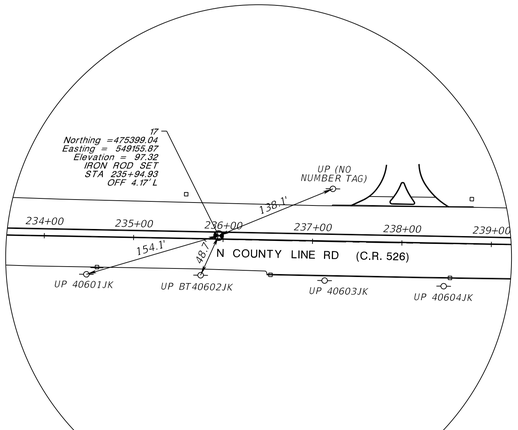
ELEVATION = 115.39



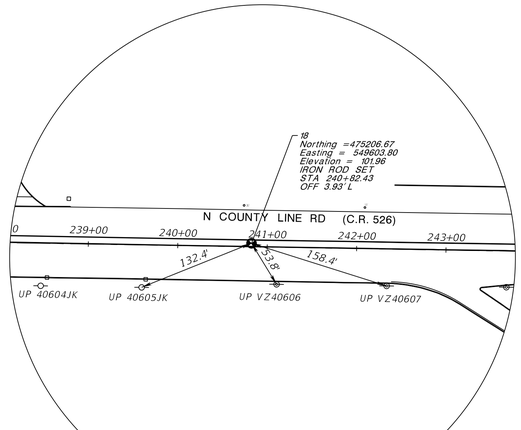
IRON ROD SET 15
STA 213+35.55
OFF 0.06' R
NORTHING = 476281.92
EASTING = 547076.12
ELEVATION = 111.74



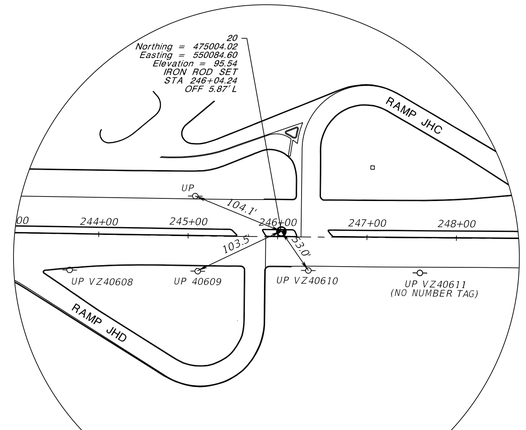
IRON ROD SET 16
STA 230+85.23
OFF 3.87' L
NORTHING = 475601.03
EASTING = 548687.92
ELEVATION = 95.62



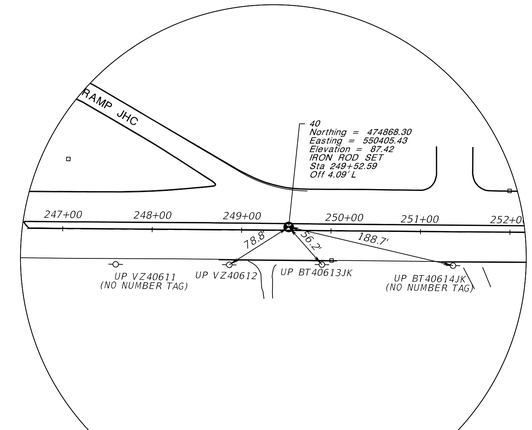
IRON ROD SET 17
STA 235+94.93
OFF 4.17' L
NORTHING =475399.04
EASTING = 549155.87
ELEVATION = 97.32



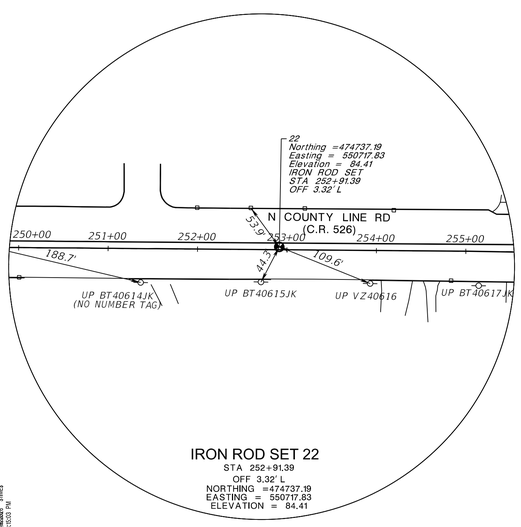
IRON ROD SET 18
STA 240+82.43
OFF 3.93' L
NORTHING =475206.67
EASTING = 549603.80
ELEVATION = 101.96



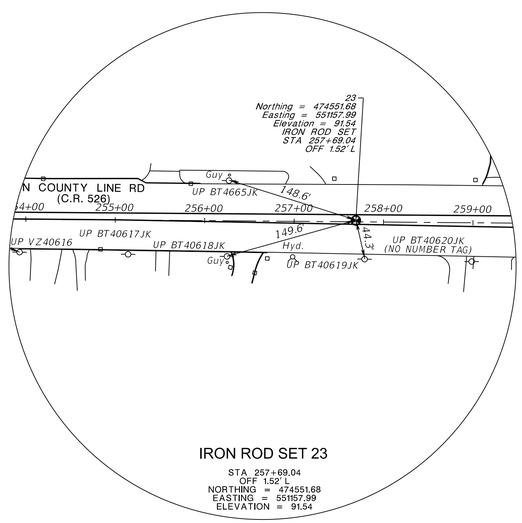
IRON ROD SET 20
STA 246+04.24
OFF 5.87' L
NORTHING = 475004.02
EASTING = 550084.60
ELEVATION = 95.54



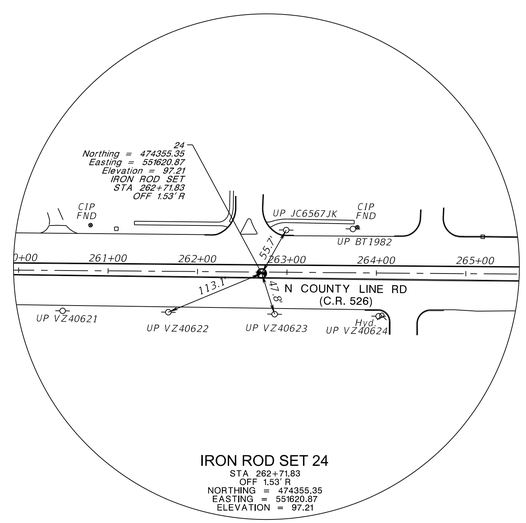
IRON ROD SET 40
STA 249+52.59
OFF 4.09' L
NORTHING = 474868.30
EASTING = 550405.43
ELEVATION = 87.42



IRON ROD SET 22
STA 252+91.39
OFF 3.32' L
NORTHING =474737.19
EASTING = 550717.83
ELEVATION = 84.41



IRON ROD SET 23
STA 257+69.04
OFF 1.52' L
NORTHING = 474551.68
EASTING = 551157.99
ELEVATION = 91.54



IRON ROD SET 24
STA 262+71.83
OFF 1.53' R
NORTHING = 474355.35
EASTING = 551620.87
ELEVATION = 97.21

N
NJSPCS
NAD83-2011

DATE	REVISIONS	BY	CHK.

Pennoni
NJ COA NO. GA28033300

RECONSTRUCTION OF
NORTH COUNTY LINE RD, C.R. 526

FROM JACKSON MILLS RD, C.R. 638
TO BENNETTS MILLS RD, C.R. 636

JACKSON TOWNSHIP, OCEAN COUNTY, NJ

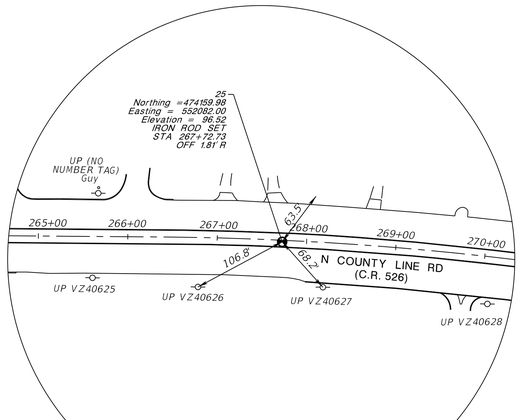
TIES

TIE03 / TIE04

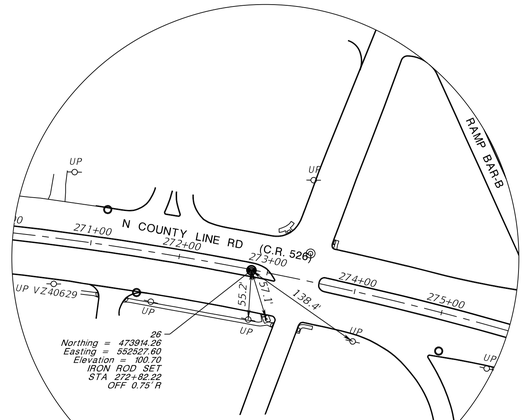
KEITH M. LUDWIG, P.L.S.
PROFESSIONAL LAND SURVEYOR
NJ LICENSE NO. 24GS04324400

FLD.BK.	DATE MARCH 6, 2026	DESIGNED M.D.D.	CHECKED K.M.L.
	SCALE NOT TO SCALE	DRAWN M.D.D.	APPROVED K.M.L.
FILE NO. 12 - 009 - 0128			SHEET 29 OF 83

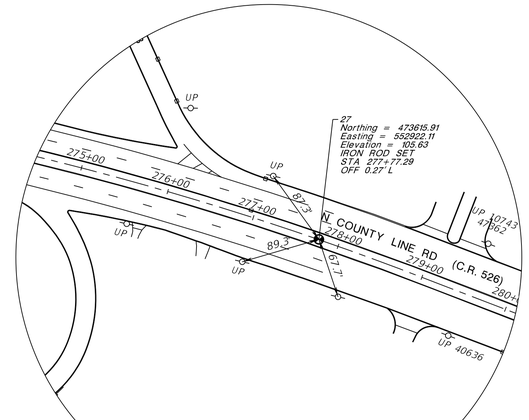
U:\Accurti\CORP\CORPNJ\0 - CP2023 NB Revised & Resurf CR GM\DESIGN_CHEETS\TIE-9.dgn
3/6/2026 11:17AM
Scale: 1 Units: FT



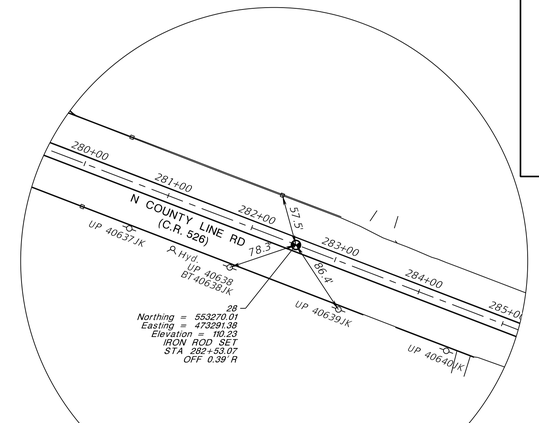
IRON ROD SET 25
STA 267+72.73
OFF 1.81' R
NORTHING =474159.98
EASTING = 552082.00
ELEVATION = 96.52



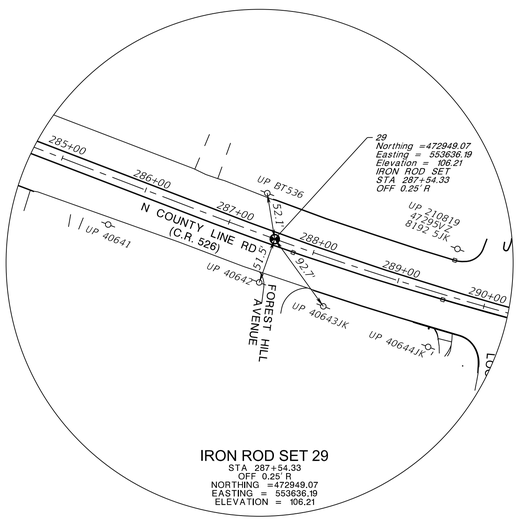
IRON ROD SET 26
STA 272+82.22
OFF 0.75' R
NORTHING = 473914.26
EASTING = 552527.60
ELEVATION = 100.70



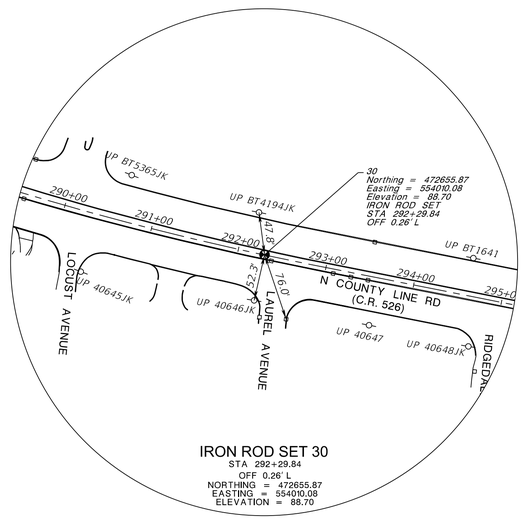
IRON ROD SET 27
STA 277+77.29
OFF 0.27' L
NORTHING = 473615.91
EASTING = 552922.11
ELEVATION = 105.63



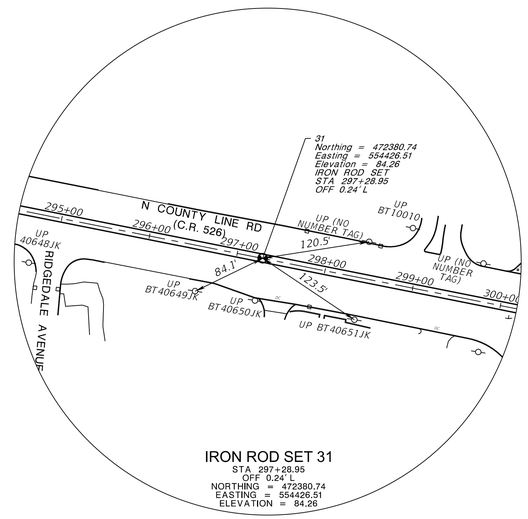
IRON ROD SET 28
STA 282+53.07
OFF 0.39' R
NORTHING = 553270.01
EASTING = 473291.38
ELEVATION = 110.23



IRON ROD SET 29
STA 287+54.33
OFF 0.25' R
NORTHING =472949.07
EASTING = 553636.19
ELEVATION = 106.21



IRON ROD SET 30
STA 292+29.84
OFF 0.26' L
NORTHING = 472655.87
EASTING = 554010.08
ELEVATION = 88.70



IRON ROD SET 31
STA 297+28.95
OFF 0.24' L
NORTHING = 472380.74
EASTING = 554426.51
ELEVATION = 84.26

N
NJSPCS
NAD83-2011

DATE	REVISIONS	BY	CHK.

Pennoni
NJ COA NO. GA28033300

RECONSTRUCTION OF
NORTH COUNTY LINE RD, C.R. 526
FROM JACKSON MILLS RD, C.R. 638
TO BENNETTS MILLS RD, C.R. 636
JACKSON TOWNSHIP, OCEAN COUNTY, NJ

TIES

FLD.BK.	DATE MARCH 6, 2026	DESIGNED M.D.D.	CHECKED K.M.L.
	SCALE NOT TO SCALE	DRAWN M.D.D.	APPROVED K.M.L.
FILE NO. 12 - 009 - 0128			SHEET 30 OF 83

TIE04
TIE04

KEITH M. LUDWIG, P.L.S.
PROFESSIONAL LAND SURVEYOR
NJ LICENSE NO. 24GS04324400

U:\Accurate\CADD\PLOT\PLOT\NCLR\ CP2025-183 Revised & Reissued CR 526\DESIGN_SHEETS\TE-4.dgn
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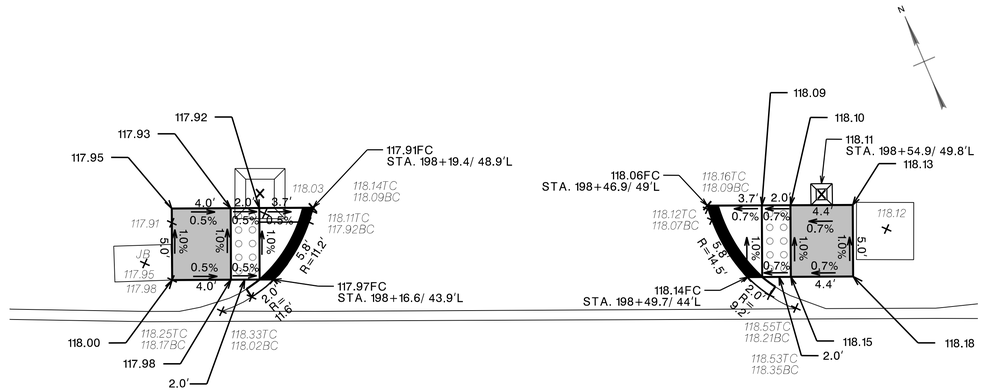
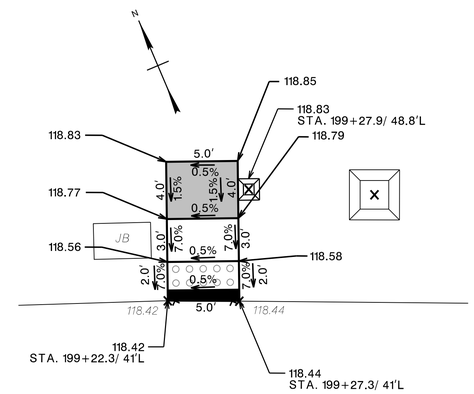
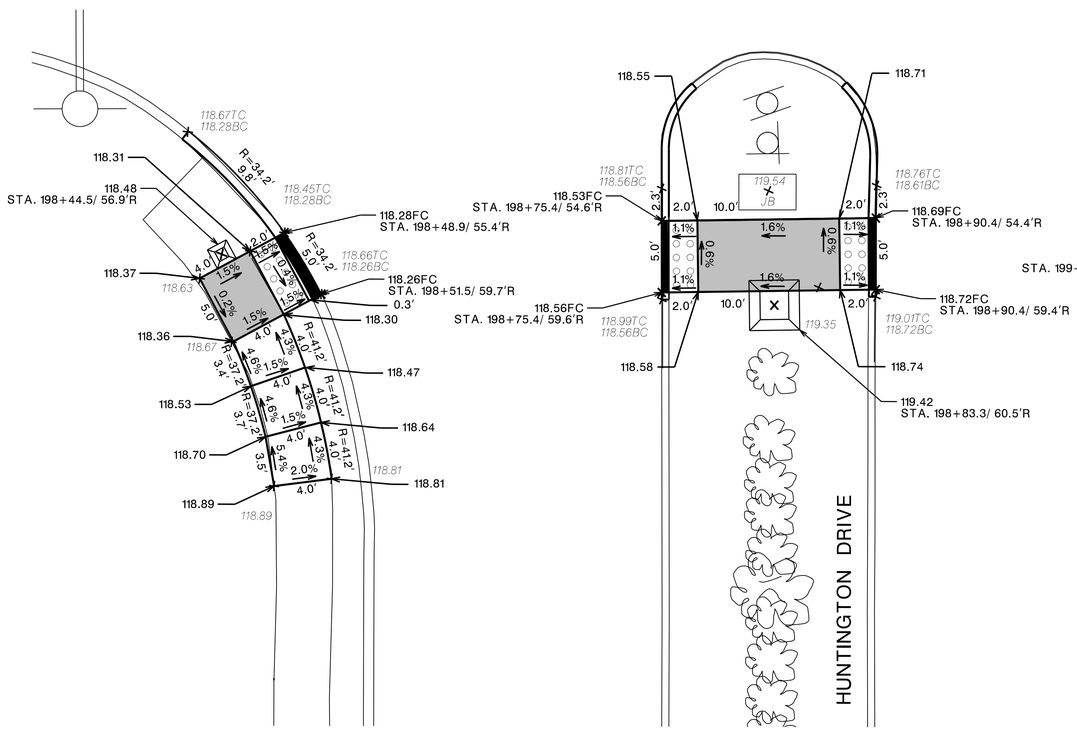
ksmith



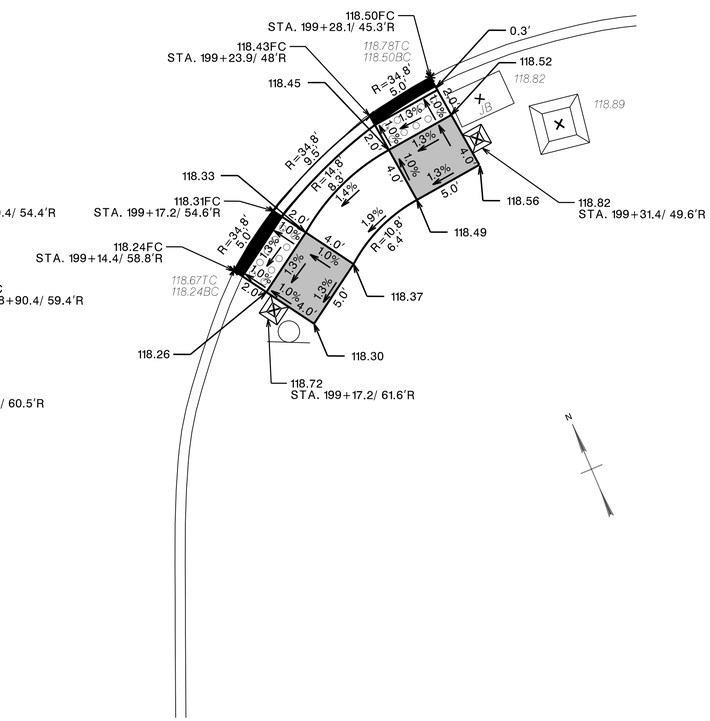
N COUNTY LINE RD
(C.R. 526)
& HUNTINGTON DRIVE
NORTHWEST DRIVEWAY
1"=5'



N COUNTY LINE RD
(C.R. 526)
& HUNTINGTON DRIVE
NORTHEAST CORNER
1"=5'



N COUNTY LINE RD
(C.R. 526)
& HUNTINGTON DRIVE
SOUTHWEST AND
SOUTHEAST CORNERS
1"=5'



NOTES:

1. CONTRACTOR SHALL REMOVE THE EXISTING CURB TO THE NEAREST JOINT OR SAWCUT A MINIMUM DISTANCE OF 5 FEET FROM THE CURB RAMP AND NEW CURB SHALL BE CONSTRUCTED TO MATCH THE LINE AND GRADE OF THE EXISTING CURB.
2. CONTRACTOR SHALL REMOVE THE EXISTING SIDEWALK TO NEAREST JOINT AND NEW SIDEWALK SHALL BE CONSTRUCTED TO MATCH THE GRADE AND LINE OF THE ADJACENT SIDEWALK.
3. CONTRACTOR SHALL ENSURE THAT ALL CURB RAMPS CONSTRUCTED IN THIS CONTRACT SHALL MEET THE ACCESSIBILITY REQUIREMENTS OF THE "AMERICANS WITH DISABILITIES ACT".
4. THE SHOWN SLOPES ARE APPROXIMATED. THE CONTRACTOR SHALL CONFIRM THE SLOPES IN THE FIELD. CONCRETE SHALL NOT BE POURED UNTIL THE FORMS ARE INSPECTED AND APPROVED.
5. THE SHOWN DIMENSIONS ARE APPROXIMATE MEASUREMENTS ALONG THE FACE OF CURB.
6. PLANS DO NOT SHOW ALL TOPOGRAPHIC FEATURES INCLUDING BUT NOT LIMITED TO THE UTILITY POLES, FIRE HYDRANTS, UTILITIES, JUNCTION BOXES, ELECTRICAL CABINETS AND TRAFFIC SIGNALS. CONTRACTOR SHALL INSPECT EACH SITE AND CONFIRM THE LOCATION OF POSSIBLE OBSTRUCTIONS TO THE CURB RAMPS AND CROSSWALKS.
7. PROPOSED FOUNDATIONS AND JUNCTION BOXES TO BE CONSTRUCTED OR RESET SHALL BE SET UTILIZING THE PROPOSED GRADES AS SHOWN IN THE CONSTRUCTION / GRADING PLANS. THE PROPOSED FOUNDATIONS AND JUNCTION BOXES SHALL CONFORM TO THE N.J.D.O.T. DETAILS. ANY FOUNDATION OR JUNCTION BOX THAT IS NOT CONSTRUCTED OR RESET IN ACCORDANCE WITH THE N.J.D.O.T DETAILS WILL BE RESET OR RECONSTRUCTED TO THE PROPER GRADES AT THE CONTRACTOR'S EXPENSE.

LEGEND

- Turning Space, 2% Max Running and Cross Slope
- Detectable Warning Surface
- 51.11FC STA. 61+42.5/ 44.5'R — Proposed Grade And Location
- Proposed Break Line
- TRANSITION TO 6" REVEAL OR PROVIDED GRADES (TYP.) 2' — Proposed Curb
- FLUSH CURB (FC)
- Proposed Controller
- Proposed Traffic Signal Foundation
- JB — Proposed Junction Box
- JB — Reset Junction Box

STATION & OFFSET LOCATION DETAIL
NOT TO SCALE

- X — STATION/OFFSET LOCATION CENTER
- SIGNAL FOUNDATION
- X — JUNCTION BOX
- CONTROLLER

NOTE:
ALL SIGNAL POLE, JUNCTION BOXES, AND CONTROLLER FOUNDATIONS SHALL CONFORM TO NJDOT DETAILS

PLAN
5' 4' 3' 2' 1' 0 5' 10'

STATE OF NEW JERSEY
BETH-ANN M. GRASSO
No. GE 43121
LICENSED PROFESSIONAL ENGINEER
G03

Pennoni
NJ COA NO. GA28033300
BETH-ANN M. GRASSO, P.E.
4/28/2026
PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

DATE	REVISIONS	BY	CHK.

RECONSTRUCTION OF
NORTH COUNTY LINE RD
FROM JACKSON MILLS RD
TO BENNETTS MILLS RD
JACKSON TOWNSHIP

GRADING PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE 1"=5'	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 31 OF 84

H:\Projects\OC\12-009\03 - 60% Set\Phase 1 Road\CR 526\526CR_SHEETS\04.dgn
Plot Date: 4/28/2026 Plot By: CO-CA-FAL-PP_ym317.pltcfg
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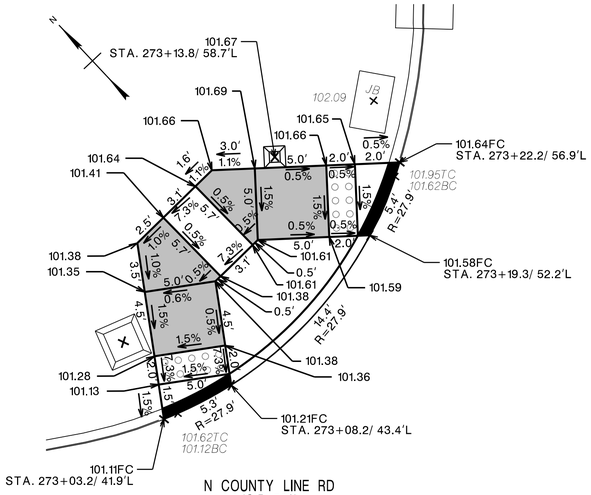
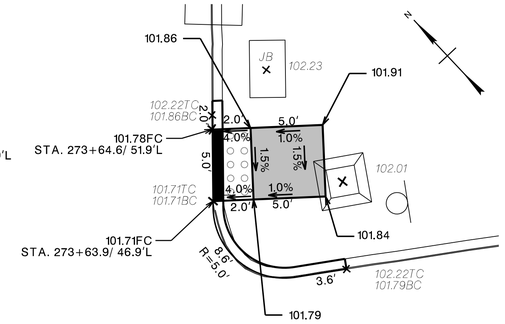
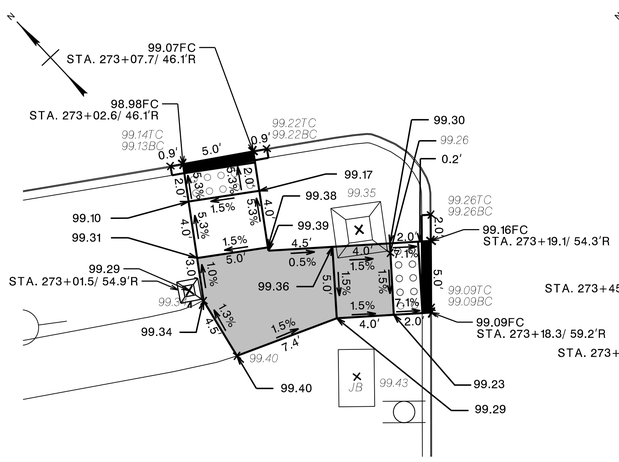


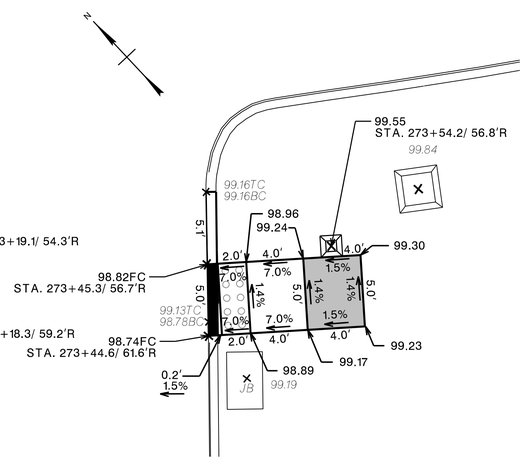
N COUNTY LINE RD
(C.R. 526)
& BARTLEY RD
NORTHWEST CORNER
1"=5'



N COUNTY LINE RD
(C.R. 526)
& BARTLEY RD
NORTHEAST CORNER
1"=5'



N COUNTY LINE RD
(C.R. 526)
& BARTLEY RD
SOUTHWEST CORNER
1"=5'



N COUNTY LINE RD
(C.R. 526)
& BARTLEY RD
SOUTHEAST CORNER
1"=5'

NOTES:

1. CONTRACTOR SHALL REMOVE THE EXISTING CURB TO THE NEAREST JOINT OR SAWCUT A MINIMUM DISTANCE OF 5 FEET FROM THE CURB RAMP AND NEW CURB SHALL BE CONSTRUCTED TO MATCH THE LINE AND GRADE OF THE EXISTING CURB.
2. CONTRACTOR SHALL REMOVE THE EXISTING SIDEWALK TO NEAREST JOINT AND NEW SIDEWALK SHALL BE CONSTRUCTED TO MATCH THE GRADE AND LINE OF THE ADJACENT SIDEWALK.
3. CONTRACTOR SHALL ENSURE THAT ALL CURB RAMPS CONSTRUCTED IN THIS CONTRACT SHALL MEET THE ACCESSIBILITY REQUIREMENTS OF THE "AMERICANS WITH DISABILITIES ACT".
4. THE SHOWN SLOPES ARE APPROXIMATED. THE CONTRACTOR SHALL CONFIRM THE SLOPES IN THE FIELD. CONCRETE SHALL NOT BE POURED UNTIL THE FORMS ARE INSPECTED AND APPROVED.
5. THE SHOWN DIMENSIONS ARE APPROXIMATE MEASUREMENTS ALONG THE FACE OF CURB.
6. PLANS DO NOT SHOW ALL TOPOGRAPHIC FEATURES INCLUDING BUT NOT LIMITED TO THE UTILITY POLES, FIRE HYDRANTS, UTILITIES, JUNCTION BOXES, ELECTRICAL CABINETS AND TRAFFIC SIGNALS. CONTRACTOR SHALL INSPECT EACH SITE AND CONFIRM THE LOCATION OF POSSIBLE OBSTRUCTIONS TO THE CURB RAMPS AND CROSSWALKS.
7. PROPOSED FOUNDATIONS AND JUNCTION BOXES TO BE CONSTRUCTED OR RESET SHALL BE SET UTILIZING THE PROPOSED GRADES AS SHOWN IN THE CONSTRUCTION / GRADING PLANS. THE PROPOSED FOUNDATIONS AND JUNCTION BOXES SHALL CONFORM TO THE N.J.D.O.T. DETAILS. ANY FOUNDATION OR JUNCTION BOX THAT IS NOT CONSTRUCTED OR RESET IN ACCORDANCE WITH THE N.J.D.O.T DETAILS WILL BE RESET OR RECONSTRUCTED TO THE PROPER GRADES AT THE CONTRACTOR'S EXPENSE.

LEGEND

- Turning Space, 2% Max Running and Cross Slope
- Detectable Warning Surface
- 51.11FC STA. 61+42.5/ 44.5'R — Proposed Grade And Location
- Proposed Break Line
- TRANSITION TO 6" REVEAL OR PROVIDED GRADES (TYP.) 2' — Proposed Curb
- FLUSH CURB (FC)
- Proposed Controller
- Proposed Traffic Signal Foundation
- JB — Proposed Junction Box
- JB — Reset Junction Box

STATION & OFFSET LOCATION DETAIL
NOT TO SCALE

- STATION/OFFSET LOCATION CENTER
- SIGNAL FOUNDATION
- JUNCTION BOX
- CONTROLLER

NOTE:
ALL SIGNAL POLE, JUNCTION BOXES, AND CONTROLLER FOUNDATIONS SHALL CONFORM TO NJDOT DETAILS

PLAN

5' 4' 3' 2' 1' 0 5' 10'

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300
BETH-ANN M. GRASSO, P.E.
4/28/2026
PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

STATE OF NEW JERSEY
BETH-ANN M. GRASSO
No. GE 43121
LICENSED PROFESSIONAL ENGINEER
G03

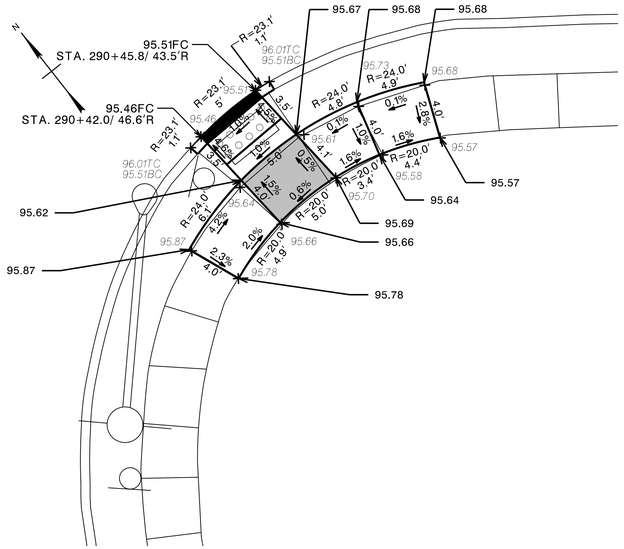
OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF
NORTH COUNTY LINE RD
FROM JACKSON MILLS RD
TO BENNETTS MILLS RD
JACKSON TOWNSHIP

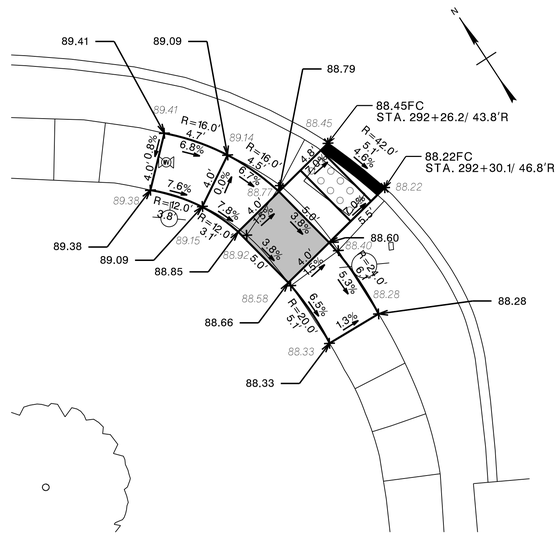
GRADING PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE 1"=5'	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 32 OF 84

J:\Accounts\OCEAN\COUNTY\12-009-0128 Reconst N. Bayville Rd\DWG\PLAN\12-009-0128-GRADING-PLAN-FILL.dwg
RENDER: GRADING PLAN
PLOTTED: 4/28/2026 PM



N COUNTY LINE RD
(C.R. 526)
& LOCUST AVENUE
SOUTHEAST CORNER
1"=5'



N COUNTY LINE RD
(C.R. 526)
& LAUREL AVENUE
SOUTHWEST CORNER
1"=5'

LEGEND

Turning Space, 2% Max Running and Cross Slope	TRANSITION TO 6" REVEAL OR PROVIDED GRADES (TYP.) 2'
Detectable Warning Surface	Proposed Curb FLUSH CURB (FC)
51.11FC STA. 61+42.5/ 44.5'R — Proposed Grade And Location	Proposed Controller
Proposed Break Line	Proposed Traffic Signal Foundation
	JB — Proposed Junction Box
	RJB — Reset Junction Box

NOTES:

1. Contractor shall remove the existing curb to the nearest joint or sawcut to a minimum distance of 5 feet from the curb ramp and new curb shall be constructed to match the line and grade of the existing curb.

2. Contractor shall remove the existing sidewalk to nearest joint and new sidewalk shall be constructed to match the grade and line of the adjacent sidewalk.

3. Contractor shall ensure that all curb ramps constructed in this contract shall meet the accessability requirements of the "Americans with Disabilities Act".

PLAN

5' 4' 3' 2' 1' 0 5' 10'

4. Plans do not show all topographic features including but not limited to the utility poles, fire hydrants, utilities, junction boxes, utility cabinets and traffic signals. Contractor shall inspect each site and confirm the location of possible obstructions to the curb ramps and crosswalks.

5. The shown pipes are approximated. The contractor shall confirm the slopes in the field. Concrete shall not be poured until the forms are inspected and approved.

6. The shown dimensions are approximate measurements along the face of curb.

STATE OF NEW JERSEY
BETH-ANN M. GRASSO
No. GE 43121
LICENSED PROFESSIONAL ENGINEER

G03

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

DATE	REVISIONS	BY	CHK.

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

GRADING PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE 1"=5'	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 33 OF 84

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LEGEND

- BREAKAWAY BARRICADES
- BREAKAWAY BARRICADES WITH SIGN
- CONSTRUCTION SIGNS
- DRUMS
- CONE
- CONSTRUCTION BARRIER CURB (TYPE SPECIFIED)
- DIRECTION OF TRAFFIC FLOW
- TRAFFIC DIRECTOR, FLAGGER
- TRAILER MOUNTED MOUNTED ARROW BOARD SHOWING CAUTION MODE
- PORTABLE VARIABLE MESSAGE SIGN (VMS)
- ILLUMINATED FLASHING ARROW MOUNTED ON TOWING VEHICLE SHOWING ARROW PATTERN (LEFT, RIGHT, BOTH) — LEFT RIGHT BOTH
- TRAFFIC CONTROL TRUCK WITH MOUNTED CRASH CUSHION AND ARROW BOARD SHOWING CAUTION MODE
- TRAFFIC CONTROL TRUCK WITH MOUNTED CRASH CUSHION AND ARROW BOARD SHOWING ARROW PATTERN (LEFT, RIGHT, BOTH) — LEFT RIGHT BOTH
- TEMPORARY CRASH CUSHION, INERTIAL BARRIER SYSTEM
- TEMPORARY CRASH CUSHION, (ALL OTHER APPROVED)
- BUFFER ZONE
- WORK AREA
- PAINT STRIPING TRUCK OR OTHER OPERATING VEHICLE

TRAFFIC CONTROL NOTES:

1. PRIOR TO COMMENCING ANY ROAD CONSTRUCTION, ALL TRAFFIC CONTROL SIGNS AND DEVICES SHALL BE INSTALLED.

2. SIDE STREET AND DRIVEWAY ACCESS TO BE MAINTAINED AT ALL TIMES.

3. WORKING HOURS ARE TO BE IN ACCORDANCE WITH THE SPECIFICATIONS, AND AS NOTED BELOW.

4. THE UTILITY COMPANIES SHALL RELOCATE ALL NECESSARY FACILITIES PRIOR TO CONSTRUCTION.

5. TRAFFIC PATTERNS PERMITTED AS NOTED BELOW:
   (NJDOT TCD-1) LEGEND AND GENERAL NOTES
   (NJDOT TCD-3) TWO LANES, UNDIVIDED, LANE AND SHOULDER CLOSING
   (NJDOT TCD-3) TWO LANES, UNDIVIDED, SHOULDER CLOSING
   (NJDOT TCD-7) TWO LANES, UNDIVIDED, INTERSECTION
   (NJDOT TCD-8) FOUR LANES, UNDIVIDED, RIGHT LANE AND SHOULDER CLOSING
   (NJDOT TCD-8) FOUR LANES, UNDIVIDED, SHOULDER CLOSING (CENTER/LEFT LANE CLOSING)
   (NJDOT TCD-9) FOUR LANES, UNDIVIDED, LEFT LANE CLOSING
   (NJDOT TCD-10) FOUR LANES, UNDIVIDED, 2 LANES AND SHOULDER ONE DIRECTION CLOSING
   (NJDOT TCD-11) FOUR LANES, UNDIVIDED, INTERSECTION
   (NJDOT TCD-12) FOUR LANES, UNDIVIDED, INTERSECTION
   (NJDOT TCD-13) FOUR LANES, UNDIVIDED, INTERSECTION
   (NJDOT TCD-14) 4 & 6 LANES, DIVIDED, RIGHT LANE AND SHOULDER CLOSING
   (NJDOT TCD-14) 4 & 6 LANES, DIVIDED, SHOULDER CLOSING
   (NJDOT TCD-15) 4 & 6 LANES, DIVIDED, LEFT LANE CLOSING
   (NJDOT TCD-16) 6 LANES, DIVIDED, LEFT TWO LANES CLOSING
   (NJDOT TCD-16) 6 LANES, DIVIDED, RIGHT TWO LANES CLOSING
   (NJDOT TCD-18) DIVIDED, EXIT RAMP CONSTRUCTION LEFT
   (NJDOT TCD-18) DIVIDED, EXIT RAMP CONSTRUCTION RIGHT
   (NJDOT TCD-19) DIVIDED EXIT RAMP CONSTRUCTION, LEFT WITH DECELERATION LANE
   (NJDOT TCD-19) DIVIDED EXIT RAMP CONSTRUCTION, RIGHT WITH DECELERATION LANE
   (NJDOT TCD-20) DIVIDED, ENTRANCE RAMP CONSTRUCTION, LEFT
   (NJDOT TCD-20) DIVIDED, ENTRANCE RAMP CONSTRUCTION, RIGHT
   (NJDOT TCD-21) DIVIDED, ENTRANCE RAMP CONSTRUCTION, LEFT WITH ACCELERATION LANE
   (NJDOT TCD-21) DIVIDED, ENTRANCE RAMP CONSTRUCTION, RIGHT WITH ACCELERATION LANE
   (NJDOT TCD-22) MULTI-LANE ROAD MOVING OPERATION
   (OC TCD **) PAINT STRIPING MOVING OPERATION

GENERAL NOTES:

1. ADVANCE WARNING SIGNS DISTANCES AND TAPER LENGTHS MAY BE EXTENDED, AT DIRECTION OF THE DEPARTMENT, TO ADJUST FOR REDUCED VISIBILITY DUE TO HORIZONTAL AND VERTICAL CURVATURE OF THE ROADWAY.

2. THE APPROXIMATE LOCATIONS OF THE ILLUMINATED FLASHING ARROW BOARDS ARE SHOWN ON THE TRAFFIC CONTROL PLANS. THESE LOCATIONS MAY BE MODIFIED AS APPROVED BY RE TO ADJUST FOR VISIBILITY DUE TO HORIZONTAL OR VERTICAL CURVATURE OF THE ROADWAY OR TO POSITION AT A SAFER LOCATION. ILLUMINATED FLASHING ARROW BOARDS ARE TO BE USED FOR TEMPORARY LANE CLOSINGS AND AT LOCATIONS SHOWN ON THE TRAFFIC CONTROL PLANS.

3. PRIOR TO ANY ROAD CONSTRUCTION, TRAFFIC CONTROL SIGNS AND DEVICES ARE TO BE IN PLACE.

4. RAMPS AND/OR SIDE STREETS ENTERING THE ROADWAY AFTER THE FIRST ADVANCE WARNING SIGN ARE TO BE PROVIDED WITH AT LEAST ONE W20-IF SIGN (ROAD WORK AHEAD) AS A MINIMUM.

5. ALL EXISTING ROAD SIGNS, PAVEMENT MARKINGS, AND / OR PLOWABLE PAVEMENT REFLECTORS WHICH CONFLICT WITH THE PROPOSED TRAFFIC CONTROL PLAN ARE TO BE COVERED, REMOVED, OR RELOCATED AS DIRECTED BY THE RE.

6. CONFLICTING OR NON-OPERATING SIGNAL INDICATIONS ON EITHER THE EXISTING, TEMPORARY,OR PROPOSED TRAFFIC SIGNAL SYSTEMS ARE TO BE BAGGED OR COVERED.

7. MAINTENANCE AND PROTECTION OF TRAFFIC TO BE IN ACCORDANCE WITH THE CURRENT MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES - PART VI "STANDARDS AND GUIDES FOR TRAFFIC CONTROL FOR STREET AND HIGHWAY CONSTRUCTION, MAINTENANCE, UTILITY, AND INCIDENT MANAGEMENT OPERATIONS", UNLESS OTHERWISE NOTED IN THE PLANS AND SPECIFICATIONS.

8. CONSTRUCTION SIGN W99-2 (GIVE US A BRAKE) TO BE LOCATED 200 FEET IN ADVANCE OF PROJECT LIMITS.

9. A W1-6 (ARROW) SIGN MOUNTED ON A BREAKAWAY BARRICADE AND CENTERED ON THE CLOSED WIDTH TO BE LOCATED 100 FEET BEYOND EACH INTERSECTION OR MAIN ACCESS POINT WITHIN THE AREA OF A LANE OR SHOULDER CLOSURE.

10. CONSTRUCTION SIGNS R11-4 (ROAD CLOSED TO THRU TRAFFIC) TO BE PLACED AT THE INTERSECTING STREETS WHICH ARE CLOSED TO TRAFFIC BECAUSE OF CONSTRUCTION.

11. CONSTRUCTION SIGNS W8-9A (SYMBOL FOR UNEVEN PAVEMENT) AND W8-14A (GROOVED PAVEMENT) TO BE USED WHEN SUCH PAVEMENT CONDITIONS EXIST. THE PLACEMENT OF THESE SIGNS TO BE AS DIRECTED BY THE RE.

12. MOVING WORK AREAS IN A LANE CLOSURE REQUIRE A TRAILER MOUNTED ILLUMINATED FLASHING ARROW TO REMAIN AT THE END OF THE TAPER, THE TRAFFIC CONTROL TRUCK WITH MOUNTED CRASH CUSHION THAT IS TO MOVE WITH THE WORK AREAS TO KEEP A 70 FEET MIN. AND 150 FEET MAX. BUFFER IN ADVANCE OF EACH WORK AREA.

13. THE CONTRACTOR TO SUBMIT A PLAN FOR THE SAFE ACCESS OF CONSTRUCTION VEHICLES THROUGHOUT THE WORK SITE WHERE SPACE CONSTRAINTS PREVENT THE USE OF LANE CLOSURES. THE PLAN TO BE SUBMITTED TO THE RE AS SPECIFIED IN THE SPECIFICATIONS.

14. BACKFILL ALL EXCAVATED AREAS WITHIN OR ADJACENT TO THE ROADWAY AND PLACE ON AT LEAST 6H:1V SLOPE BEFORE THE END OF EACH WORK DAY. OTHER EXCAVATED AREA WITHIN THE CLEAR ZONE ARE TO BE BACKFILLED.

15. WHERE REQUIRED, THE CONTRACTOR IS TO MAKE PROVISIONS FOR MAINTAINING PEDESTRIAN CROSSING LOCATIONS AND TYPE AS DIRECTED BY THE RE.

16. BITUMINOUS CONCRETE PLACED DURING THE VARIOUS CONSTRUCTION STAGES TO BE TRANSITIONED ON A MINIMUM 20H:1V SLOPE TO MEET THE ADJACENT EXISTING GRADE AT THE LONGITUDINAL AND TRANSVERSE LIMITS OF THE STAGE CONSTRUCTION AREAS UNLESS OTHERWISE NOTED ON THE STAGE CONSTRUCTION PLANS.

17. THE PLACEMENT AND / OR RELOCATION OF CONSTRUCTION BARRIER CURB TO BE DONE DURING APPROVED OFF-PEAK HOURS WHEN TRAFFIC MAY BE REDUCED TO ONE LANE IN EACH DIRECTION.

18. CONSTRUCTION ZONE SPEED LIMIT WILL BE DETERMINED BY THE COUNTY ENGINEER, AT THE TIME OF OR DURING CONSTRUCTION, AS REQUESTED BY THE RE.

19. THE SPEED LIMIT, R2-1 (BLACK ON WHITE) WITH ADDED WORK ZONE PLATE (BLACK ON ORANGE) SIGNS TO BE LOCATED THROUGH WORK AREAS AS DIRECTED BY THE COUNTY ENGINEER.

20. THE REDUCED SPEED AHEAD SIGN, W3-5(S) (BLACK ON ORANGE) TO BE LOCATED IN ADVANCE OF SPEED LIMIT R2-1 SIGNS WHICH REDUCE THE NORMAL POSTED SPEED LIMIT THROUGH THE CONSTRUCTION ZONE.

21. TRAFFIC FINES DOUBLED IN WORK AREA R(NJ)5-17(S) , 4 FEET BY 2.5 FEET SIGN TO BE LOCATED 500 FEET AFTER THE FIRST ADVANCE WARNING SIGN, (W20 SERIES) AT EACH WORK AREA LOCATED WITHIN URBAN AREAS. THIS SIGN TO ALSO BE USED ON PROJECTS REQUIRING MOVING OPERATIONS IN WHICH CASE THE SIGN IS TO BE MOUNTED ON A SLOW MOVING CONSTRUCTION VEHICLE.

22. DO NOT CONSTRUCT THE FINAL HMA SURFACE PAVEMENT UNTIL THE FINAL STAGE OF THE PROJECT UNLESS OTHERWISE DIRECTED BY THE RE OR INDICATED ON THE PLANS. SET MANHOLES AND INLETS TO FINISHED GRADE AND CONSTRUCT TEMPORARY PAVEMENT RAMPS AROUND THEM WITH A MINIMUM 20H:1V SLOPE IN ALL DIRECTIONS USING HOT MIX ASPHALT PAVEMENT. THIS TEMPORARY MATERIAL WILL BE REMOVED IMMEDIATELY PRIOR TO PLACING THE SURFACE COURSE.

23. PLACE TRAFFIC CONTROL DEVICES FOR LANE CLOSURES INCLUDING SIGNS, CONES, BARRICADES, ETC. AS SHOWN ON PLANS. NO SIGNS ARE TO BE PLACED WITHOUT ACTUAL LANE CLOSURES AND REMOVE IMMEDIATELY UPON REMOVAL OF THE CLOSURES.

24. CONES MAY BE SUBSTITUTED FOR DRUMS AND INSTALLED UPON THE APPROVAL OF THE RE.

25. TRAFFIC IMPACT NOTICES AND CHANGES

    A. TERMS:
    WHEN THE FOLLOWING TERMS ARE USED, THE INTENT AND MEANING IS AS FOLLOWS:

    i. IMPACTS TO NORMAL TRAFFIC FLOW - WORK THAT REQUIRES A PORTION OF THE PAVED ROADWAY BEING BLOCKED OR CLOSED WITH SAFETY DEVICES OR VEHICLES, INCLUDING, BUT NOT LIMITED TO, FULL OR PARTIAL LANE CLOSURES, FULL OR PARTIAL RAMP CLOSURES, SHOULDER CLOSURES, MOVING OPERATIONS SUCH AS TRAFFIC STRIPING OR SWEEPING, LANE SHIFTS, OR ALTERNATING TRAFFIC. THIS APPLIES EVEN WHEN DETOURS ARE PROVIDED.

    ii. TEMPORARY LANE CLOSURES - WORK DESCRIBED UNDER "IMPACTS TO NORMAL TRAFFIC FLOW" WHICH IS ROUTINELY SET UP AND REMOVED ON A DAILY BASIS.

    iii. PERMANENT LANE CLOSURES - WORK DESCRIBED UNDER "IMPACTS TO NORMAL TRAFFIC FLOW" WHICH REMAINS IN PLACE CONTINUOUSLY FOR 24 HOURS OR MORE.

    B. ADVANCE NOTICES

    FOR THE INITIAL START OF WORK THAT REQUIRES "IMPACTS TO NORMAL TRAFFIC FLOW", THE CONTRACTOR IS TO NOTIFY THE RE IN WRITING, ON THE ADVANCE FORM TO-103 PROVIDED BY THE DEPARTMENT, OF THE PROPOSED DATE. THE NOTICE IS TO BE SUBMITTED AT LEAST TWENTY-EIGHT CALENDAR DAYS, BUT NOT MORE THAN SIXTY CALENDAR DAYS, BEFORE THE PROPOSED DATE. START OF WORK THAT IMPACTS NORMAL TRAFFIC FLOW WILL NOT BE PERMITTED PRIOR TO THE DATE STATED IN THE NOTICE. THE CONTRACTOR IS TO CONFIRM, IN WRITING TO THE RE, THE PROPOSED DATE SEVEN (AND/OR FOURTEEN) CALENDAR DAYS BEFORE STARTING THE ESTABLISHMENT OF THE TRAFFIC CONTROL MEASURES FOR THE TRAFFIC IMPACT. THE CONTRACTOR IS TO IMMEDIATELY NOTIFY THE RE IF THE PROPOSED ESTABLISHMENT CANNOT BE COMPLETED ON THE PROPOSED DATE.

    FOR A "PERMANENT LANE CLOSURE", THE CONTRACTOR IS TO NOTIFY THE RE IN WRITING, ON ADVANCE FORM TO-103, OF THE PROPOSED DATE A NEW TRAFFIC PATTERN WILL BE ESTABLISHED. THE NOTICE IS TO BE SUBMITTED AT LEAST TWENTY-EIGHT CALENDAR DAYS, BUT NOT MORE THAN SIXTY CALENDAR DAYS, IN ADVANCE OF THE PROPOSED DATE. START OF A NEW TRAFFIC PATTERN WILL NOT BE PERMITTED PRIOR TO THE DATE STATED IN THE NOTICE. THE CONTRACTOR IS TO CONFIRM, IN WRITING TO THE RE, THE PROPOSED DATE OF THE NEW TRAFFIC PATTERN SEVEN (AND/OR FOURTEEN) DAYS BEFORE STARTING TRAFFIC CONTROL MEASURES FOR THE ESTABLISHMENT OF THE NEW PATTERN. THE CONTRACTOR IS TO IMMEDIATELY NOTIFY THE RE IF THE PROPOSED ESTABLISHMENT CANNOT BE COMPLETED ON THE PROPOSED DATE.

    STARTING THE ESTABLISHMENT OF A NEW PERMANENT TRAFFIC PATTERN IS TO BEGIN NO EARLIER THAN 11:00 PM FRIDAY AND BE COMPLETED AND READY FOR OPERATIONS BY 6:00 PM THE FOLLOWING SUNDAY. THE ESTABLISHMENT IS TO BE COMPLETED IN ACCORDANCE WITH THE LANE CLOSURE HOURS SPECIFIED IN THE CONTRACT.

    ADVANCE NOTICES SENT PRIOR TO THE PRE-CONSTRUCTION MEETING ARE TO BE ADDRESSED TO THE CONTACT PERSON AS SPECIFIED IN SUBSECTION 101.04 OF THE SPECIAL PROVISIONS.

    C. PROGRESS NOTICES

    ALL "IMPACTS TO NORMAL TRAFFIC FLOW" SCHEDULED FOR THE SEVEN DAY PERIOD STARTING ON THE FOLLOWING MONDAY ARE TO BE SUBMITTED TO THE RE BY 9:00 AM OF EACH FRIDAY ON WEEKLY FORM TO-100 PROVIDED BY THE DEPARTMENT.

    EACH DAY OF "TEMPORARY LANE CLOSURES" ARE TO BE SUBMITTED TO THE RE BY 9:00 AM THE DAY IN ADVANCE OF THE START OF THOSE OPERATIONS ON DAILY FORM TO-101 PROVIDED BY THE DEPARTMENT.

    "TEMPORARY LANE CLOSURES" FOR WEEKENDS ARE TO BE SUBMITTED TO THE RE BY 9:00 AM ON THE IMMEDIATELY PRECEDING FRIDAY ON THE DAILY FORM TO-101 PROVIDED BY THE DEPARTMENT.

    D. CHANGES TO THE SCHEDULED CLOSURES
    REQUEST FOR A CHANGE TO THE TRAFFIC CONTROL REQUIREMENTS IN THE CONTRACT DOCUMENTS ARE TO BE SUBMITTED IN WRITING TO THE RE AS FOLLOWS:

    CHANGES TO THE SCHEDULED HOURS FOR "TEMPORARY LANE CLOSURES" ARE TO BE SUBMITTED TO THE RE AT LEAST EIGHT CALENDAR DAYS IN ADVANCE OF WHEN THE CHANGE IS PROPOSED TO START.

    OTHER PROPOSED CHANGES TO "TEMPORARY LANE CLOSURES" AND ALL CHANGES TO "PERMANENT LANE CLOSURES" ARE TO BE SUBMITTED TO THE RE AS SPECIFIED IN THE SPECIFICATIONS.

26. WHERE FINAL HMA PAVING IS PERFORMED AND THE LANE IS TO BE RE-OPENED TO TRAFFIC AND THE ITEM TRAFFIC STRIPES IS UNABLE TO BE APPLIED, APPLY THE ITEM TRAFFIC STRIPES, LATEX. ENSURE THAT THE ITEM TRAFFIC STRIPES IS APPLIED WITHIN 14 DAYS.

WORK HOURS

- WORK WHICH WILL INTERFERE WITH TRAFFIC OR RESTRICT THE WIDTH OF TRAVELED WAY AVAILABLE FOR TRAFFIC SHALL NOT BE PERFORMED ON SATURDAYS, SUNDAYS, OR LEGAL HOLIDAYS.
- DAYTIME WORK HOURS: MONDAY - FRIDAY 9:00 AM - 5:00 PM
- NIGHTTIME WORK HOURS: SUNDAY - THURSDAY 9:00 PM - 5:00 AM
- THERE SHALL BE NO DEVIATION TO THE WORK DAYS AND HOURS LISTED ON THIS PLAN WITHOUT PRIOR APPROVAL FROM THE COUNTY ENGINEER.
- THERE SHALL BE NO CONSTRUCTION ACTIVITY LATER THAN 5:00 P.M. ON ANY GIVEN FRIDAY WITHOUT PRIOR APPROVAL FROM THE COUNTY ENGINEER.
- THERE SHALL BE NO DETOURS UNLESS APPROVED BY THE OCEAN COUNTY ENGINEER.

DATE	REVISIONS	BY	CHK.

STATE OF NEW JERSEY · BETH-ANN M. GRASSO · No. GE 43121 · LICENSED · PROFESSIONAL ENGINEER

TCD1 / TCD2

Pennoni
NJ COA NO. GA28033300
BETH-ANN M. GRASSO, P.E.
4/28/2026
PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF
NORTH COUNTY LINE RD
FROM JACKSON MILLS RD
TO BENNETTS MILLS RD
JACKSON TOWNSHIP

TRAFFIC CONTROL DETAILS

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE NOT TO SCALE	DRAWN C.D.	APPROVED B.M.G.
FILE NO.	12 - 009 - 0128		SHEET 34 OF 84

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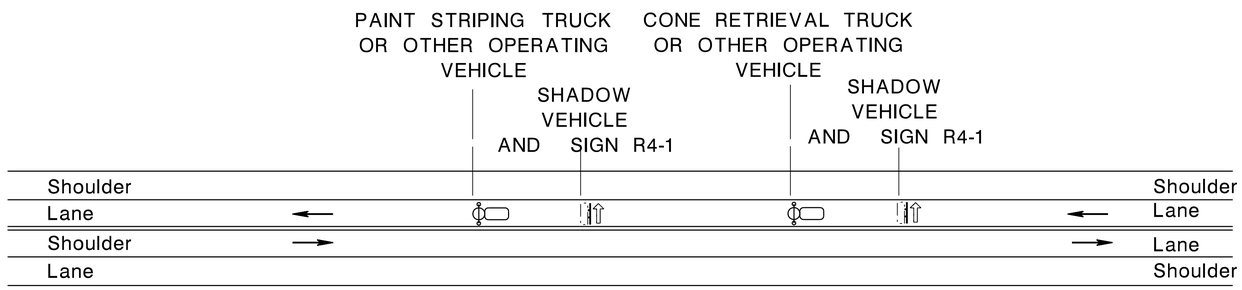

PAINT STRIPING MOVING OPERATION

NOTE:

1. SHADOW VEHICLE SHALL MAINTAIN A DISTANCE OF 70 FEET MINIMUM TO A MAXIMUM OF 150 FEET BEHIND THE OPERATING VEHICLE.

2. REFERENCE MUTCD FIGURE 6H-17 TYPICAL APPLICATION 17, MOBILE OPERATIONS ON A TWO-LANE ROAD.

SIGN LEGEND

LEGEND

"W" IS THE WIDTH OF LANE CLOSURE IN FEET
"L" IS THE LENGTH OF TAPER
"#" IS NUMBER OF CONES IN TAPER AT 40' SPACING

LENGTH OF TAPER CHART FOR MOVING OPERATIONS

W	25 M.P.H.		30 M.P.H.		35 M.P.H.		40 M.P.H.		45 M.P.H.		50 M.P.H.		55 M.P.H.	
	L	#	L	#	L	#	L	#	L	#	L	#	L	#
1	10	2	15	2	20	2	30	2	45	3	50	3	55	3
2	25	2	30	2	45	3	55	3	90	4	100	4	110	4
3	35	2	45	3	65	3	80	3	135	5	150	5	165	5
4	45	3	60	3	85	4	110	4	180	6	200	6	220	7
5	55	3	75	3	105	4	135	5	225	7	250	7	275	8
6	65	3	90	4	125	5	160	5	270	8	300	9	330	9
10	105	4	150	5	205	6	270	8	450	13	500	14	550	15
11	115	4	165	5	225	7	295	9	495	14	550	15	605	16
12	125	5	180	6	245	7	320	9	540	15	600	16	660	18

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300
BETH-ANN M. GRASSO, P.E.
4/28/2026
PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

STATE OF NEW JERSEY
BETH-ANN M. GRASSO
No. GE 43121
LICENSED PROFESSIONAL ENGINEER

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF
NORTH COUNTY LINE RD
FROM JACKSON MILLS RD
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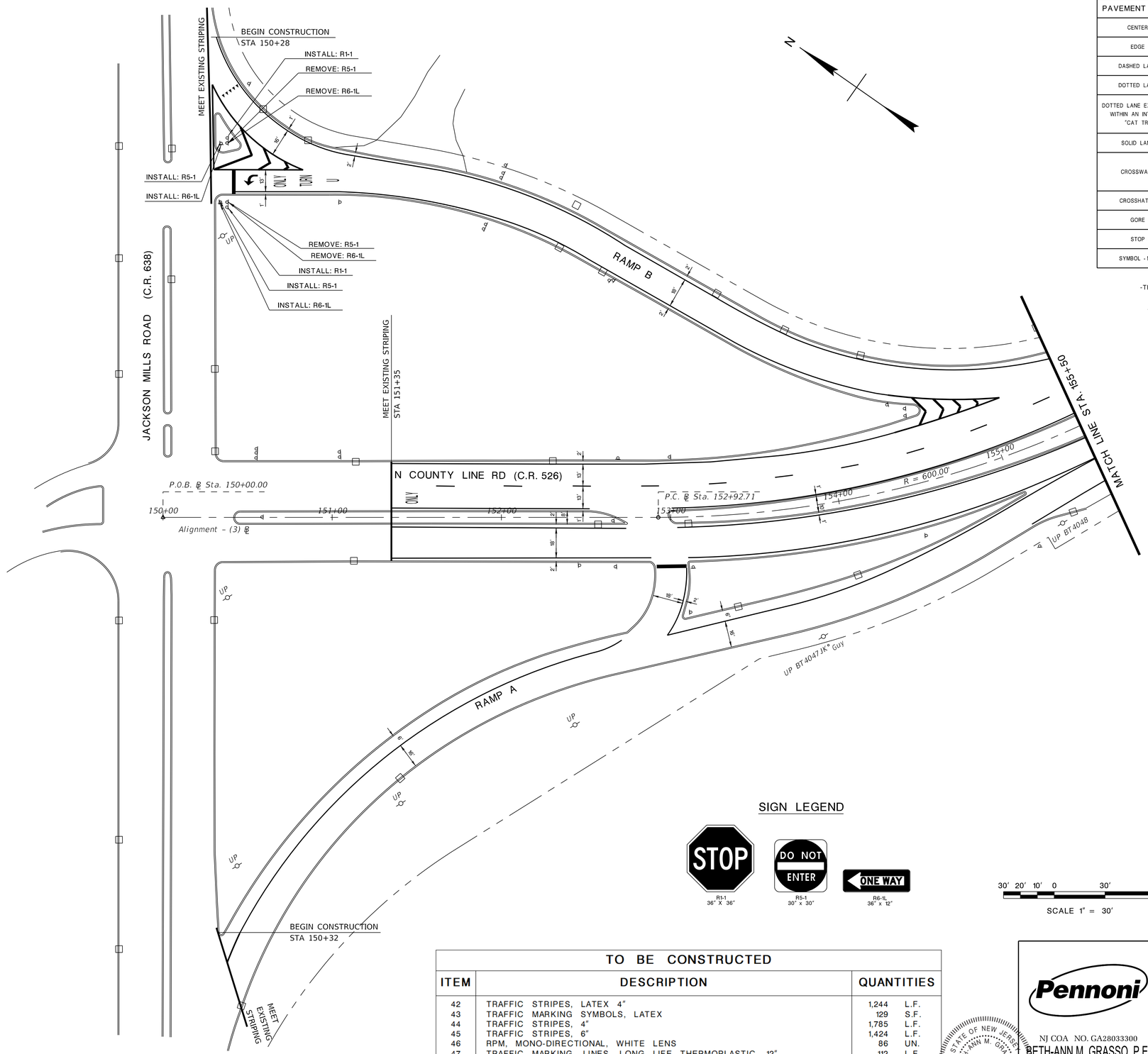
TCD2



*PAVEMENT MARKING SCHEDULE

PAVEMENT MARKING	MATERIAL	DESCRIPTION	CONTRACT ITEM
CENTER LINE	EPOXY RESIN	4" DASHED 10' LINE WITH 30' GAPS OR SOLID YELLOW	610003M - TRAFFIC STRIPES, 4" (LF)
EDGE LINE	EPOXY RESIN	4" YELLOW OR 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DASHED LANE LINE	EPOXY RESIN	10' LINE WITH 30' GAPS, 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DOTTED LANE LINE	THERMOPLASTIC	3' LINE WITH 9' GAPS, 8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
DOTTED LANE EXTENSION LINE WITHIN AN INTERSECTION "CAT TRACKS"	THERMOPLASTIC	2' LINE WITH 4' GAPS, 4" YELLOW OR 6" WHITE	610109M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 4" (LF) 610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 6" (LF)
SOLID LANE LINE	THERMOPLASTIC	8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
CROSSWALK LINE	THERMOPLASTIC	24" WHITE, 6' LONG SPACED AT 4' ON CENTER OR 12" WHITE, SPACED AT 7' ON CENTER	610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF) 610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
CROSSHATCH LINE	THERMOPLASTIC	24" YELLOW OR 24" WHITE SPACED 12' ON CENTER AT 45°	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
GORE LINE	THERMOPLASTIC	12" WHITE	610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
STOP LINE	THERMOPLASTIC	24" WHITE	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
SYMBOL - MARKING	THERMOPLASTIC	WHITE	610124M - TRAFFIC MARKINGS, SYMBOLS, LONG LIFE, THERMOPLASTIC (SF)

*UNLESS OTHERWISE NOTED

-THE MINIMUM DISTANCE BETWEEN STOP LINE AND CROSSWALK SHALL BE 4'.
-ALL LINES AND SIGNS SHALL CONFORM TO THE CURRENT "MUTCD".
-ALL SIGNS AND MARKINGS IN CONFLICT WITH PLAN SHALL BE REMOVED.

SIGN LEGEND

R1-1 36" X 36" | R5-1 30" x 30" | R6-1L 36" x 12"

30' 20' 10' 0 30' 60'

SCALE 1" = 30'

NOTE:

1. RIGHT OF WAY PLOTTED FROM RECORD PLANS AND TAX MAPS.

TO BE CONSTRUCTED

ITEM	DESCRIPTION	QUANTITIES	
42	TRAFFIC STRIPES, LATEX 4"	1,244	L.F.
43	TRAFFIC MARKING SYMBOLS, LATEX	129	S.F.
44	TRAFFIC STRIPES, 4"	1,785	L.F.
45	TRAFFIC STRIPES, 6"	1,424	L.F.
46	RPM, MONO-DIRECTIONAL, WHITE LENS	86	UN.
47	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 12"	112	L.F.
48	RPM, MONO-DIRECTIONAL, AMBER LENS	25	UN.
51	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 8"	332	L.F.
52	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 24"	33	L.F.
53	TRAFFIC MARKING, SYMBOLS, LONG LIFE THERMOPLASTIC	129	S.F.
54	REGULATORY AND WARNING SIGN	19	S.F.

STATE OF NEW JERSEY
BETH-ANN M. GRASSO
No. GE 43121
LICENSED PROFESSIONAL ENGINEER
TSS18

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

DATE	REVISIONS	BY	CHK.

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

TRAFFIC SIGNING & STRIPING PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE 1"=30'	DRAWN C.D.	APPROVED B.M.G.
FILE NO.	12 - 009 - 0128		SHEET 36 OF 84

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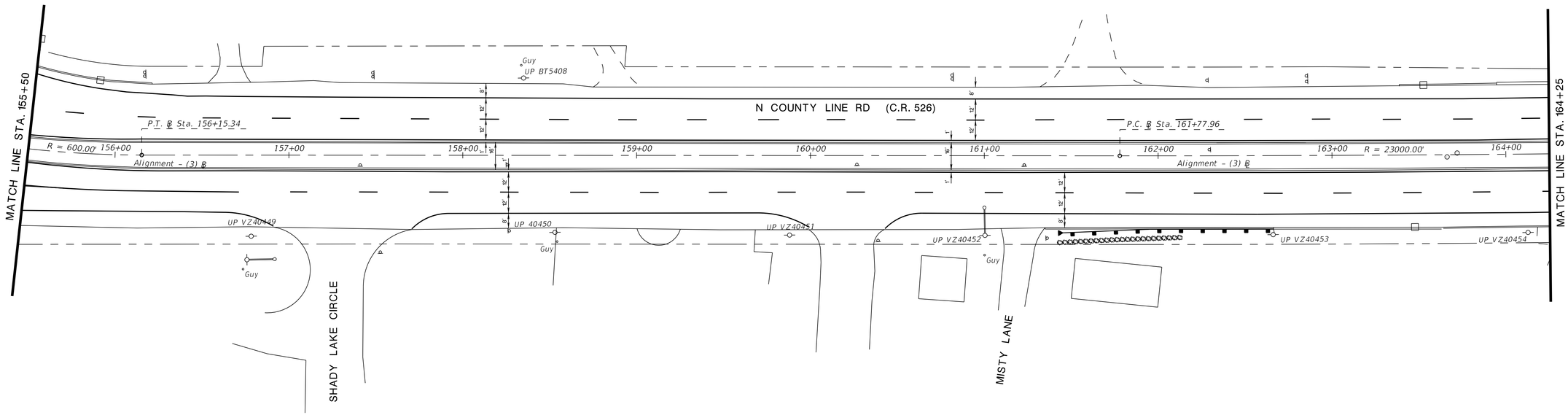
PENNONI - 10/28/24 - AM



*PAVEMENT MARKING SCHEDULE			
PAVEMENT MARKING	MATERIAL	DESCRIPTION	CONTRACT ITEM
CENTER LINE	EPOXY RESIN	4" DASHED 10' LINE WITH 30' GAPS OR SOLID YELLOW	610003M - TRAFFIC STRIPES, 4" (LF)
EDGE LINE	EPOXY RESIN	4" YELLOW OR 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DASHED LANE LINE	EPOXY RESIN	10' LINE WITH 30' GAPS, 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DOTTED LANE LINE	THERMOPLASTIC	3' LINE WITH 9' GAPS, 8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
DOTTED LANE EXTENSION LINE WITHIN AN INTERSECTION "CAT TRACKS"	THERMOPLASTIC	2' LINE WITH 4' GAPS, 4" YELLOW OR 6" WHITE	610109M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 4" (LF) 610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 6" (LF)
SOLID LANE LINE	THERMOPLASTIC	8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
CROSSWALK LINE	THERMOPLASTIC	24" WHITE, 6' LONG SPACED AT 4' ON CENTER OR 12" WHITE, SPACED AT 7' ON CENTER	610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF) 610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
CROSSHATCH LINE	THERMOPLASTIC	24" YELLOW OR 24" WHITE SPACED 12' ON CENTER AT 45°	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
GORE LINE	THERMOPLASTIC	12" WHITE	610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
STOP LINE	THERMOPLASTIC	24" WHITE	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
SYMBOL - MARKING	THERMOPLASTIC	WHITE	610124M - TRAFFIC MARKINGS, SYMBOLS, LONG LIFE, THERMOPLASTIC (SF)

*UNLESS OTHERWISE NOTED

-THE MINIMUM DISTANCE BETWEEN STOP LINE AND CROSSWALK SHALL BE 4'.

-ALL LINES AND SIGNS SHALL CONFORM TO THE CURRENT "MUTCD".

-ALL SIGNS AND MARKINGS IN CONFLICT WITH PLAN SHALL BE REMOVED.

TO BE CONSTRUCTED		
ITEM	DESCRIPTION	QUANTITIES
42	TRAFFIC STRIPES, LATEX 4"	2,188 L.F.
44	TRAFFIC STRIPES, 4"	1,775 L.F.
45	TRAFFIC STRIPES, 6"	2,214 L.F.
46	RPM, MONO-DIRECTIONAL, WHITE LENS	26 UN.
48	RPM, MONO-DIRECTIONAL, AMBER LENS	22 UN.

NOTE:

1. RIGHT OF WAY PLOTTED FROM RECORD PLANS AND TAX MAPS.

30' 20' 10' 0 30' 60'

SCALE 1" = 30'

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

STATE OF NEW JERSEY
BETH-ANN M. GRASSO
No. GE 43121
LICENSED PROFESSIONAL ENGINEER
TSS18

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

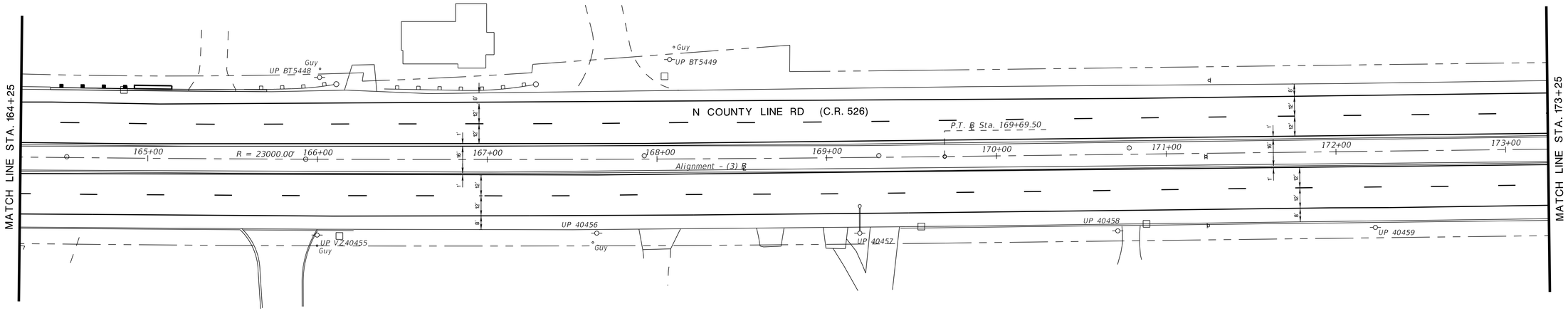
FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

TRAFFIC SIGNING & STRIPING PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE 1"=30'	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 37 OF 84

I:\Accurati\OCEAN\OCEAN\CE - OCEAN\12-009\Phase II Final\CE and\CE\Plans_SHEETS\TS-1.dgn
FENZONA-ERA-CSSN-CO-ALL-FULL_PS_ms\RTCPS
PLOTTED: 4/28/2026 PM



*PAVEMENT MARKING SCHEDULE

PAVEMENT MARKING	MATERIAL	DESCRIPTION	CONTRACT ITEM
CENTER LINE	EPOXY RESIN	4" DASHED 10' LINE WITH 30' GAPS OR SOLID YELLOW	610003M - TRAFFIC STRIPES, 4" (LF)
EDGE LINE	EPOXY RESIN	4" YELLOW OR 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DASHED LANE LINE	EPOXY RESIN	10' LINE WITH 30' GAPS, 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DOTTED LANE LINE	THERMOPLASTIC	3' LINE WITH 9' GAPS, 8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
DOTTED LANE EXTENSION LINE WITHIN AN INTERSECTION "CAT TRACKS"	THERMOPLASTIC	2' LINE WITH 4' GAPS, 4" YELLOW OR 6" WHITE	610109M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 4" (LF) 610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 6" (LF)
SOLID LANE LINE	THERMOPLASTIC	8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
CROSSWALK LINE	THERMOPLASTIC	24" WHITE, 6' LONG SPACED AT 4' ON CENTER OR 12" WHITE, SPACED AT 7' ON CENTER	610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF) 610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
CROSSHATCH LINE	THERMOPLASTIC	24" YELLOW OR 24" WHITE SPACED 12' ON CENTER AT 45°	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
GORE LINE	THERMOPLASTIC	12" WHITE	610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
STOP LINE	THERMOPLASTIC	24" WHITE	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
SYMBOL - MARKING	THERMOPLASTIC	WHITE	610124M - TRAFFIC MARKINGS, SYMBOLS, LONG LIFE, THERMOPLASTIC (SF)

*UNLESS OTHERWISE NOTED

-THE MINIMUM DISTANCE BETWEEN STOP LINE AND CROSSWALK SHALL BE 4'.

-ALL LINES AND SIGNS SHALL CONFORM TO THE CURRENT "MUTCD".

-ALL SIGNS AND MARKINGS IN CONFLICT WITH PLAN SHALL BE REMOVED.

TO BE CONSTRUCTED

ITEM	DESCRIPTION	QUANTITIES	
42	TRAFFIC STRIPES, LATEX 4"	2,250	L.F.
44	TRAFFIC STRIPES, 4"	1,801	L.F.
45	TRAFFIC STRIPES, 6"	2,302	L.F.
46	RPM, MONO-DIRECTIONAL, WHITE LENS	22	UN.
48	RPM, MONO-DIRECTIONAL, AMBER LENS	22	UN.

NOTE:

1. RIGHT OF WAY PLOTTED FROM RECORD PLANS AND TAX MAPS.

30' 20' 10' 0 30' 60'

SCALE 1" = 30'

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

STATE OF NEW JERSEY BETH-ANN M. GRASSO No. GE 43121 LICENSED PROFESSIONAL ENGINEER TSS18

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

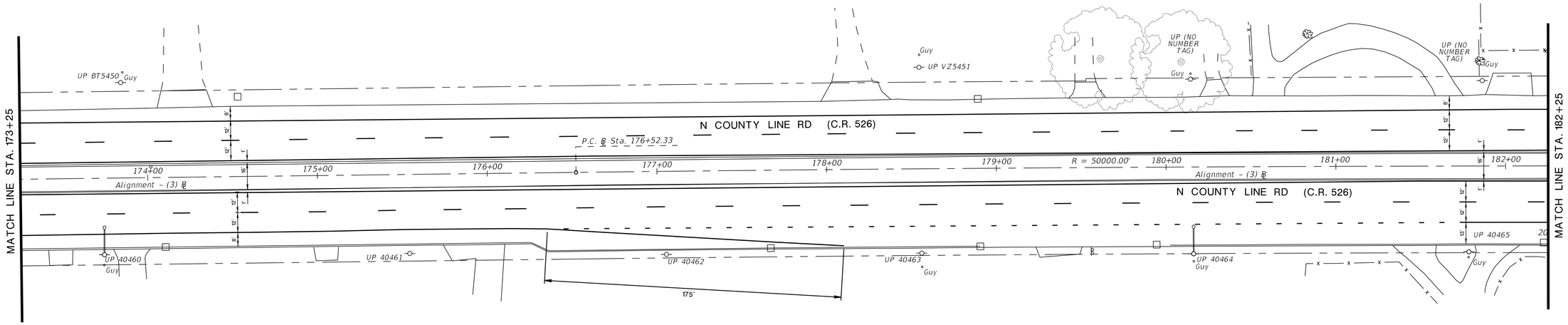
FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

TRAFFIC SIGNING & STRIPING PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE 1"=30'	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 38 OF 84

J:\Accounts\COUNTY\OCEAN\12-009\12-009-50 Rounds 6 Rural\DS and\DGN_SHEETS\18-sig.dgn
FENZONA-ERA-CSSN-CO-ALL-FILL_PS_ms\RTCPS
PLOTTED: 4/28/2026 PM



*PAVEMENT MARKING SCHEDULE			
PAVEMENT MARKING	MATERIAL	DESCRIPTION	CONTRACT ITEM
CENTER LINE	EPOXY RESIN	4" DASHED 10' LINE WITH 30' GAPS OR SOLID YELLOW	610003M - TRAFFIC STRIPES, 4" (LF)
EDGE LINE	EPOXY RESIN	4" YELLOW OR 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DASHED LANE LINE	EPOXY RESIN	10' LINE WITH 30' GAPS, 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DOTTED LANE LINE	THERMOPLASTIC	3' LINE WITH 9' GAPS, 8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
DOTTED LANE EXTENSION LINE WITHIN AN INTERSECTION "CAT TRACKS"	THERMOPLASTIC	2' LINE WITH 4' GAPS, 4" YELLOW OR 6" WHITE	610109M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 4" (LF) 610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 6" (LF)
SOLID LANE LINE	THERMOPLASTIC	8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
CROSSWALK LINE	THERMOPLASTIC	24" WHITE, 6' LONG SPACED AT 4' ON CENTER OR 12" WHITE, SPACED AT 7' ON CENTER	610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF) 610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
CROSSHATCH LINE	THERMOPLASTIC	24" YELLOW OR 24" WHITE SPACED 12' ON CENTER AT 45°	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
GORE LINE	THERMOPLASTIC	12" WHITE	610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
STOP LINE	THERMOPLASTIC	24" WHITE	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
SYMBOL - MARKING	THERMOPLASTIC	WHITE	610124M - TRAFFIC MARKINGS, SYMBOLS, LONG LIFE, THERMOPLASTIC (SF)

*UNLESS OTHERWISE NOTED

-THE MINIMUM DISTANCE BETWEEN STOP LINE AND CROSSWALK SHALL BE 4'.

-ALL LINES AND SIGNS SHALL CONFORM TO THE CURRENT "MUTCD".

-ALL SIGNS AND MARKINGS IN CONFLICT WITH PLAN SHALL BE REMOVED.

TO BE CONSTRUCTED		
ITEM	DESCRIPTION	QUANTITIES
42	TRAFFIC STRIPES, LATEX 4"	2,250 L.F.
44	TRAFFIC STRIPES, 4"	1,801 L.F.
45	TRAFFIC STRIPES, 6"	1,897 L.F.
46	RPM, MONO-DIRECTIONAL, WHITE LENS	31 UN.
48	RPM, MONO-DIRECTIONAL, AMBER LENS	22 UN.
51	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 8"	185 L.F.

NOTE:

1. RIGHT OF WAY PLOTTED FROM RECORD PLANS AND TAX MAPS.

30' 20' 10' 0 30' 60'

SCALE 1" = 30'

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

STATE OF NEW JERSEY
BETH-ANN M. GRASSO
No. GE 43121
LICENSED PROFESSIONAL ENGINEER
TSS18

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

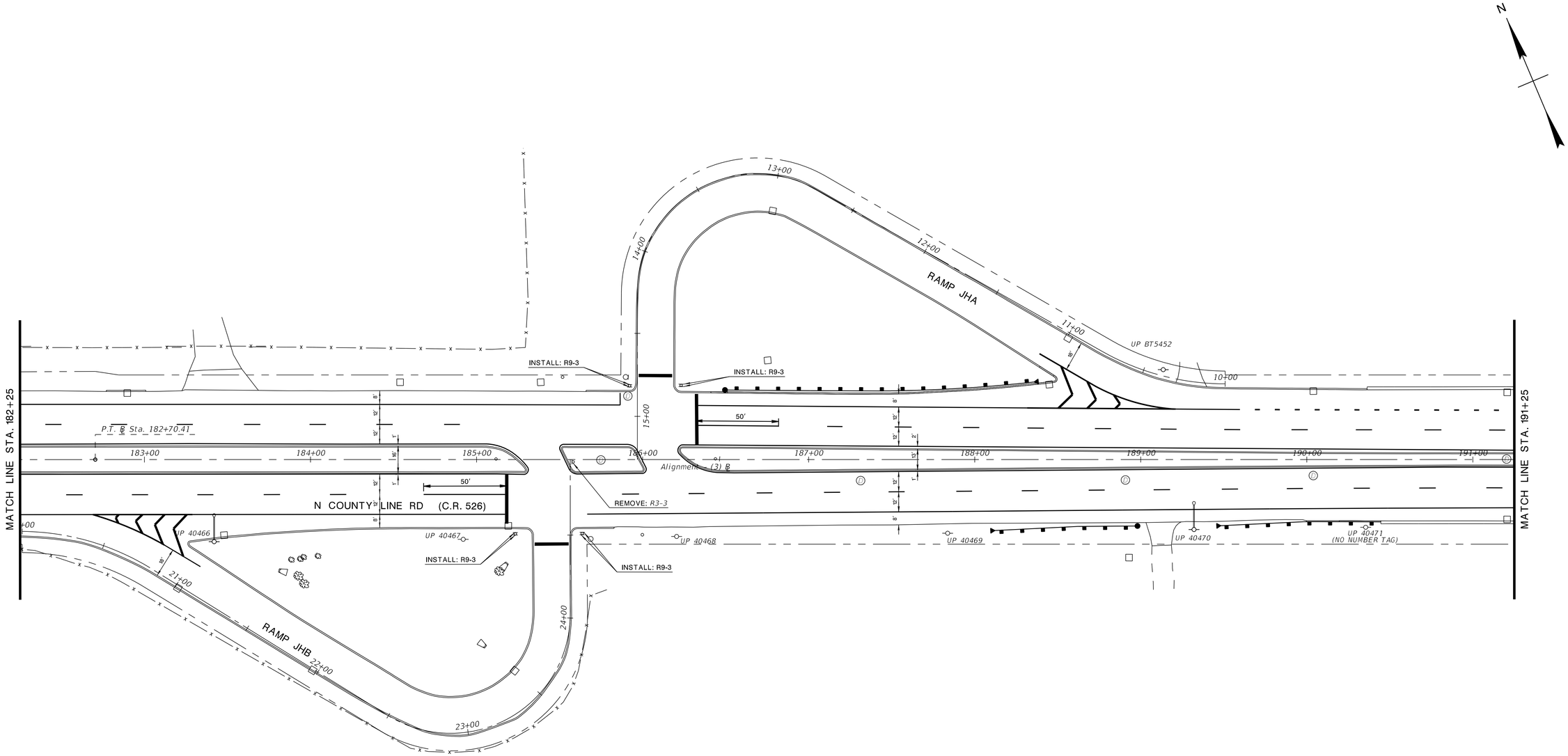
FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

TRAFFIC SIGNING & STRIPING PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE 1"=30'	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 39 OF 84

I:\Accurti\CORP\CORP\ACCT - CORP\25-NJ Round 6 Rural\25-NJR\25NJR_PE\STSR-4.dgn
PENNONI-GRASSO-CRAIG-CO-ALL-FILL_PE_ms\RTCPS
PLOTTED: 12/22/2025 PM



*PAVEMENT MARKING SCHEDULE

PAVEMENT MARKING	MATERIAL	DESCRIPTION	CONTRACT ITEM
CENTER LINE	EPOXY RESIN	4" DASHED 10' LINE WITH 30' GAPS OR SOLID YELLOW	610003M - TRAFFIC STRIPES, 4" (LF)
EDGE LINE	EPOXY RESIN	4" YELLOW OR 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DASHED LANE LINE	EPOXY RESIN	10' LINE WITH 30' GAPS, 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DOTTED LANE LINE	THERMOPLASTIC	3' LINE WITH 9' GAPS, 8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
DOTTED LANE EXTENSION LINE WITHIN AN INTERSECTION "CAT TRACKS"	THERMOPLASTIC	2' LINE WITH 4' GAPS, 4" YELLOW OR 6" WHITE	610109M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 4" (LF) 610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 6" (LF)
SOLID LANE LINE	THERMOPLASTIC	8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
CROSSWALK LINE	THERMOPLASTIC	24" WHITE, 6' LONG SPACED AT 4' ON CENTER OR 12" WHITE, SPACED AT 7' ON CENTER	610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF) 610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
CROSSHATCH LINE	THERMOPLASTIC	24" YELLOW OR 24" WHITE SPACED 12' ON CENTER AT 45°	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
GORE LINE	THERMOPLASTIC	12" WHITE	610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
STOP LINE	THERMOPLASTIC	24" WHITE	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
SYMBOL - MARKING	THERMOPLASTIC	WHITE	610124M - TRAFFIC MARKINGS, SYMBOLS, LONG LIFE, THERMOPLASTIC (SF)

*UNLESS OTHERWISE NOTED

-THE MINIMUM DISTANCE BETWEEN STOP LINE AND CROSSWALK SHALL BE 4'.

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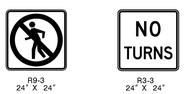
-ALL SIGNS AND MARKINGS IN CONFLICT WITH PLAN SHALL BE REMOVED.

SIGN LEGEND



R9-3
24" X 24"

R3-3
24" X 24"

TO BE CONSTRUCTED

ITEM	DESCRIPTION	QUANTITIES	
42	TRAFFIC STRIPES, LATEX 4"	2,859	L.F.
44	TRAFFIC STRIPES, 4"	1,760	L.F.
45	TRAFFIC STRIPES, 6"	1,720	L.F.
46	RPM, MONO-DIRECTIONAL, WHITE LENS	63	UN.
47	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 12"	107	L.F.
48	RPM, MONO-DIRECTIONAL, AMBER LENS	23	UN.
50	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 6"	99	L.F.
51	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 8"	411	L.F.
52	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 24"	103	L.F.
54	REGULATORY AND WARNING SIGN	32	S.F.

NOTE:

1. RIGHT OF WAY PLOTTED FROM RECORD PLANS AND TAX MAPS.

30' 20' 10' 0 30' 60'

SCALE 1" = 30'

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

STATE OF NEW JERSEY
BETH-ANN M. GRASSO
No. GE 43121
LICENSED PROFESSIONAL ENGINEER
TSS18

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

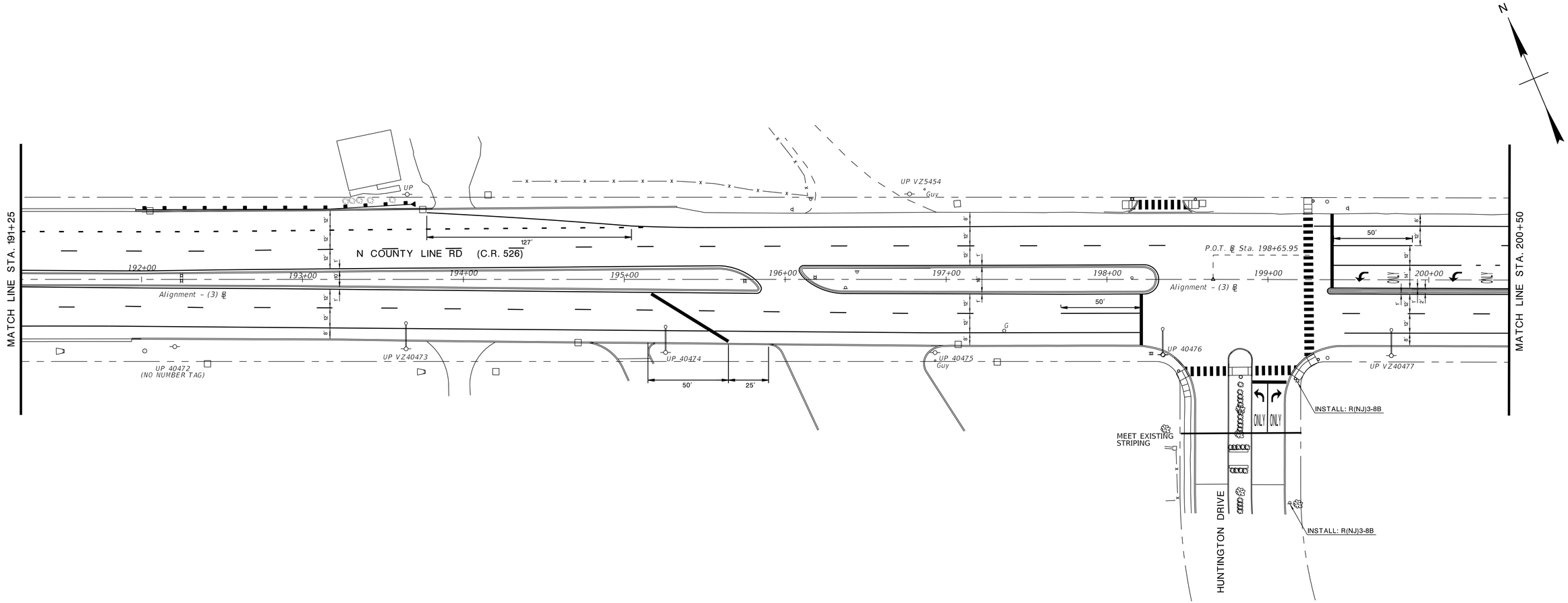
FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

TRAFFIC SIGNING & STRIPING PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE 1"=30'	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 40 OF 84

U:\Accounts\OCEAN\OCEAN\DGN\12-009-0128 North County Line Road\Phase 1\Plans\CR 526\CR526_PLAN_SIGNS-STRIPE.dgn
PENNONI-BCH-DGN-CO-ALL-PF_mp8.PLTCFG
PENNONI, 10/22/2024



SIGN LEGEND

R(NJ)3-8B
30" x 30"

NOTE:

1. RIGHT OF WAY PLOTTED FROM RECORD PLANS AND TAX MAPS.

30' 20' 10' 0 30' 60'

SCALE 1" = 30'

*PAVEMENT MARKING SCHEDULE

PAVEMENT MARKING	MATERIAL	DESCRIPTION	CONTRACT ITEM
CENTER LINE	EPOXY RESIN	4" DASHED 10' LINE WITH 30' GAPS OR SOLID YELLOW	610003M - TRAFFIC STRIPES, 4" (LF)
EDGE LINE	EPOXY RESIN	4" YELLOW OR 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DASHED LANE LINE	EPOXY RESIN	10' LINE WITH 30' GAPS, 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DOTTED LANE LINE	THERMOPLASTIC	3' LINE WITH 9' GAPS, 8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
DOTTED LANE EXTENSION LINE WITHIN AN INTERSECTION "CAT TRACKS"	THERMOPLASTIC	2' LINE WITH 4' GAPS, 4" YELLOW OR 6" WHITE	610109M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 4" (LF) 610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 6" (LF)
SOLID LANE LINE	THERMOPLASTIC	8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
CROSSWALK LINE	THERMOPLASTIC	24" WHITE, 6' LONG SPACED AT 4' ON CENTER OR 12" WHITE, SPACED AT 7' ON CENTER	610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF) 610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
CROSSHATCH LINE	THERMOPLASTIC	24" YELLOW OR 24" WHITE SPACED 12' ON CENTER AT 45°	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
GORE LINE	THERMOPLASTIC	12" WHITE	610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
STOP LINE	THERMOPLASTIC	24" WHITE	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
SYMBOL - MARKING	THERMOPLASTIC	WHITE	610124M - TRAFFIC MARKINGS, SYMBOLS, LONG LIFE, THERMOPLASTIC (SF)

*UNLESS OTHERWISE NOTED

-THE MINIMUM DISTANCE BETWEEN STOP LINE AND CROSSWALK SHALL BE 4'.

-ALL LINES AND SIGNS SHALL CONFORM TO THE CURRENT "MUTCD".

-ALL SIGNS AND MARKINGS IN CONFLICT WITH PLAN SHALL BE REMOVED.

TO BE CONSTRUCTED

ITEM	DESCRIPTION	QUANTITIES	
42	TRAFFIC STRIPES, LATEX 4"	4,751	L.F.
43	TRAFFIC MARKING SYMBOLS, LATEX	184	S.F.
44	TRAFFIC STRIPES, 4"	1,622	L.F.
45	TRAFFIC STRIPES, 6"	1,856	L.F.
46	RPM, MONO-DIRECTIONAL, WHITE LENS	32	UN.
48	RPM, MONO-DIRECTIONAL, AMBER LENS	18	UN.
50	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 6"	131	L.F.
51	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 8"	212	L.F.
52	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 24"	407	L.F.
53	TRAFFIC MARKING, SYMBOLS, LONG LIFE THERMOPLASTIC	184	S.F.
54	REGULATORY AND WARNING SIGN	13	S.F.

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

STATE OF NEW JERSEY
BETH-ANN M. GRASSO
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4/28/2026

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NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

TRAFFIC SIGNING & STRIPING PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE 1"=30'	DRAWN C.D.	APPROVED B.M.G.
FILE NO.	12 - 009 - 0128		SHEET 41 OF 84

J:\Accounts\COCR\COCR0025 - COCR-526 Rehab & Resurf CR 526\CADD\PLS\TSS18-ESP
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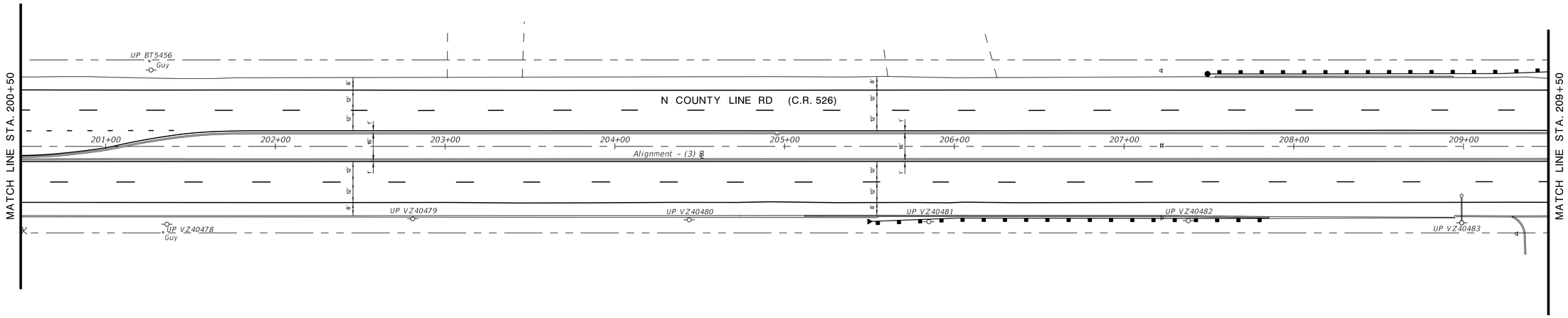
PLOTTED: 4/28/2026 - 9:16 AM
PLOTTED BY: Chris Dunn



*PAVEMENT MARKING SCHEDULE

PAVEMENT MARKING	MATERIAL	DESCRIPTION	CONTRACT ITEM
CENTER LINE	EPOXY RESIN	4" DASHED 10' LINE WITH 30' GAPS OR SOLID YELLOW	610003M - TRAFFIC STRIPES, 4" (LF)
EDGE LINE	EPOXY RESIN	4" YELLOW OR 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DASHED LANE LINE	EPOXY RESIN	10' LINE WITH 30' GAPS, 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DOTTED LANE LINE	THERMOPLASTIC	3' LINE WITH 9' GAPS, 8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
DOTTED LANE EXTENSION LINE WITHIN AN INTERSECTION "CAT TRACKS"	THERMOPLASTIC	2' LINE WITH 4' GAPS, 4" YELLOW OR 6" WHITE	610109M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 4" (LF) 610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 6" (LF)
SOLID LANE LINE	THERMOPLASTIC	8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
CROSSWALK LINE	THERMOPLASTIC	24" WHITE, 6' LONG SPACED AT 4' ON CENTER OR 12" WHITE, SPACED AT 7' ON CENTER	610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF) 610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
CROSSHATCH LINE	THERMOPLASTIC	24" YELLOW OR 24" WHITE SPACED 12' ON CENTER AT 45°	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
GORE LINE	THERMOPLASTIC	12" WHITE	610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
STOP LINE	THERMOPLASTIC	24" WHITE	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
SYMBOL - MARKING	THERMOPLASTIC	WHITE	610124M - TRAFFIC MARKINGS, SYMBOLS, LONG LIFE, THERMOPLASTIC (SF)

*UNLESS OTHERWISE NOTED

-THE MINIMUM DISTANCE BETWEEN STOP LINE AND CROSSWALK SHALL BE 4'.

-ALL LINES AND SIGNS SHALL CONFORM TO THE CURRENT "MUTCD".

-ALL SIGNS AND MARKINGS IN CONFLICT WITH PLAN SHALL BE REMOVED.

TO BE CONSTRUCTED

ITEM	DESCRIPTION	QUANTITIES	
42	TRAFFIC STRIPES, LATEX 4"	2,251	L.F.
44	TRAFFIC STRIPES, 4"	1,802	L.F.
45	TRAFFIC STRIPES, 6"	2,306	L.F.
46	RPM, MONO-DIRECTIONAL, WHITE LENS	24	UN.
48	RPM, MONO-DIRECTIONAL, AMBER LENS	22	UN.
51	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 8"	24	L.F.

NOTE:

1. RIGHT OF WAY PLOTTED FROM RECORD PLANS AND TAX MAPS.

30' 20' 10' 0 30' 60'

SCALE 1" = 30'

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

STATE OF NEW JERSEY
BETH-ANN M. GRASSO
No. GE 43121
LICENSED PROFESSIONAL ENGINEER
TSS18

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

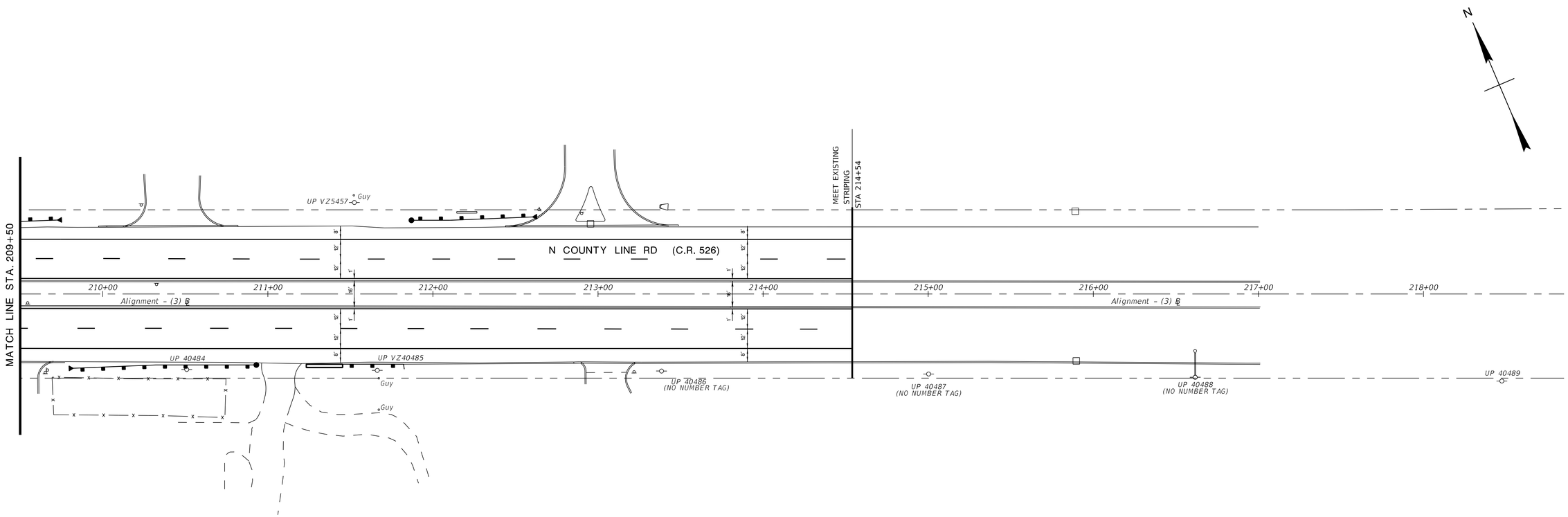
FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

TRAFFIC SIGNING & STRIPING PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE 1"=30'	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 42 OF 84

J:\Accurate\CORP\CORP\ACCE - CORP\25-NJ Round 6 Rural\25-NJR2GBRA_SHEETS\25-NJR-1.dgn
PENNONI-GRA-CSGNC-CO-ALL-FULL_PS_ms\RTCPS
PLOTTED: 4/28/2026 PM



*PAVEMENT MARKING SCHEDULE

PAVEMENT MARKING	MATERIAL	DESCRIPTION	CONTRACT ITEM
CENTER LINE	EPOXY RESIN	4" DASHED 10' LINE WITH 30' GAPS OR SOLID YELLOW	610003M - TRAFFIC STRIPES, 4" (LF)
EDGE LINE	EPOXY RESIN	4" YELLOW OR 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DASHED LANE LINE	EPOXY RESIN	10' LINE WITH 30' GAPS, 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DOTTED LANE LINE	THERMOPLASTIC	3' LINE WITH 9' GAPS, 8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
DOTTED LANE EXTENSION LINE WITHIN AN INTERSECTION "CAT TRACKS"	THERMOPLASTIC	2' LINE WITH 4' GAPS, 4" YELLOW OR 6" WHITE	610109M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 4" (LF) 610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 6" (LF)
SOLID LANE LINE	THERMOPLASTIC	8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
CROSSWALK LINE	THERMOPLASTIC	24" WHITE, 6' LONG SPACED AT 4' ON CENTER OR 12" WHITE, SPACED AT 7' ON CENTER	610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF) 610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
CROSSHATCH LINE	THERMOPLASTIC	24" YELLOW OR 24" WHITE SPACED 12' ON CENTER AT 45°	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
GORE LINE	THERMOPLASTIC	12" WHITE	610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
STOP LINE	THERMOPLASTIC	24" WHITE	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
SYMBOL - MARKING	THERMOPLASTIC	WHITE	610124M - TRAFFIC MARKINGS, SYMBOLS, LONG LIFE, THERMOPLASTIC (SF)

*UNLESS OTHERWISE NOTED

-THE MINIMUM DISTANCE BETWEEN STOP LINE AND CROSSWALK SHALL BE 4'.

-ALL LINES AND SIGNS SHALL CONFORM TO THE CURRENT "MUTCD".

-ALL SIGNS AND MARKINGS IN CONFLICT WITH PLAN SHALL BE REMOVED.

TO BE CONSTRUCTED

ITEM	DESCRIPTION	QUANTITIES	
42	TRAFFIC STRIPES, LATEX 4"	1,261	L.F.
44	TRAFFIC STRIPES, 4"	1,008	L.F.
45	TRAFFIC STRIPES, 6"	1,296	L.F.
46	RPM, MONO-DIRECTIONAL, WHITE LENS	12	UN.
48	RPM, MONO-DIRECTIONAL, AMBER LENS	12	UN.

NOTE:

1. RIGHT OF WAY PLOTTED FROM RECORD PLANS AND TAX MAPS.

30' 20' 10' 0 30' 60'

SCALE 1" = 30'

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

STATE OF NEW JERSEY
BETH-ANN M. GRASSO
No. GE 43121
LICENSED PROFESSIONAL ENGINEER
TSS18

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

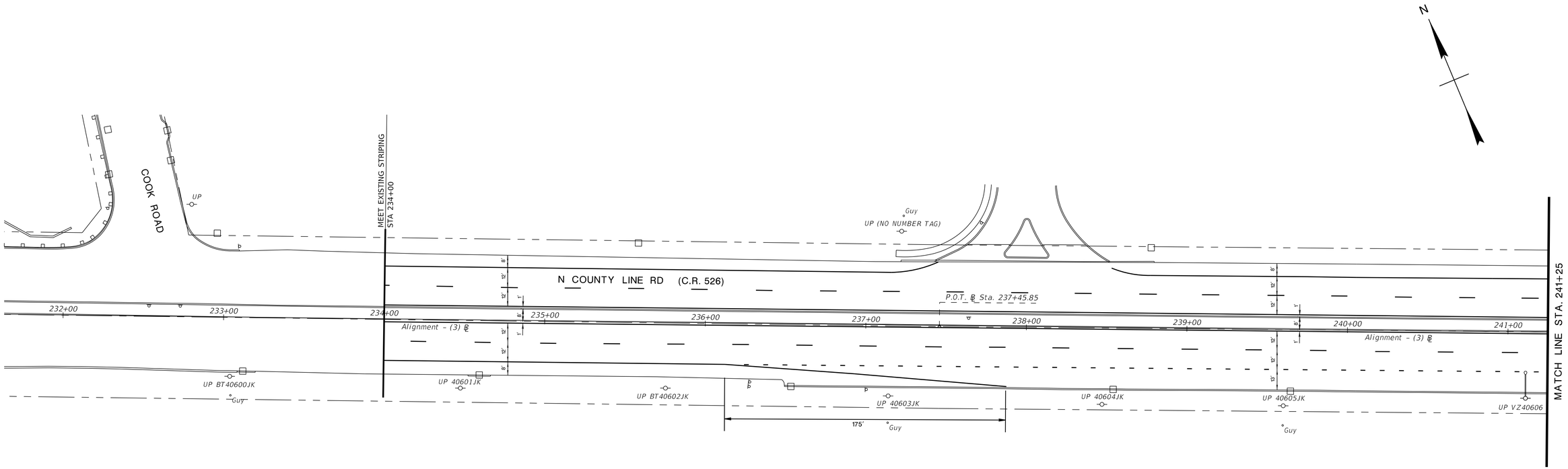
FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

TRAFFIC SIGNING & STRIPING PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE 1"=30'	DRAWN C.D.	APPROVED B.M.G.
FILE NO.	12 - 009 - 0128		SHEET 43 OF 84

I:\Accurati\CORP\CORP\SAVE - CORP\25-30 Round 6 Rural\25-30\25BOA_SHEETS\25-6.dgn
PENNONI-BA-CSSAN-CO-ALL-ALL_PS_ms\RTCPS
4/28/2026 10:09 PM



*PAVEMENT MARKING SCHEDULE			
PAVEMENT MARKING	MATERIAL	DESCRIPTION	CONTRACT ITEM
CENTER LINE	EPOXY RESIN	4" DASHED 10' LINE WITH 30' GAPS OR SOLID YELLOW	610003M - TRAFFIC STRIPES, 4" (LF)
EDGE LINE	EPOXY RESIN	4" YELLOW OR 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DASHED LANE LINE	EPOXY RESIN	10' LINE WITH 30' GAPS, 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DOTTED LANE LINE	THERMOPLASTIC	3' LINE WITH 9' GAPS, 8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
DOTTED LANE EXTENSION LINE WITHIN AN INTERSECTION "CAT TRACKS"	THERMOPLASTIC	2' LINE WITH 4' GAPS, 4" YELLOW OR 6" WHITE	610109M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 4" (LF) 610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 6" (LF)
SOLID LANE LINE	THERMOPLASTIC	8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
CROSSWALK LINE	THERMOPLASTIC	24" WHITE, 6' LONG SPACED AT 4' ON CENTER OR 12" WHITE, SPACED AT 7' ON CENTER	610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF) 610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
CROSSHATCH LINE	THERMOPLASTIC	24" YELLOW OR 24" WHITE SPACED 12' ON CENTER AT 45°	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
GORE LINE	THERMOPLASTIC	12" WHITE	610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
STOP LINE	THERMOPLASTIC	24" WHITE	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
SYMBOL - MARKING	THERMOPLASTIC	WHITE	610124M - TRAFFIC MARKINGS, SYMBOLS, LONG LIFE, THERMOPLASTIC (SF)

*UNLESS OTHERWISE NOTED

-THE MINIMUM DISTANCE BETWEEN STOP LINE AND CROSSWALK SHALL BE 4'.

-ALL LINES AND SIGNS SHALL CONFORM TO THE CURRENT "MUTCD".

-ALL SIGNS AND MARKINGS IN CONFLICT WITH PLAN SHALL BE REMOVED.

TO BE CONSTRUCTED			
ITEM	DESCRIPTION	QUANTITIES	
42	TRAFFIC STRIPES, LATEX 4"	1,813	L.F.
44	TRAFFIC STRIPES, 4"	1,451	L.F.
45	TRAFFIC STRIPES, 6"	1,409	L.F.
46	RPM, MONO-DIRECTIONAL, WHITE LENS	25	UN.
48	RPM, MONO-DIRECTIONAL, AMBER LENS	19	UN.
51	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 8"	126	L.F.

NOTE:

1. RIGHT OF WAY PLOTTED FROM RECORD PLANS AND TAX MAPS.

30' 20' 10' 0 30' 60'

SCALE 1" = 30'

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

STATE OF NEW JERSEY BETH-ANN M. GRASSO No. GE 43121 LICENSED PROFESSIONAL ENGINEER TSS18

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

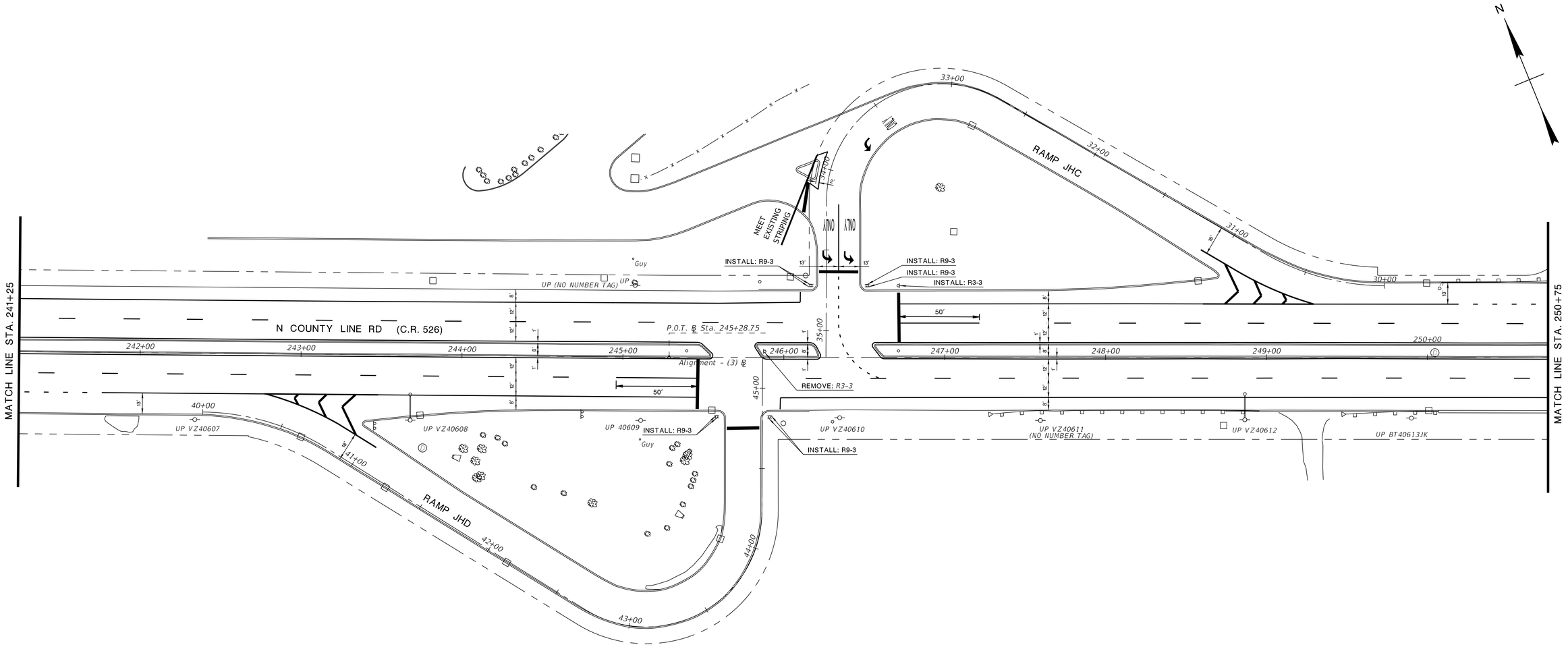
FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

TRAFFIC SIGNING & STRIPING PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE 1"=30'	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 44 OF 84

I:\Accurcit\CORP\CORP\SAVE - CORP\25-163 Round 6 Rural\DS and\SIGN_STRIPING-sgp
PENNONI - APH-CSSN-CO-ALL-FULL.dgn ms\RTCPS
PLOTTED: 4/28/2026 PM



*PAVEMENT MARKING SCHEDULE

PAVEMENT MARKING	MATERIAL	DESCRIPTION	CONTRACT ITEM
CENTER LINE	EPOXY RESIN	4" DASHED 10' LINE WITH 30' GAPS OR SOLID YELLOW	610003M - TRAFFIC STRIPES, 4" (LF)
EDGE LINE	EPOXY RESIN	4" YELLOW OR 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DASHED LANE LINE	EPOXY RESIN	10' LINE WITH 30' GAPS, 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DOTTED LANE LINE	THERMOPLASTIC	3' LINE WITH 9' GAPS, 8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
DOTTED LANE EXTENSION LINE WITHIN AN INTERSECTION "CAT TRACKS"	THERMOPLASTIC	2' LINE WITH 4' GAPS, 4" YELLOW OR 6" WHITE	610109M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 4" (LF) 610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 6" (LF)
SOLID LANE LINE	THERMOPLASTIC	8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
CROSSWALK LINE	THERMOPLASTIC	24" WHITE, 6' LONG SPACED AT 4' ON CENTER OR 12" WHITE, SPACED AT 7' ON CENTER	610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF) 610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
CROSSHATCH LINE	THERMOPLASTIC	24" YELLOW OR 24" WHITE SPACED 12' ON CENTER AT 45°	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
GORE LINE	THERMOPLASTIC	12" WHITE	610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
STOP LINE	THERMOPLASTIC	24" WHITE	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
SYMBOL - MARKING	THERMOPLASTIC	WHITE	610124M - TRAFFIC MARKINGS, SYMBOLS, LONG LIFE, THERMOPLASTIC (SF)

*UNLESS OTHERWISE NOTED

-THE MINIMUM DISTANCE BETWEEN STOP LINE AND CROSSWALK SHALL BE 4'.

-ALL LINES AND SIGNS SHALL CONFORM TO THE CURRENT "MUTCD".

-ALL SIGNS AND MARKINGS IN CONFLICT WITH PLAN SHALL BE REMOVED.

SIGN LEGEND

R9-3
24" X 24"

R3-3
24" X 24"

TO BE CONSTRUCTED

ITEM	DESCRIPTION	QUANTITIES	
42	TRAFFIC STRIPES, LATEX 4"	3,116	L.F.
43	TRAFFIC MARKING SYMBOLS, LATEX	138	S.F.
44	TRAFFIC STRIPES, 4"	1,821	L.F.
45	TRAFFIC STRIPES, 6"	1,831	L.F.
46	RPM, MONO-DIRECTIONAL, WHITE LENS	64	UN.
47	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 12"	89	L.F.
48	RPM, MONO-DIRECTIONAL, AMBER LENS	24	UN.
50	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 6"	167	L.F.
51	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 8"	511	L.F.
52	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 24"	126	L.F.
53	TRAFFIC MARKING, SYMBOLS, LONG LIFE THERMOPLASTIC	138	S.F.
54	REGULATORY AND WARNING SIGN	28	S.F.

NOTE:

1. RIGHT OF WAY PLOTTED FROM RECORD PLANS AND TAX MAPS.

30' 20' 10' 0 30' 60'

SCALE 1" = 30'

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

STATE OF NEW JERSEY BETH-ANN M. GRASSO No. GE 43121 LICENSED PROFESSIONAL ENGINEER TSS18

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

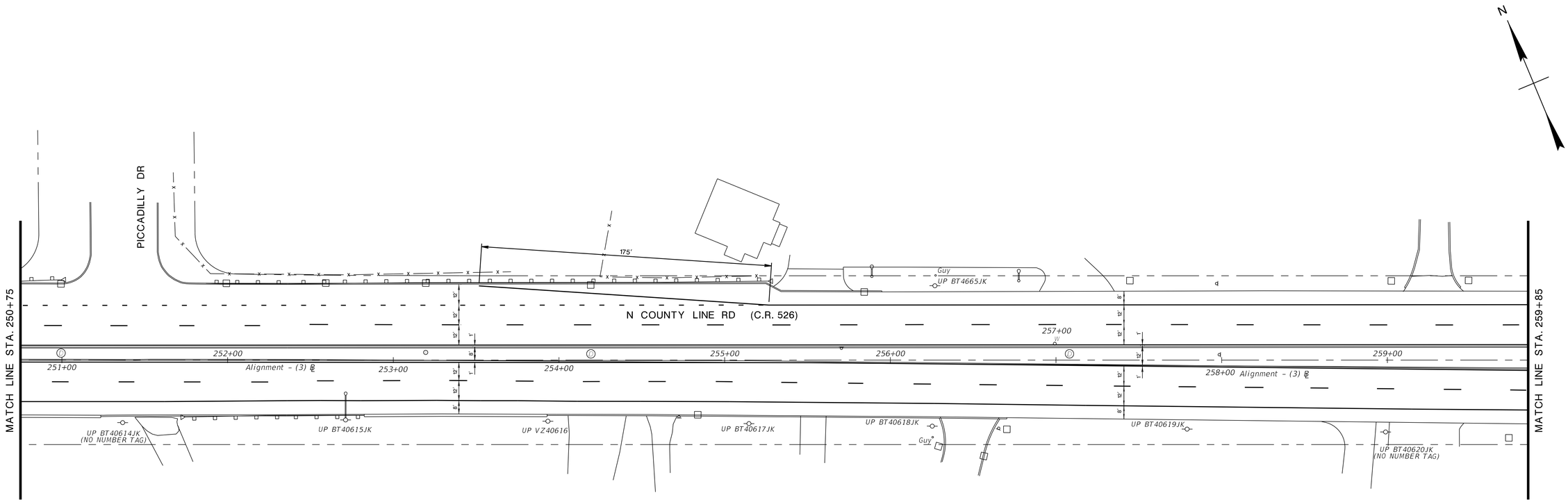
FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

TRAFFIC SIGNING & STRIPING PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE 1"=30'	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 45 OF 84

Y:\Annual\CORP\CORP\NJCO - OCEAN NJ\Route 9 Rout 28 and 530\BPU_SHEETS\18-Sign
PENNONI - ARCHITECTURE - CIVIL - PLANNING
PENNONI - 1000 Phillips Ave.
Camden, NJ 08105



*PAVEMENT MARKING SCHEDULE

PAVEMENT MARKING	MATERIAL	DESCRIPTION	CONTRACT ITEM
CENTER LINE	EPOXY RESIN	4" DASHED 10' LINE WITH 30' GAPS OR SOLID YELLOW	610003M - TRAFFIC STRIPES, 4" (LF)
EDGE LINE	EPOXY RESIN	4" YELLOW OR 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DASHED LANE LINE	EPOXY RESIN	10' LINE WITH 30' GAPS, 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DOTTED LANE LINE	THERMOPLASTIC	3' LINE WITH 9' GAPS, 8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
DOTTED LANE EXTENSION LINE WITHIN AN INTERSECTION "CAT TRACKS"	THERMOPLASTIC	2' LINE WITH 4' GAPS, 4" YELLOW OR 6" WHITE	610109M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 4" (LF) 610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 6" (LF)
SOLID LANE LINE	THERMOPLASTIC	8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
CROSSWALK LINE	THERMOPLASTIC	24" WHITE, 6' LONG SPACED AT 4' ON CENTER OR 12" WHITE, SPACED AT 7' ON CENTER	610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF) 610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
CROSSHATCH LINE	THERMOPLASTIC	24" YELLOW OR 24" WHITE SPACED 12' ON CENTER AT 45°	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
GORE LINE	THERMOPLASTIC	12" WHITE	610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
STOP LINE	THERMOPLASTIC	24" WHITE	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
SYMBOL - MARKING	THERMOPLASTIC	WHITE	610124M - TRAFFIC MARKINGS, SYMBOLS, LONG LIFE, THERMOPLASTIC (SF)

*UNLESS OTHERWISE NOTED

-THE MINIMUM DISTANCE BETWEEN STOP LINE AND CROSSWALK SHALL BE 4'.

-ALL LINES AND SIGNS SHALL CONFORM TO THE CURRENT "MUTCD".

-ALL SIGNS AND MARKINGS IN CONFLICT WITH PLAN SHALL BE REMOVED.

TO BE CONSTRUCTED

ITEM	DESCRIPTION	QUANTITIES	
42	TRAFFIC STRIPES, LATEX 4"	2,276	L.F.
44	TRAFFIC STRIPES, 4"	1,821	L.F.
45	TRAFFIC STRIPES, 6"	2,050	L.F.
46	RPM, MONO-DIRECTIONAL, WHITE LENS	33	UN.
48	RPM, MONO-DIRECTIONAL, AMBER LENS	23	UN.
51	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 8"	114	L.F.

NOTE:

1. RIGHT OF WAY PLOTTED FROM RECORD PLANS AND TAX MAPS.

30' 20' 10' 0 30' 60'

SCALE 1" = 30'

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

STATE OF NEW JERSEY BETH-ANN M. GRASSO No. GE 43121 LICENSED PROFESSIONAL ENGINEER

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

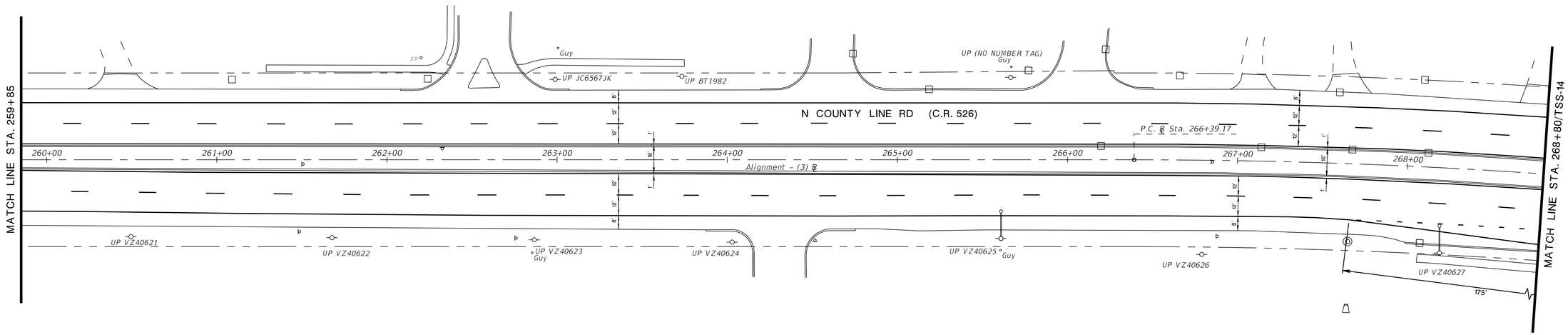
FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

TRAFFIC SIGNING & STRIPING PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE 1"=30'	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 46 OF 84

J:\Accurati\COSTA\CO\PROJECT - CO\2012-NO Rounds & Rural\DB and\DGN\PLS\TSS18-striping.dgn
FILE\20a-4Pc-CS3\N-CO-ALL-FULL.PS_ms\RTCFG
PLOTTED: 4/28/2026 PM



*PAVEMENT MARKING SCHEDULE			
PAVEMENT MARKING	MATERIAL	DESCRIPTION	CONTRACT ITEM
CENTER LINE	EPOXY RESIN	4" DASHED 10' LINE WITH 30' GAPS OR SOLID YELLOW	610003M - TRAFFIC STRIPES, 4" (LF)
EDGE LINE	EPOXY RESIN	4" YELLOW OR 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DASHED LANE LINE	EPOXY RESIN	10' LINE WITH 30' GAPS, 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DOTTED LANE LINE	THERMOPLASTIC	3' LINE WITH 9' GAPS, 8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
DOTTED LANE EXTENSION LINE WITHIN AN INTERSECTION "CAT TRACKS"	THERMOPLASTIC	2' LINE WITH 4' GAPS, 4" YELLOW OR 6" WHITE	610109M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 4" (LF) 610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 6" (LF)
SOLID LANE LINE	THERMOPLASTIC	8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
CROSSWALK LINE	THERMOPLASTIC	24" WHITE, 6' LONG SPACED AT 4' ON CENTER OR 12" WHITE, SPACED AT 7' ON CENTER	610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF) 610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
CROSSHATCH LINE	THERMOPLASTIC	24" YELLOW OR 24" WHITE SPACED 12' ON CENTER AT 45°	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
GORE LINE	THERMOPLASTIC	12" WHITE	610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
STOP LINE	THERMOPLASTIC	24" WHITE	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
SYMBOL - MARKING	THERMOPLASTIC	WHITE	610124M - TRAFFIC MARKINGS, SYMBOLS, LONG LIFE, THERMOPLASTIC (SF)

*UNLESS OTHERWISE NOTED

-THE MINIMUM DISTANCE BETWEEN STOP LINE AND CROSSWALK SHALL BE 4'.

-ALL LINES AND SIGNS SHALL CONFORM TO THE CURRENT "MUTCD".

-ALL SIGNS AND MARKINGS IN CONFLICT WITH PLAN SHALL BE REMOVED.

TO BE CONSTRUCTED			
ITEM	DESCRIPTION	QUANTITIES	
42	TRAFFIC STRIPES, LATEX 4"	2,238	L.F.
44	TRAFFIC STRIPES, 4"	1,791	L.F.
45	TRAFFIC STRIPES, 6"	2,282	L.F.
46	RPM, MONO-DIRECTIONAL, WHITE LENS	24	UN.
48	RPM, MONO-DIRECTIONAL, AMBER LENS	22	UN.
51	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 8"	27	L.F.

NOTE:

1. RIGHT OF WAY PLOTTED FROM RECORD PLANS AND TAX MAPS.

30' 20' 10' 0 30' 60'

SCALE 1" = 30'

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

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TSS18

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

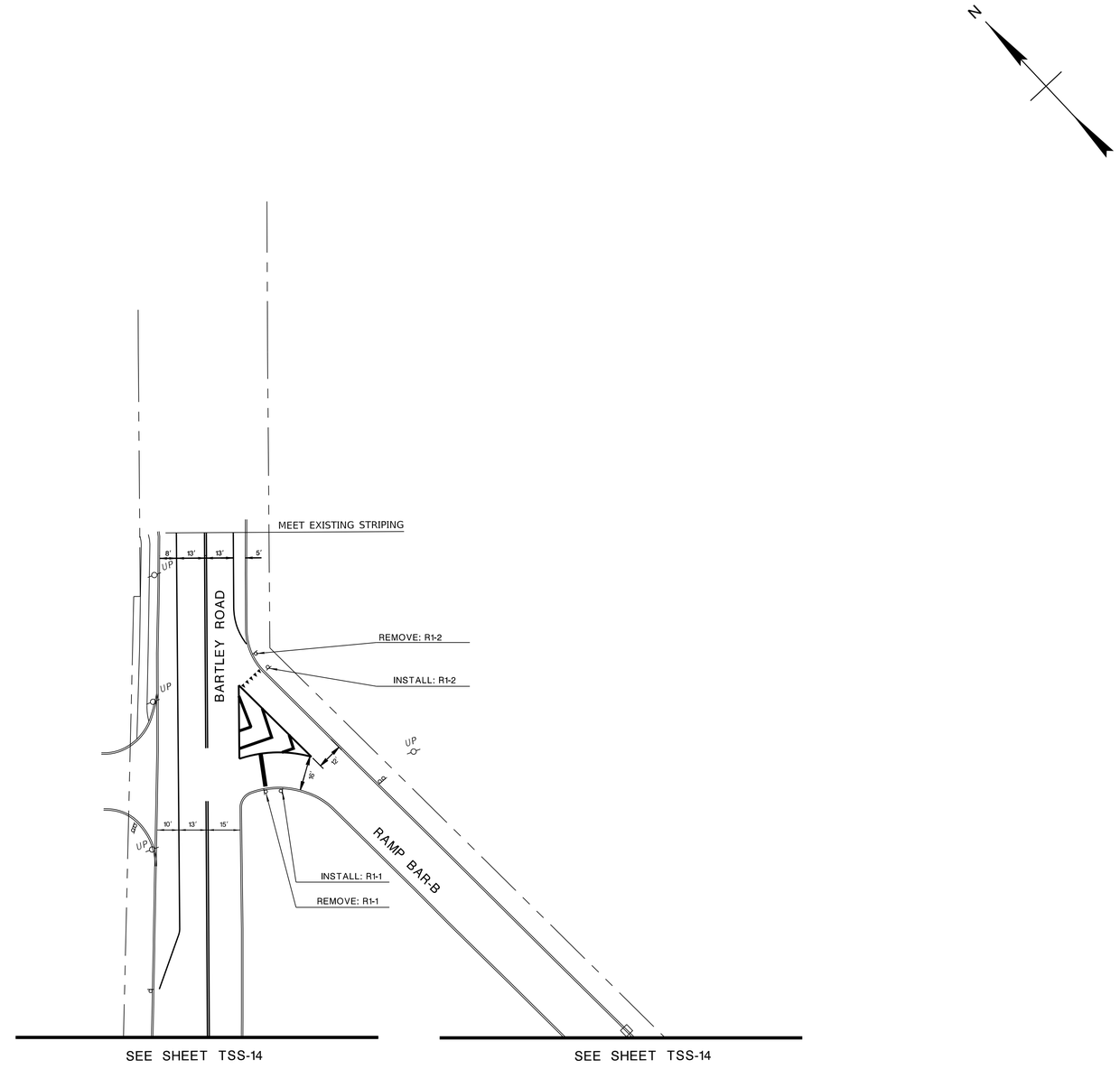
FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

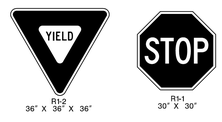
TRAFFIC SIGNING & STRIPING PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE 1"=30'	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 47 OF 84

I:\Accurati\CORP\CORP\SAVE - CORP\25-NJ Round 6 Rural\25-NJR\SUBMIT_SHEETS\TSS-Sign
FENTON-4R-CSSN-CO-ALL-FULL.dgn ms\RTCPS
PLOTTED: 5/28/26 PM



SIGN LEGEND



30' 20' 10' 0 30' 60'

SCALE 1" = 30'

NOTE:

1. RIGHT OF WAY PLOTTED FROM RECORD PLANS AND TAX MAPS.

*PAVEMENT MARKING SCHEDULE

PAVEMENT MARKING	MATERIAL	DESCRIPTION	CONTRACT ITEM
CENTER LINE	EPOXY RESIN	4" DASHED 10' LINE WITH 30' GAPS OR SOLID YELLOW	610003M - TRAFFIC STRIPES, 4" (LF)
EDGE LINE	EPOXY RESIN	4" YELLOW OR 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DASHED LANE LINE	EPOXY RESIN	10' LINE WITH 30' GAPS, 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DOTTED LANE LINE	THERMOPLASTIC	3' LINE WITH 9' GAPS, 8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
DOTTED LANE EXTENSION LINE WITHIN AN INTERSECTION "CAT TRACKS"	THERMOPLASTIC	2' LINE WITH 4' GAPS, 4" YELLOW OR 6" WHITE	610109M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 4" (LF) 610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 6" (LF)
SOLID LANE LINE	THERMOPLASTIC	8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
CROSSWALK LINE	THERMOPLASTIC	24" WHITE, 6' LONG SPACED AT 4' ON CENTER OR 12" WHITE, SPACED AT 7' ON CENTER	610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF) 610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
CROSSHATCH LINE	THERMOPLASTIC	24" YELLOW OR 24" WHITE SPACED 12" ON CENTER AT 45°	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
GORE LINE	THERMOPLASTIC	12" WHITE	610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
STOP LINE	THERMOPLASTIC	24" WHITE	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
SYMBOL - MARKING	THERMOPLASTIC	WHITE	610124M - TRAFFIC MARKINGS, SYMBOLS, LONG LIFE, THERMOPLASTIC (SF)

*UNLESS OTHERWISE NOTED

-THE MINIMUM DISTANCE BETWEEN STOP LINE AND CROSSWALK SHALL BE 4'.

-ALL LINES AND SIGNS SHALL CONFORM TO THE CURRENT "MUTCD".

-ALL SIGNS AND MARKINGS IN CONFLICT WITH PLAN SHALL BE REMOVED.

TO BE CONSTRUCTED

ITEM	DESCRIPTION	QUANTITIES
42	TRAFFIC STRIPES, LATEX 4"	93 L.F.
43	TRAFFIC MARKING SYMBOLS, LATEX	8 S.F.
44	TRAFFIC STRIPES, 4"	422 L.F.
45	TRAFFIC STRIPES, 6"	214 L.F.
46	RPM, MONO-DIRECTIONAL, WHITE LENS	8 UN.
47	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 12"	66 L.F.
49	RPM, BI-DIRECTIONAL, AMBER LENS	4 UN.
51	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 8"	117 L.F.
52	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 24"	16 L.F.
53	TRAFFIC MARKING, SYMBOLS, LONG LIFE THERMOPLASTIC	8 S.F.
54	REGULATORY AND WARNING SIGN	13 S.F.

STATE OF NEW JERSEY
BETH-ANN M. GRASSO
No. GE 43121
LICENSED PROFESSIONAL ENGINEER
TSS18

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

DATE	REVISIONS	BY	CHK.

RECONSTRUCTION OF

NORTH COUNTY LINE RD

FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

TRAFFIC SIGNING & STRIPING PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE 1"=30'	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 48 OF 84

G:\Accounts\OCEAN\CONTRACT - 2022\12-009 Bennett & Bartley SIG\SIGN_&_PEDESTS\Sig
ESN\Sig-Ch-CGAL\CLA-LT-FLL-PP_mapR1D5
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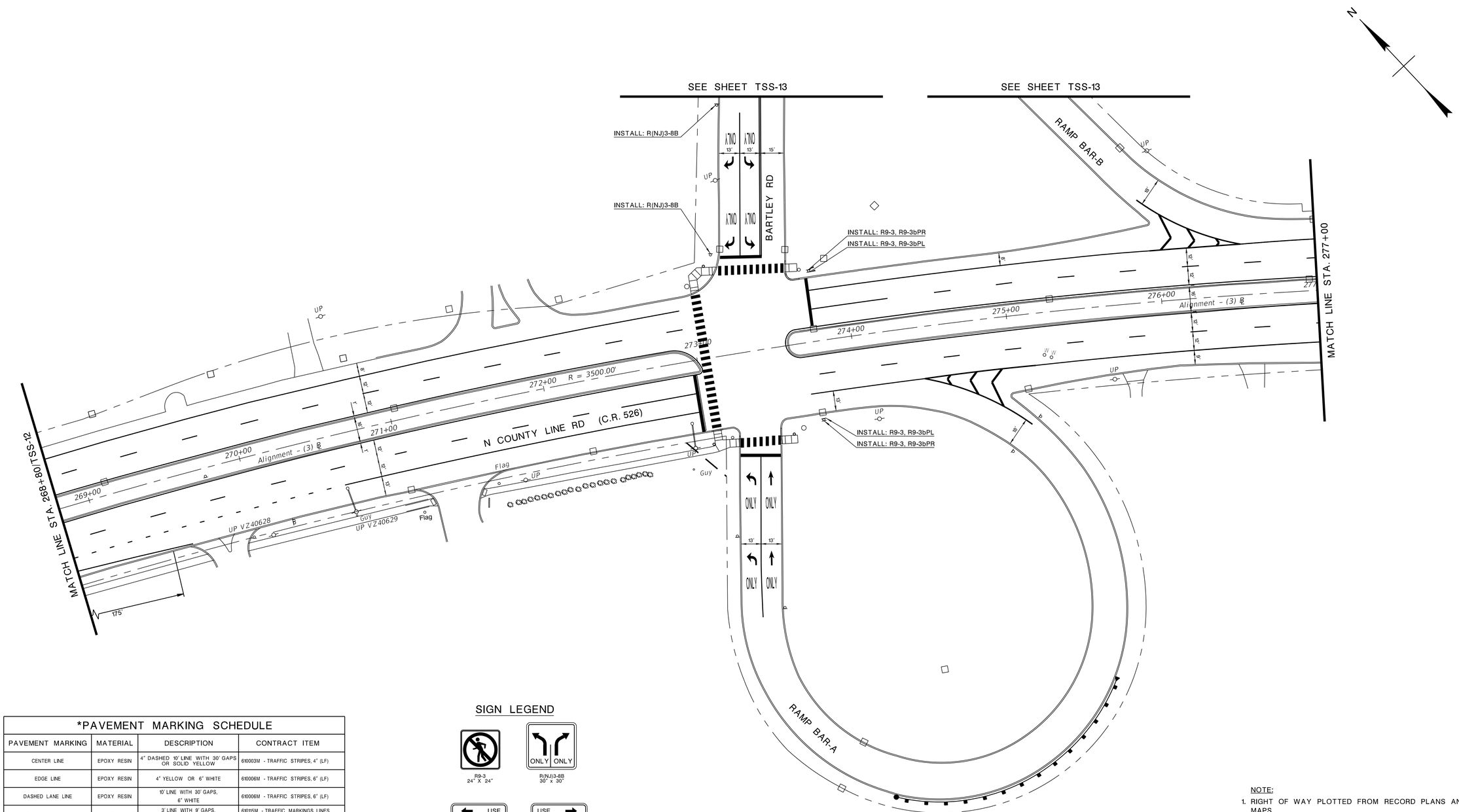

*PAVEMENT MARKING SCHEDULE

PAVEMENT MARKING	MATERIAL	DESCRIPTION	CONTRACT ITEM
CENTER LINE	EPOXY RESIN	4" DASHED 10' LINE WITH 30' GAPS OR SOLID YELLOW	610003M - TRAFFIC STRIPES, 4" (LF)
EDGE LINE	EPOXY RESIN	4" YELLOW OR 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DASHED LANE LINE	EPOXY RESIN	10' LINE WITH 30' GAPS, 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DOTTED LANE LINE	THERMOPLASTIC	3' LINE WITH 9' GAPS, 8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
DOTTED LANE EXTENSION LINE WITHIN AN INTERSECTION "CAT TRACKS"	THERMOPLASTIC	2' LINE WITH 4' GAPS, 4" YELLOW OR 6" WHITE	610109M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 4" (LF) 610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 6" (LF)
SOLID LANE LINE	THERMOPLASTIC	8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
CROSSWALK LINE	THERMOPLASTIC	24" WHITE, 6' LONG SPACED AT 4' ON CENTER OR 12" WHITE, SPACED AT 7' ON CENTER	610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF) 610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
CROSSHATCH LINE	THERMOPLASTIC	24" YELLOW OR 24" WHITE SPACED 12' ON CENTER AT 45°	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
GORE LINE	THERMOPLASTIC	12" WHITE	610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
STOP LINE	THERMOPLASTIC	24" WHITE	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
SYMBOL - MARKING	THERMOPLASTIC	WHITE	610124M - TRAFFIC MARKINGS, SYMBOLS, LONG LIFE, THERMOPLASTIC (SF)

*UNLESS OTHERWISE NOTED

-THE MINIMUM DISTANCE BETWEEN STOP LINE AND CROSSWALK SHALL BE 4'.

-ALL LINES AND SIGNS SHALL CONFORM TO THE CURRENT "MUTCD".

-ALL SIGNS AND MARKINGS IN CONFLICT WITH PLAN SHALL BE REMOVED.

SIGN LEGEND

R9-3
24" X 24"

R(NJ)3-8B
30" x 30"

R9-3bPL
18" x 12"

R9-3bPR
18" x 12"

TO BE CONSTRUCTED

ITEM	DESCRIPTION	QUANTITIES	
42	TRAFFIC STRIPES, LATEX 4"	4,221	L.F.
43	TRAFFIC MARKING SYMBOLS, LATEX	362	S.F.
44	TRAFFIC STRIPES, 4"	1,755	L.F.
45	TRAFFIC STRIPES, 6"	1,277	L.F.
46	RPM, MONO-DIRECTIONAL, WHITE LENS	77	UN.
47	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 12"	80	L.F.
48	RPM, MONO-DIRECTIONAL, AMBER LENS	21	UN.
49	RPM, BI-DIRECTIONAL, AMBER LENS	3	UN.
50	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 6"	346	L.F.
51	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 8"	681	L.F.
52	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 24"	366	L.F.
53	TRAFFIC MARKING, SYMBOLS, LONG LIFE THERMOPLASTIC	362	S.F.
54	REGULATORY AND WARNING SIGN	35	S.F.

30' 20' 10' 0 30' 60'

SCALE 1" = 30'

NOTE:

1. RIGHT OF WAY PLOTTED FROM RECORD PLANS AND TAX MAPS.

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

STATE OF NEW JERSEY
BETH-ANN M. GRASSO
No. GE 43121
LICENSED PROFESSIONAL ENGINEER
TSS18

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

TRAFFIC SIGNING & STRIPING PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE 1"=30'	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 49 OF 84

C:\Users\CPENNONI\DESKTOP - OFFICE\96 Bowers & Bartlett SIGNSBAR_SHEETS\TSS-Layout
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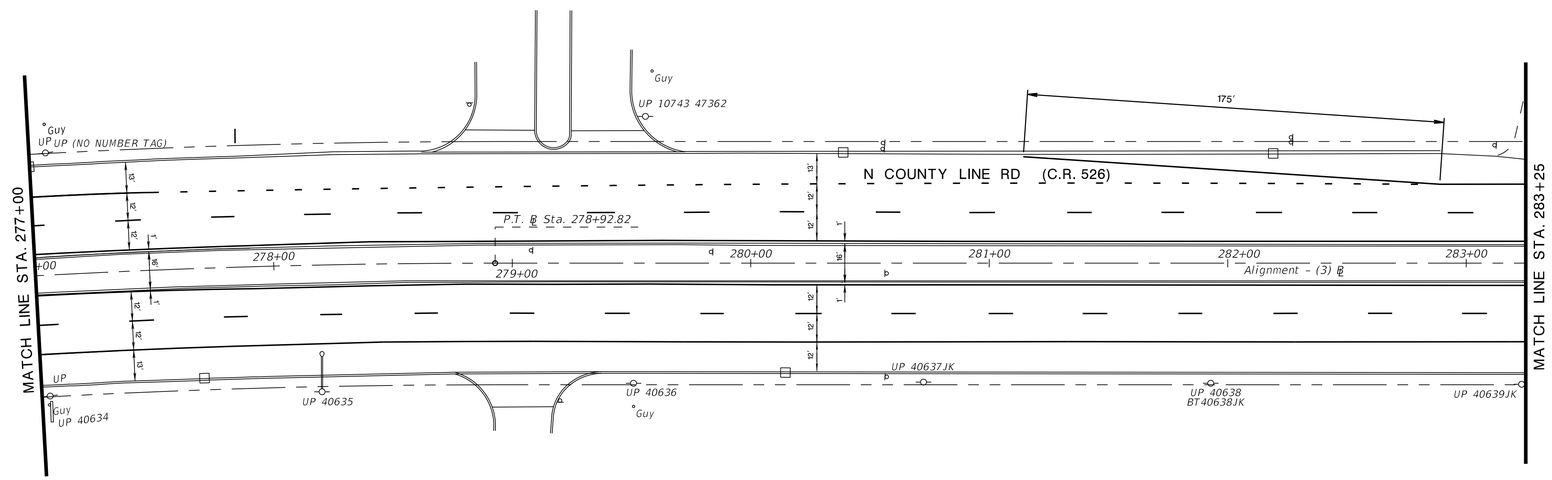

*PAVEMENT MARKING SCHEDULE

PAVEMENT MARKING	MATERIAL	DESCRIPTION	CONTRACT ITEM
CENTER LINE	EPOXY RESIN	4" DASHED 10' LINE WITH 30' GAPS OR SOLID YELLOW	610003M - TRAFFIC STRIPES, 4" (LF)
EDGE LINE	EPOXY RESIN	4" YELLOW OR 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DASHED LANE LINE	EPOXY RESIN	10' LINE WITH 30' GAPS, 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DOTTED LANE LINE	THERMOPLASTIC	3' LINE WITH 9' GAPS, 8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
DOTTED LANE EXTENSION LINE WITHIN AN INTERSECTION "CAT TRACKS"	THERMOPLASTIC	2' LINE WITH 4' GAPS, 4" YELLOW OR 6" WHITE	610109M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 4" (LF) 610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 6" (LF)
SOLID LANE LINE	THERMOPLASTIC	8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
CROSSWALK LINE	THERMOPLASTIC	24" WHITE, 6' LONG, SPACED AT 4' ON CENTER OR 12" WHITE, SPACED AT 7' ON CENTER	610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF) 610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
CROSSHATCH LINE	THERMOPLASTIC	24" YELLOW OR 24" WHITE SPACED 12' ON CENTER AT 45°	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
GORE LINE	THERMOPLASTIC	12" WHITE	610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
STOP LINE	THERMOPLASTIC	24" WHITE	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
SYMBOL - MARKING	THERMOPLASTIC	WHITE	610124M - TRAFFIC MARKINGS, SYMBOLS, LONG LIFE, THERMOPLASTIC (SF)

*UNLESS OTHERWISE NOTED

-THE MINIMUM DISTANCE BETWEEN STOP LINE AND CROSSWALK SHALL BE 4'.

-ALL LINES AND SIGNS SHALL CONFORM TO THE CURRENT "MUTCD".

-ALL SIGNS AND MARKINGS IN CONFLICT WITH PLAN SHALL BE REMOVED.

TO BE CONSTRUCTED

ITEM	DESCRIPTION	QUANTITIES	
42	TRAFFIC STRIPES, LATEX 4"	1,563	L.F.
44	TRAFFIC STRIPES, 4"	1,251	L.F.
45	TRAFFIC STRIPES, 6"	1,186	L.F.
46	RPM, MONO-DIRECTIONAL, WHITE LENS	27	UN.
48	RPM, MONO-DIRECTIONAL, AMBER LENS	16	UN.
50	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 6"	42	L.F.
51	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 8"	186	L.F.

NOTE:

1. RIGHT OF WAY PLOTTED FROM RECORD PLANS AND TAX MAPS.

30' 20' 10' 0 30' 60'

SCALE 1" = 30'

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

STATE OF NEW JERSEY BETH-ANN M. GRASSO No. GE 43121 LICENSED PROFESSIONAL ENGINEER TSS18

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

TRAFFIC SIGNING & STRIPING PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE 1"=30'	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 50 OF 84

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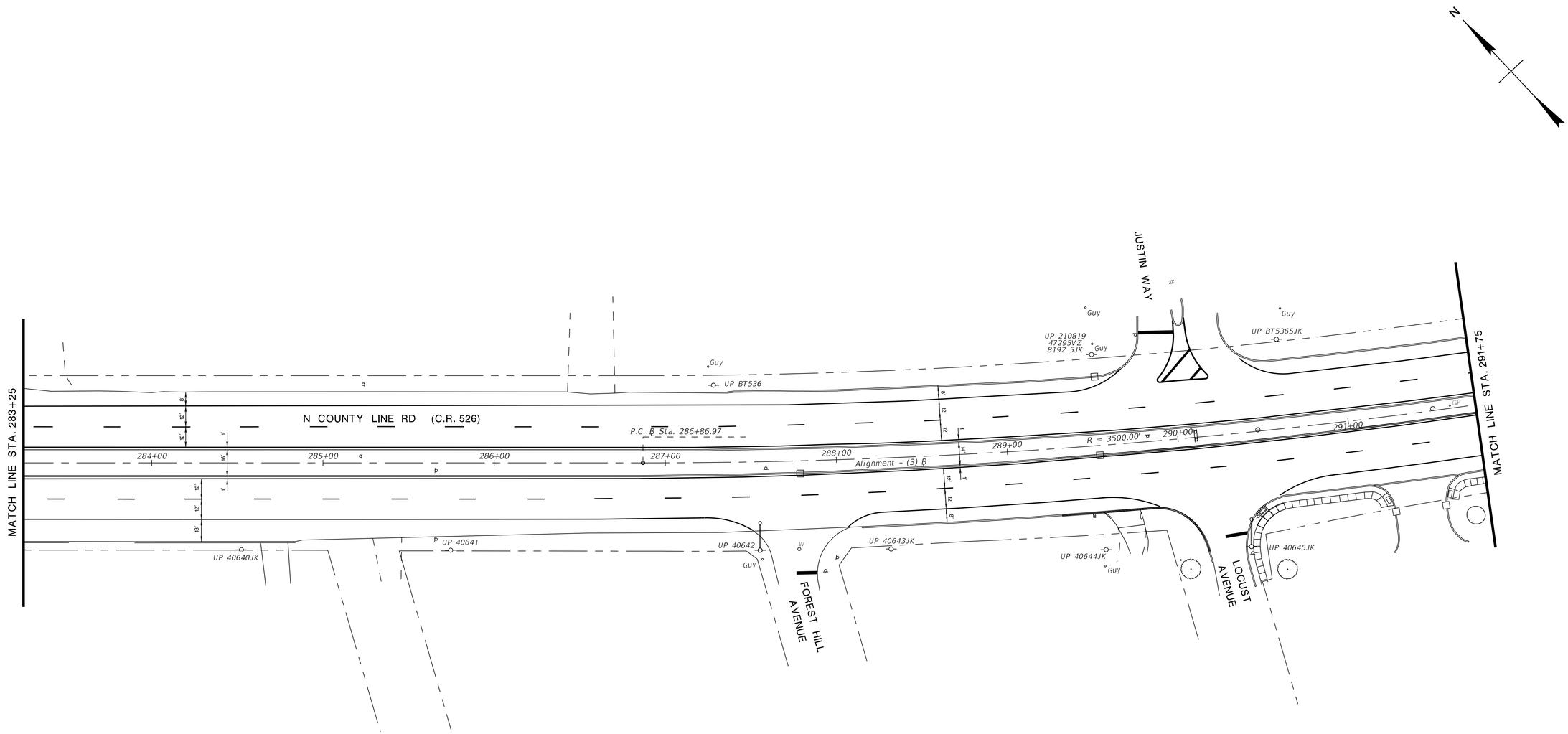
PENNONI - NEWARK, NJ



*PAVEMENT MARKING SCHEDULE

PAVEMENT MARKING	MATERIAL	DESCRIPTION	CONTRACT ITEM
CENTER LINE	EPOXY RESIN	4" DASHED 10' LINE WITH 30' GAPS OR SOLID YELLOW	610003M - TRAFFIC STRIPES, 4" (LF)
EDGE LINE	EPOXY RESIN	4" YELLOW OR 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DASHED LANE LINE	EPOXY RESIN	10' LINE WITH 30' GAPS, 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DOTTED LANE LINE	THERMOPLASTIC	3' LINE WITH 9' GAPS, 8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
DOTTED LANE EXTENSION LINE WITHIN AN INTERSECTION "CAT TRACKS"	THERMOPLASTIC	2' LINE WITH 4' GAPS, 4" YELLOW OR 6" WHITE	610109M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 4" (LF) 610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 6" (LF)
SOLID LANE LINE	THERMOPLASTIC	8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
CROSSWALK LINE	THERMOPLASTIC	24" WHITE, 6' LONG SPACED AT 4' ON CENTER OR 12" WHITE, SPACED AT 7' ON CENTER	610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF) 610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
CROSSHATCH LINE	THERMOPLASTIC	24" YELLOW OR 24" WHITE SPACED 12' ON CENTER AT 45°	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
GORE LINE	THERMOPLASTIC	12" WHITE	610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
STOP LINE	THERMOPLASTIC	24" WHITE	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
SYMBOL - MARKING	THERMOPLASTIC	WHITE	610124M - TRAFFIC MARKINGS, SYMBOLS, LONG LIFE, THERMOPLASTIC (SF)

*UNLESS OTHERWISE NOTED

-THE MINIMUM DISTANCE BETWEEN STOP LINE AND CROSSWALK SHALL BE 4'.

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TO BE CONSTRUCTED

ITEM	DESCRIPTION	QUANTITIES	
42	TRAFFIC STRIPES, LATEX 4"	2,396	L.F.
44	TRAFFIC STRIPES, 4"	1,700	L.F.
45	TRAFFIC STRIPES, 6"	1,991	L.F.
46	RPM, MONO-DIRECTIONAL, WHITE LENS	21	UN.
47	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 12"	36	L.F.
48	RPM, MONO-DIRECTIONAL, AMBER LENS	21	UN.
51	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 8"	108	L.F.
52	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 24"	47	L.F.

NOTE:

1. RIGHT OF WAY PLOTTED FROM RECORD PLANS AND TAX MAPS.

30' 20' 10' 0 30' 60'

SCALE 1" = 30'

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

STATE OF NEW JERSEY BETH-ANN M. GRASSO No. GE 43121 LICENSED PROFESSIONAL ENGINEER

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

TRAFFIC SIGNING & STRIPING PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE 1"=30'	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 51 OF 84

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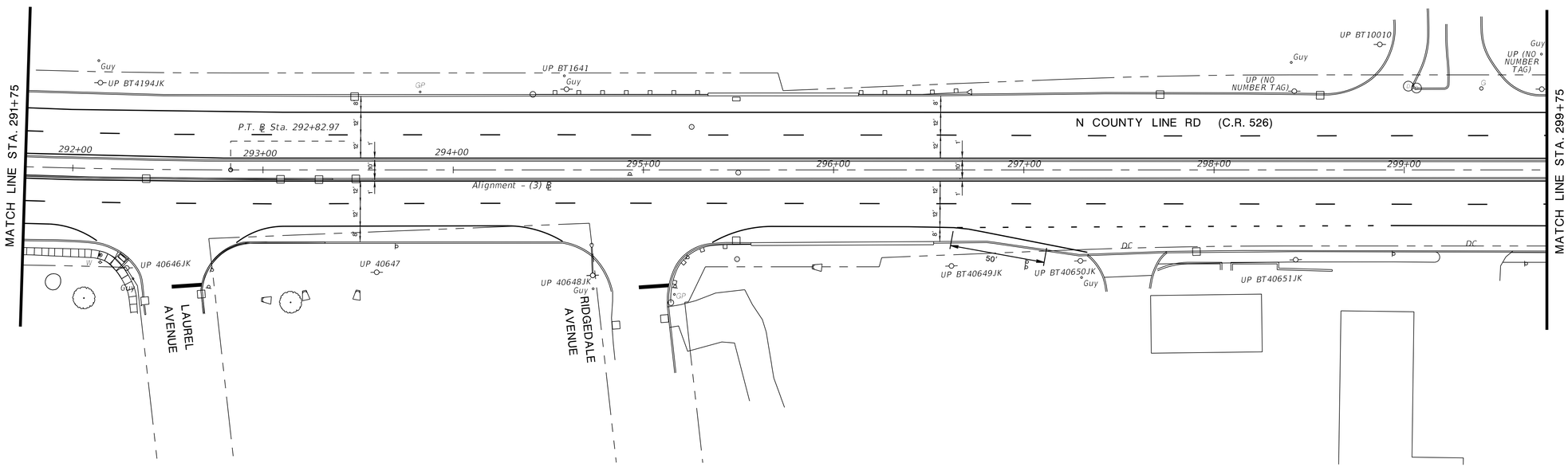

*PAVEMENT MARKING SCHEDULE

PAVEMENT MARKING	MATERIAL	DESCRIPTION	CONTRACT ITEM
CENTER LINE	EPOXY RESIN	4" DASHED 10' LINE WITH 30' GAPS OR SOLID YELLOW	610003M - TRAFFIC STRIPES, 4" (LF)
EDGE LINE	EPOXY RESIN	4" YELLOW OR 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DASHED LANE LINE	EPOXY RESIN	10' LINE WITH 30' GAPS, 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DOTTED LANE LINE	THERMOPLASTIC	3' LINE WITH 9' GAPS, 8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
DOTTED LANE EXTENSION LINE WITHIN AN INTERSECTION "CAT TRACKS"	THERMOPLASTIC	2' LINE WITH 4' GAPS, 4" YELLOW OR 6" WHITE	610109M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 4" (LF) 610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 6" (LF)
SOLID LANE LINE	THERMOPLASTIC	8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
CROSSWALK LINE	THERMOPLASTIC	24" WHITE, 6' LONG SPACED AT 4' ON CENTER OR 12" WHITE, SPACED AT 7' ON CENTER	610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF) 610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
CROSSHATCH LINE	THERMOPLASTIC	24" YELLOW OR 24" WHITE SPACED 12' ON CENTER AT 45°	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
GORE LINE	THERMOPLASTIC	12" WHITE	610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
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SYMBOL - MARKING	THERMOPLASTIC	WHITE	610124M - TRAFFIC MARKINGS, SYMBOLS, LONG LIFE, THERMOPLASTIC (SF)

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-ALL SIGNS AND MARKINGS IN CONFLICT WITH PLAN SHALL BE REMOVED.

TO BE CONSTRUCTED

ITEM	DESCRIPTION	QUANTITIES
42	TRAFFIC STRIPES, LATEX 4"	2,188 L.F.
44	TRAFFIC STRIPES, 4"	1,601 L.F.
45	TRAFFIC STRIPES, 6"	1,664 L.F.
46	RPM, MONO-DIRECTIONAL, WHITE LENS	26 UN.
48	RPM, MONO-DIRECTIONAL, AMBER LENS	19 UN.
51	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 8"	140 L.F.
52	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 24"	32 L.F.

NOTE:

1. RIGHT OF WAY PLOTTED FROM RECORD PLANS AND TAX MAPS.

30' 20' 10' 0 30' 60'

SCALE 1" = 30'

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

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4/28/2026

PROFESSIONAL ENGINEER
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OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

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TO BENNETTS MILLS RD

JACKSON TOWNSHIP

TRAFFIC SIGNING & STRIPING PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE 1"=30'	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 52 OF 84

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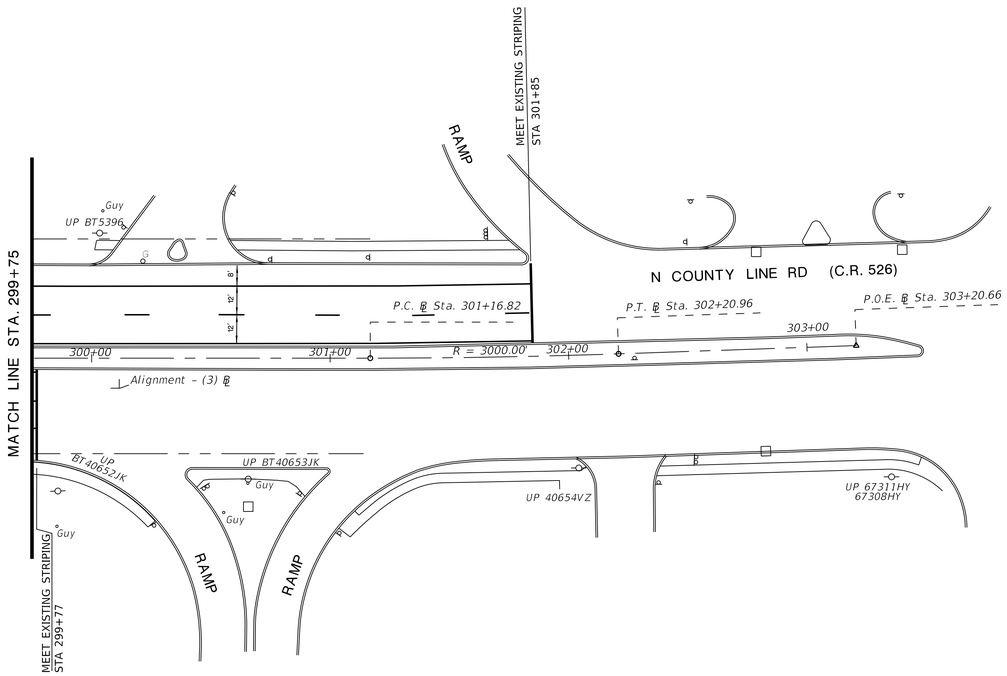
FORMAT: SIGNS 24



*PAVEMENT MARKING SCHEDULE

PAVEMENT MARKING	MATERIAL	DESCRIPTION	CONTRACT ITEM
CENTER LINE	EPOXY RESIN	4" DASHED 10' LINE WITH 30' GAPS OR SOLID YELLOW	610003M - TRAFFIC STRIPES, 4" (LF)
EDGE LINE	EPOXY RESIN	4" YELLOW OR 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DASHED LANE LINE	EPOXY RESIN	10' LINE WITH 30' GAPS, 6" WHITE	610006M - TRAFFIC STRIPES, 6" (LF)
DOTTED LANE LINE	THERMOPLASTIC	3' LINE WITH 9' GAPS, 8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
DOTTED LANE EXTENSION LINE WITHIN AN INTERSECTION "CAT TRACKS"	THERMOPLASTIC	2' LINE WITH 4' GAPS, 4" YELLOW OR 6" WHITE	610109M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 4" (LF) 610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 6" (LF)
SOLID LANE LINE	THERMOPLASTIC	8" WHITE	610115M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 8" (LF)
CROSSWALK LINE	THERMOPLASTIC	24" WHITE, 6' LONG SPACED AT 4' ON CENTER OR 12" WHITE, SPACED AT 7' ON CENTER	610112M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF) 610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
CROSSHATCH LINE	THERMOPLASTIC	24" YELLOW OR 24" WHITE SPACED 12' ON CENTER AT 45°	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
GORE LINE	THERMOPLASTIC	12" WHITE	610118M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 12" (LF)
STOP LINE	THERMOPLASTIC	24" WHITE	610121M - TRAFFIC MARKINGS, LINES, LONG LIFE, THERMOPLASTIC, 24" (LF)
SYMBOL - MARKING	THERMOPLASTIC	WHITE	610124M - TRAFFIC MARKINGS, SYMBOLS, LONG LIFE, THERMOPLASTIC (SF)

*UNLESS OTHERWISE NOTED

-THE MINIMUM DISTANCE BETWEEN STOP LINE AND CROSSWALK SHALL BE 4'.

-ALL LINES AND SIGNS SHALL CONFORM TO THE CURRENT "MUTCD".

-ALL SIGNS AND MARKINGS IN CONFLICT WITH PLAN SHALL BE REMOVED.

TO BE CONSTRUCTED

ITEM	DESCRIPTION	QUANTITIES	
42	TRAFFIC STRIPES, LATEX 4"	265	L.F.
44	TRAFFIC STRIPES, 4"	212	L.F.
45	TRAFFIC STRIPES, 6"	275	L.F.
46	RPM, MONO-DIRECTIONAL, WHITE LENS	10	UN.
48	RPM, MONO-DIRECTIONAL, AMBER LENS	3	UN.
51	TRAFFIC MARKING, LINES, LONG LIFE THERMOPLASTIC, 8"	2	L.F.

NOTE:

1. RIGHT OF WAY PLOTTED FROM RECORD PLANS AND TAX MAPS.

30' 20' 10' 0 30' 60'

SCALE 1" = 30'

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

STATE OF NEW JERSEY BETH-ANN M. GRASSO No. GE 43121 LICENSED PROFESSIONAL ENGINEER TSS18

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

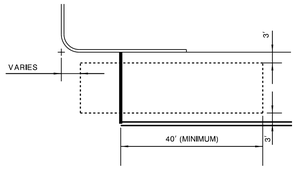
JACKSON TOWNSHIP

TRAFFIC SIGNING & STRIPING PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE 1"=30'	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 53 OF 84

G:\Accounts\OCEAN\2009\009 - Office of Roads & Bridges\Traffic Signals\009_SHEET53TSS-Map
ISO50% =8%-CS3H-CO-ALA-FILL-PR_ePlot\TCPS
Plotted: 10/28/2025

TYPICAL AREA OF DETECTION

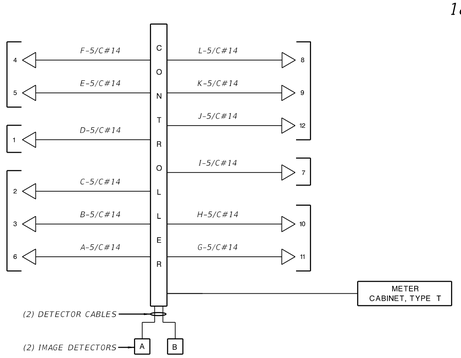


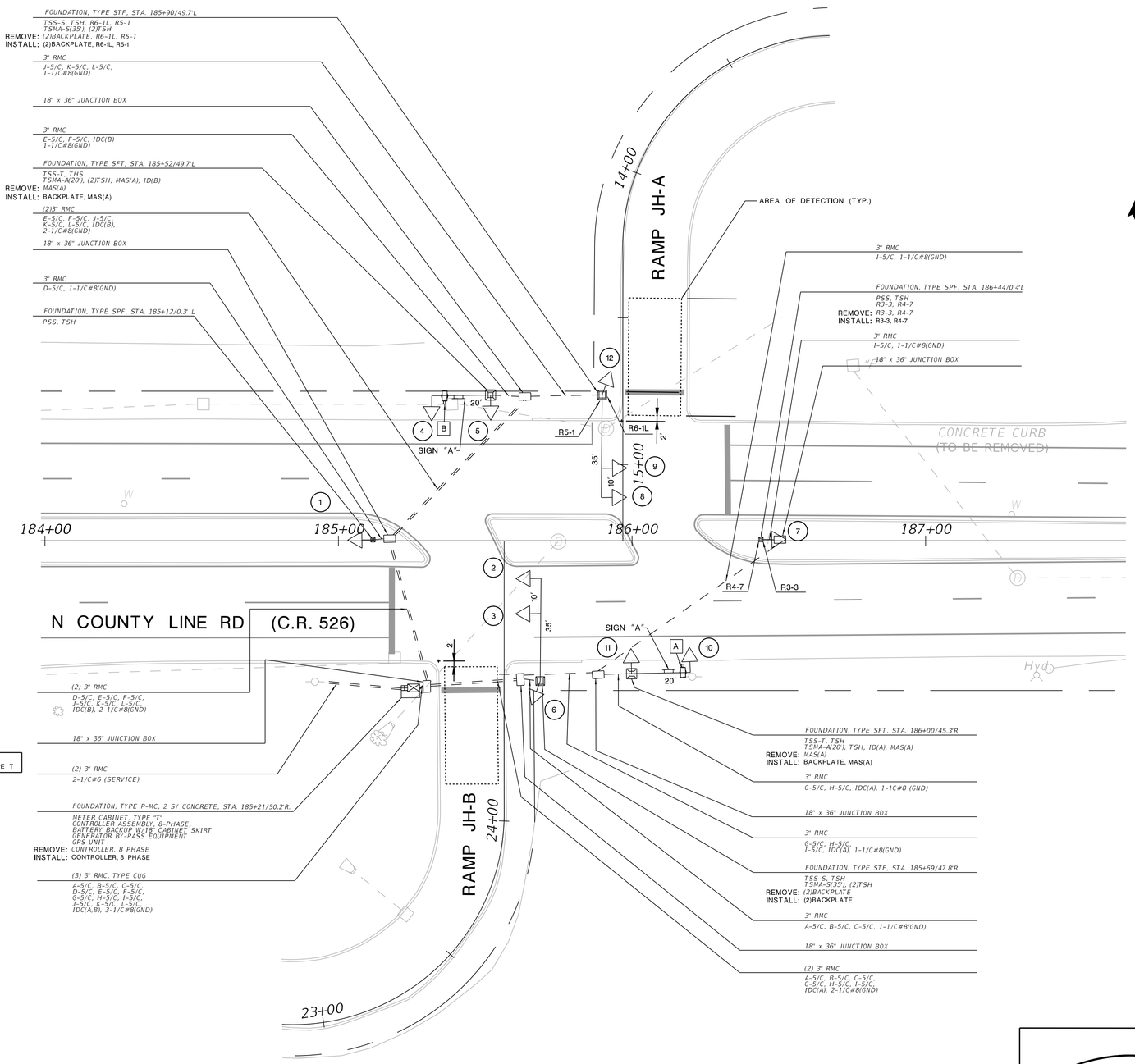
STATION & OFFSET LOCATION DETAIL
NOT TO SCALE

- X — STATION/OFFSET LOCATION CENTER
- SIGNAL FOUNDATION
- JUNCTION BOX
- CONTROLLER

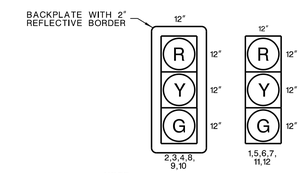
NOTE:
ALL SIGNAL POLE, JUNCTION BOXES, AND CONTROLLER FOUNDATIONS SHALL CONFORM TO NJDOT DETAILS

BLOCK WIRING DIAGRAM





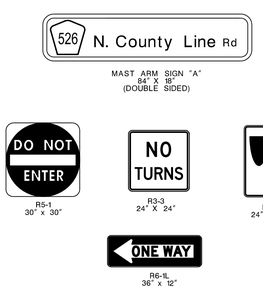
SIGNAL LEGEND



NOTE:

1. HEADS 1, 5, 6, 7, 11 & 12 ARE MOUNTED AT 10'.

SIGN LEGEND



GENERAL NOTES:

1. THE DESIGN AND INSTALLATION OF THIS SIGNAL HAS BEEN ACCOMPLISHED IN ACCORDANCE WITH THE CURRENT MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS (MUTCD).

2. THE ELECTRICAL SERVICE FOR THE INTERSECTION IS IN THE NAME OF THE MUNICIPALITY OF JACKSON TOWNSHIP.

3. PROPOSED FOUNDATIONS AND JUNCTION BOXES TO BE CONSTRUCTED OR RESET SHALL BE SET UTILIZING THE PROPOSED GRADES AS SHOWN IN THE CONSTRUCTION / GRADING PLANS. THE PROPOSED FOUNDATIONS AND JUNCTION BOXES SHALL CONFORM TO THE N.J.D.O.T. DETAILS. ANY FOUNDATION OR JUNCTION BOX THAT IS NOT CONSTRUCTED OR RESET IN ACCORDANCE WITH THE N.J.D.O.T DETAILS WILL BE RESET OR RECONSTRUCTED TO THE PROPER GRADES AT THE CONTRACTOR'S EXPENSE.

4. THIS PLAN SUPERSEDES PREVIOUS PLAN: 12-009-0120, 2-04, REVISION DATE, RONALD LOTRECCHIO

5. THE MANUAL CONTROL IS TO BE DISCONNECTED.

6. THE RIGHT OF WAY IS CALCULATED OR BASED ON PROVIDED AS-BUILT PLANS AND PUBLICLY AVAILABLE TAX MAP AND DEED INFORMATION.

7. THE GENERATOR TRANSFER SWITCH SHALL BE SET TO MANUAL.

8. THERE IS TO BE ONE GROUND WIRE PER CONDUIT. GROUND WIRE SHALL BE GROUNDED IN EACH JUNCTION BOX AND FOUNDATION VIA THE GROUNDING ROD.

9. THE WIRE SHALL BE THE SAME WIRE SHOWN IN THE BLOCK WIRING DIAGRAM FOR EACH SIGNAL COMPONENT. NO REDUCTION IN SIZE SHALL BE ALLOWED IN THE TRAFFIC SIGNAL STANDARD AND MAST ARM.

10. THE IMAGE DETECTION MUST BE INSTALLED AND OPERATIONAL PRIOR TO ANY MILLING OPERATIONS.

11. THE CONTRACTOR SHALL SUBMIT A COMPLETED OC DC-14 FOR EACH INTERSECTION IN THE PROJECT PRIOR TO COMMENCING WORK AT THE SIGNAL THAT WILL DIRECTLY AFFECT THE OPERATING TRAFFIC SIGNAL EQUIPMENT. THE CONTRACTOR IS RESPONSIBLE FOR THE TRAFFIC SIGNAL MAINTENANCE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS SECTION 702.03 CONSTRUCTION.

TO BE CONSTRUCTED

ITEM	DESCRIPTION	QUANTITIES	
55	MAST ARM STREET NAME SIGNS	21	S.F.
67	BACKPLATE	6	UN.
70	CONTROLLER TURN-ON	1	UN.
71	REGULATORY AND WARNING SIGN, SIGNAL MOUNTED	19	S.F.
72	CONTROLLER, 8 PHASE, REPLACEMENT	1	UN.

PLAN

20' 10' 0 20' 40'

SCALE 1" = 20'

STATE OF NEW JERSEY — BETH-ANN M. GRASSO — No. GE 43121 — LICENSED PROFESSIONAL ENGINEER — E06

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

DATE	REVISIONS	BY	CHK.

TRAFFIC SIGNAL JK-29

C.R.#9 (ROUTE 526 NORTH COUNTY LINE ROAD)
AND
RAMP AT STA 186+00

JACKSON TOWNSHIP

ELECTRICAL PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M	CHECKED B.M.G
	SCALE 1" = 20'	DRAWN R.M.E	APPROVED B.M.G
FILE NO. 12 - 009- 0120			SHEET 54 OF 84

4/27/2026
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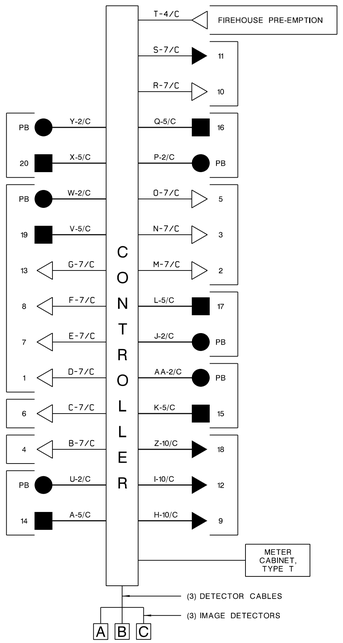
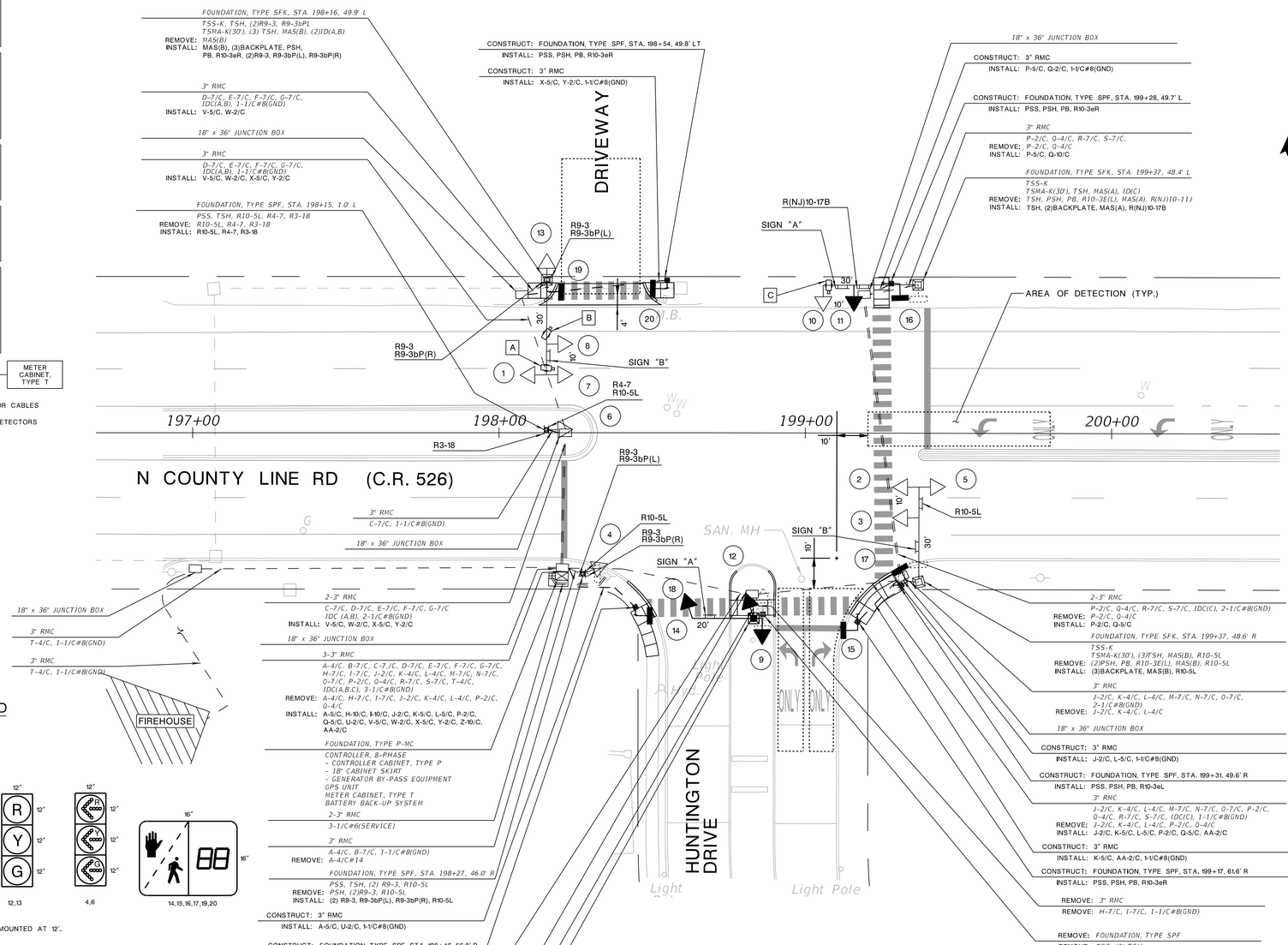
BLOCK WIRING DIAGRAM



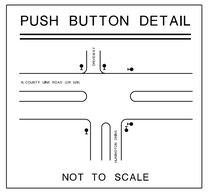


STATION & OFFSET LOCATION DETAIL
NOT TO SCALE

- X — STATION/OFFSET LOCATION CENTER
- SIGNAL FOUNDATION
- JUNCTION BOX
- CONTROLLER

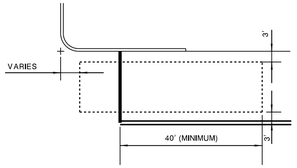
NOTE:
ALL SIGNAL POLE, JUNCTION BOXES, AND CONTROLLER FOUNDATIONS SHALL CONFORM TO NJDOT DETAILS

PUSH BUTTON DETAIL

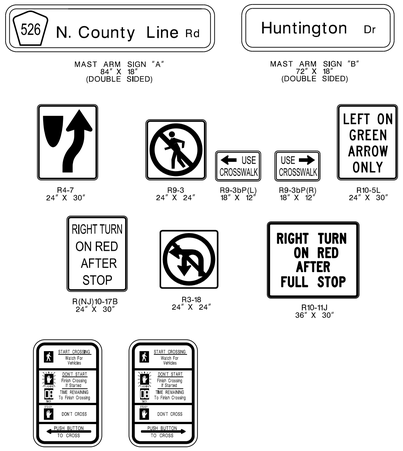


NOT TO SCALE

TYPICAL AREA OF DETECTION

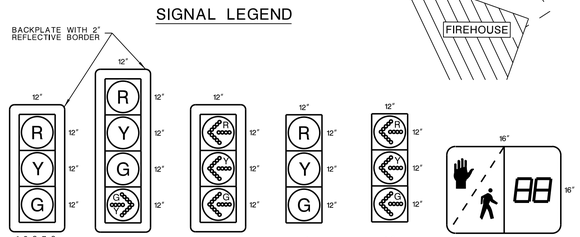


SIGN LEGEND



NOTE:
1. ALL R10-3i SIGNS SHALL BE MOUNTED TO AN ALUMINUM PANEL PER NJDOT SPECIFICATION 911.01.02A.

SIGNAL LEGEND



NOTES:
1. HEADS #4, #6, #9, #12 AND #13 ARE MOUNTED AT 12'.

PLAN

20' 10' 0 20' 40'

SCALE 1" = 20'

GENERAL NOTES:

1. THE DESIGN AND INSTALLATION OF THIS SIGNAL HAS BEEN ACCOMPLISHED IN ACCORDANCE WITH THE CURRENT MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS (MUTCD).

2. THE ELECTRICAL SERVICE FOR THE INTERSECTION IS IN THE NAME OF THE MUNICIPALITY OF JACKSON TOWNSHIP.

3. PROPOSED FOUNDATIONS AND JUNCTION BOXES TO BE CONSTRUCTED OR RESET SHALL BE SET UTILIZING THE PROPOSED GRADES AS SHOWN IN THE CONSTRUCTION / GRADING PLANS. THE PROPOSED FOUNDATIONS AND JUNCTION BOXES SHALL CONFORM TO THE N.J.D.O.T. DETAILS. ANY FOUNDATION OR JUNCTION BOX THAT IS NOT CONSTRUCTED OR RESET IN ACCORDANCE WITH THE N.J.D.O.T DETAILS WILL BE RESET OR RECONSTRUCTED TO THE PROPER GRADES AT THE CONTRACTOR'S EXPENSE.

4. THIS PLAN SUPERSEDES PREVIOUS PLAN: 12-009-0122, 4-09, REVISION DATE, RONALD LOTRECCHIO

5. THE MANUAL CONTROL IS TO BE DISCONNECTED.

6. THE RIGHT OF WAY IS CALCULATED OR BASED ON PROVIDED AS-BUILT PLANS AND PUBLICLY AVAILABLE TAX MAP AND DEED INFORMATION.

7. THE GENERATOR TRANSFER SWITCH SHALL BE SET TO MANUAL.

8. THERE IS TO BE ONE GROUND WIRE PER CONDUIT. GROUND WIRE SHALL BE GROUNDED IN EACH JUNCTION BOX AND FOUNDATION VIA THE GROUNDING ROD.

9. THE WIRE SHALL BE THE SAME WIRE SHOWN IN THE BLOCK WIRING DIAGRAM FOR EACH SIGNAL COMPONENT. NO REDUCTION IN SIZE SHALL BE ALLOWED IN THE TRAFFIC SIGNAL STANDARD AND MAST ARM.

10. THE IMAGE DETECTION MUST BE INSTALLED AND OPERATIONAL PRIOR TO ANY MILLING OPERATIONS.

11. THE CONTRACTOR SHALL SUBMIT A COMPLETED OC DC-14 FOR EACH INTERSECTION IN THE PROJECT PRIOR TO COMMENCING WORK AT THE SIGNAL THAT WILL DIRECTLY AFFECT THE OPERATING TRAFFIC SIGNAL EQUIPMENT. THE CONTRACTOR IS RESPONSIBLE FOR THE TRAFFIC SIGNAL MAINTENANCE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS SECTION 702.03 CONSTRUCTION.

TO BE CONSTRUCTED

ITEM	DESCRIPTION	QUANTITIES	
55	MAST ARM STREET NAME SIGNS	39	S.F.
56	3" RIGID METALLIC CONDUIT	145	L.F.
57	FOUNDATION, TYPE SPF	5	UN.
58	FOUNDATION, TYPE SFK	1	UN.
59	GROUND WIRE, NO. 8 AWG	145	L.F.
60	TRAFFIC SIGNAL STANDARD, ALUMINUM	1	UN.
61	PEDESTRIAN SIGNAL STANDARD	5	UN.
62	TRAFFIC SIGNAL MAST ARM, ALUMINUM	1	UN.
63	TRAFFIC SIGNAL CABLE, 2 CONDUCTOR	1,035	L.F.
64	TRAFFIC SIGNAL CABLE, 5 CONDUCTOR	1,095	L.F.
65	TRAFFIC SIGNAL CABLE, 10 CONDUCTOR	345	L.F.
66	TRAFFIC SIGNAL HEAD	4	UN.
67	BACKPLATE	9	UN.
68	PEDESTRIAN SIGNAL HEAD	6	UN.
69	PUSH BUTTON	6	UN.
71	REGULATORY AND WARNING SIGN, SIGNAL MOUNTED	47	EA.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

STATE OF NEW JERSEY — BETH-ANN M. GRASSO — No. GE 43121 — LICENSED PROFESSIONAL ENGINEER

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

DATE	REVISIONS	BY	CHK.

TRAFFIC SIGNAL JK-33
C.R.#9 (ROUTE 526 NORTH COUNTY LINE ROAD)
AND
HUNTINGTON DRIVE
JACKSON TOWNSHIP

ELECTRICAL PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M	CHECKED B.M.G
	SCALE 1" = 20'	DRAWN R.M.E	APPROVED B.M.G
FILE NO. 12 - 009- 0122			SHEET 55 OF 84

4/27/2026
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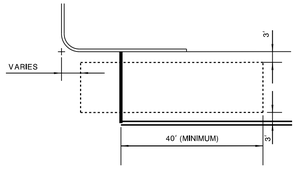
TO BE CONSTRUCTED		
ITEM	DESCRIPTION	QUANTITIES
55	MAST ARM STREET NAME SIGNS	21 S.F.
67	BACKPLATE	6 UN.
70	CONTROLLER TURN-ON	1 UN.
71	REGULATORY AND WARNING SIGN, SIGNAL MOUNTED	3 S.F.
72	CONTROLLER, 8 PHASE, REPLACEMENT	1 UN.

TYPICAL AREA OF DETECTION

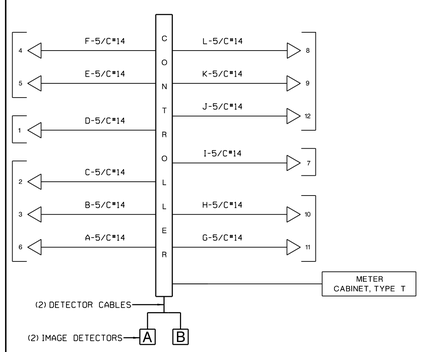


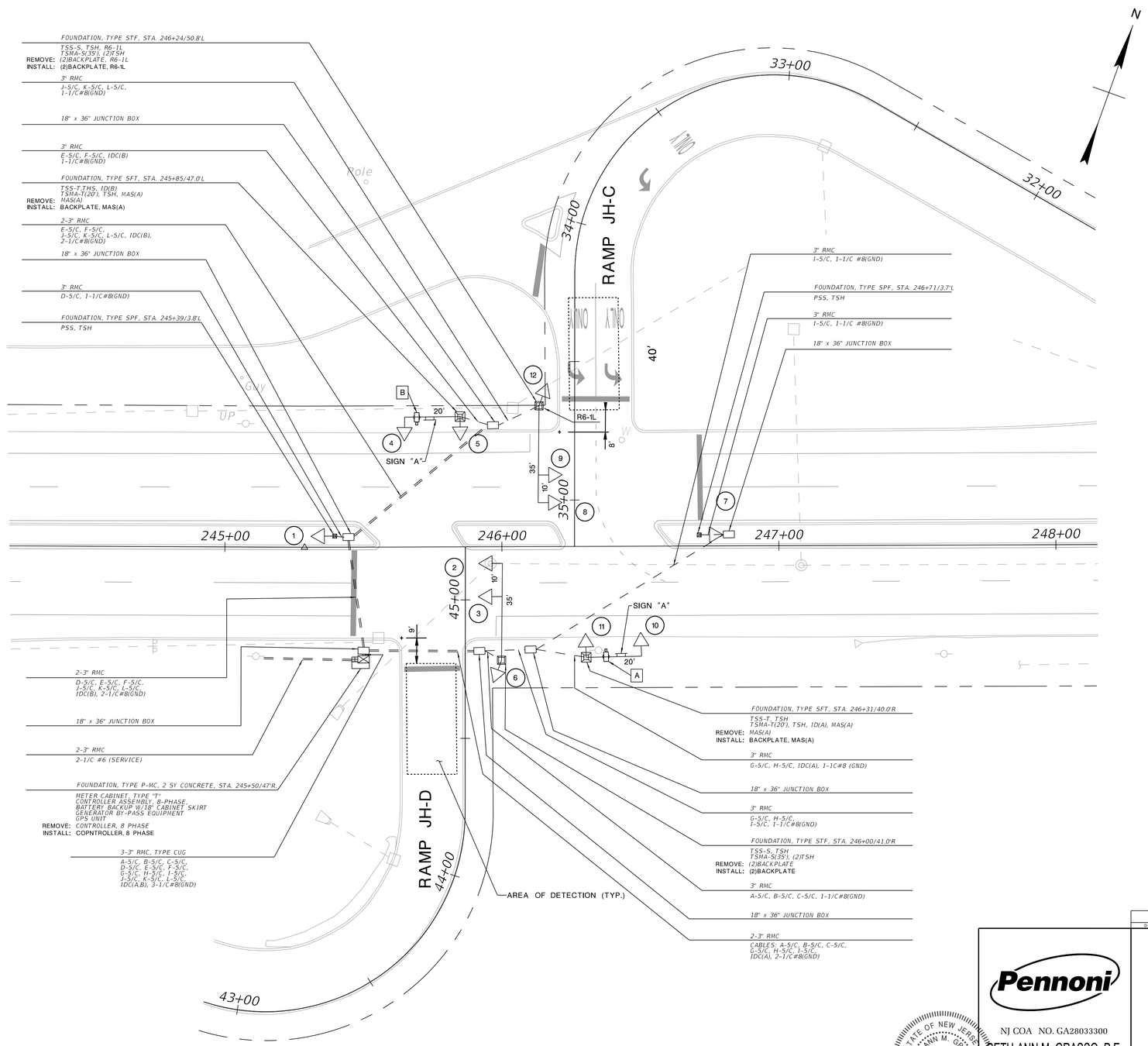
STATION & OFFSET LOCATION DETAIL
NOT TO SCALE

- X — STATION/OFFSET LOCATION CENTER
- SIGNAL FOUNDATION
- JUNCTION BOX
- CONTROLLER

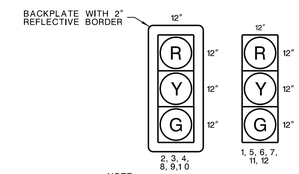
NOTE:
ALL SIGNAL POLE, JUNCTION BOXES, AND CONTROLLER FOUNDATIONS SHALL CONFORM TO NJDOT DETAILS

BLOCK WIRING DIAGRAM





SIGNAL LEGEND



NOTE:

1. HEADS 1, 5, 6, 7, 11 & 12 ARE MOUNTED AT 10'.

SIGN LEGEND

MAST ARM SIGN "A"
84" X 18"
(DOUBLE SIDED)

R6-1L
36" x 12"

GENERAL NOTES:

1. THE DESIGN AND INSTALLATION OF THIS SIGNAL HAS BEEN ACCOMPLISHED IN ACCORDANCE WITH THE CURRENT MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS (MUTCD).

2. THE ELECTRICAL SERVICE FOR THE INTERSECTION IS IN THE NAME OF THE MUNICIPALITY OF JACKSON TOWNSHIP.

3. PROPOSED FOUNDATIONS AND JUNCTION BOXES TO BE CONSTRUCTED OR RESET SHALL BE SET UTILIZING THE PROPOSED GRADES AS SHOWN IN THE CONSTRUCTION / GRADING PLANS. THE PROPOSED FOUNDATIONS AND JUNCTION BOXES SHALL CONFORM TO THE N.J.D.O.T. DETAILS. ANY FOUNDATION OR JUNCTION BOX THAT IS NOT CONSTRUCTED OR RESET IN ACCORDANCE WITH THE N.J.D.O.T DETAILS WILL BE RESET OR RECONSTRUCTED TO THE PROPER GRADES AT THE CONTRACTOR'S EXPENSE.

4. THIS PLAN SUPERSEDES PREVIOUS PLAN: 12-009-0119, 2-04, REVISION DATE, RONALD LOTRECCHIO

5. THE MANUAL CONTROL IS TO BE DISCONNECTED.

6. THE RIGHT OF WAY IS CALCULATED OR BASED ON PROVIDED AS-BUILT PLANS AND PUBLICLY AVAILABLE TAX MAP AND DEED INFORMATION.

7. THE GENERATOR TRANSFER SWITCH SHALL BE SET TO MANUAL.

8. THERE IS TO BE ONE GROUND WIRE PER CONDUIT. GROUND WIRE SHALL BE GROUNDED IN EACH JUNCTION BOX AND FOUNDATION VIA THE GROUNDING ROD.

9. THE WIRE SHALL BE THE SAME WIRE SHOWN IN THE BLOCK WIRING DIAGRAM FOR EACH SIGNAL COMPONENT. NO REDUCTION IN SIZE SHALL BE ALLOWED IN THE TRAFFIC SIGNAL STANDARD AND MAST ARM.

10. THE IMAGE DETECTION MUST BE INSTALLED AND OPERATIONAL PRIOR TO ANY MILLING OPERATIONS.

11. THE CONTRACTOR SHALL SUBMIT A COMPLETED OC DC-14 FOR EACH INTERSECTION IN THE PROJECT PRIOR TO COMMENCING WORK AT THE SIGNAL THAT WILL DIRECTLY AFFECT THE OPERATING TRAFFIC SIGNAL EQUIPMENT. THE CONTRACTOR IS RESPONSIBLE FOR THE TRAFFIC SIGNAL MAINTENANCE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS SECTION 702.03 CONSTRUCTION.

PLAN

20' 10' 0 20' 40'

SCALE 1" = 20'

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

STATE OF NEW JERSEY — BETH-ANN M. GRASSO — No. GE 43121 — LICENSED PROFESSIONAL ENGINEER — E06

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

TRAFFIC SIGNAL JK–31
C.R.#9 (ROUTE 526 NORTH COUNTY LINE ROAD)
AND
RAMP AT STA. 246+00
JACKSON TOWNSHIP

ELECTRICAL PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M	CHECKED B.M.G
	SCALE 1" = 20'	DRAWN R.M.E	APPROVED B.M.G
FILE NO. 12 - 009- 0119			SHEET 56 OF 84

4/27/2026
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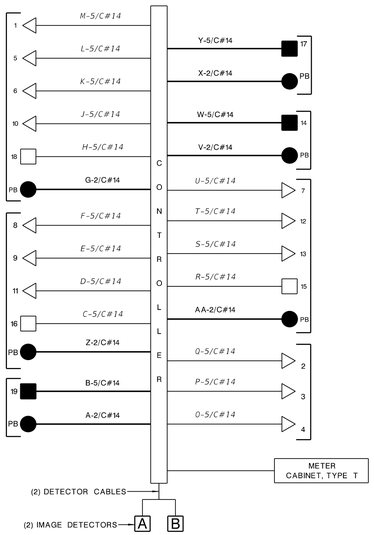
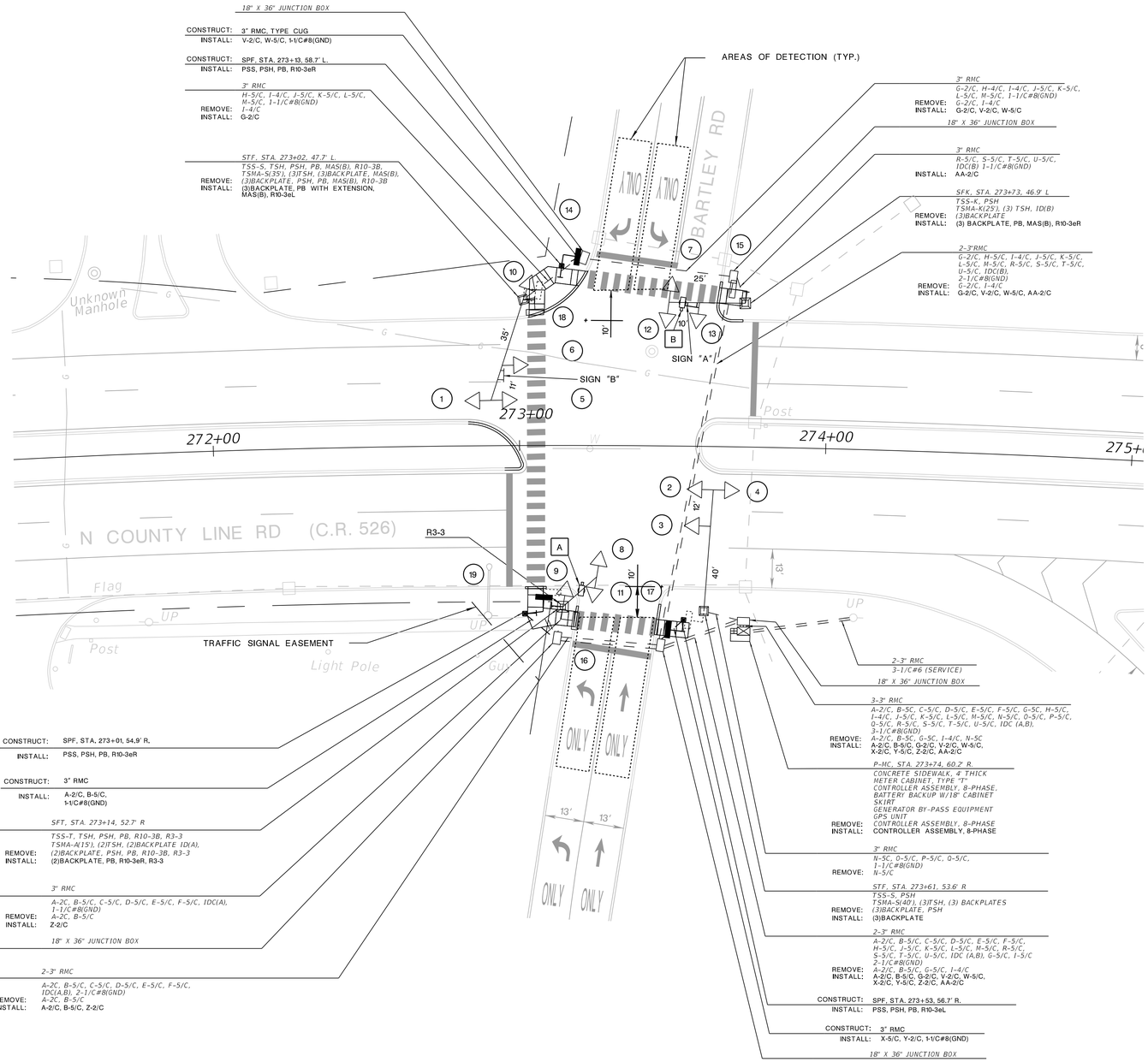
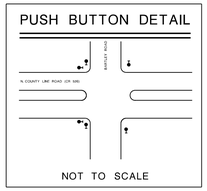
BLOCK WIRING DIAGRAM

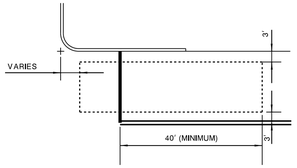




PUSH BUTTON DETAIL



TYPICAL AREA OF DETECTION

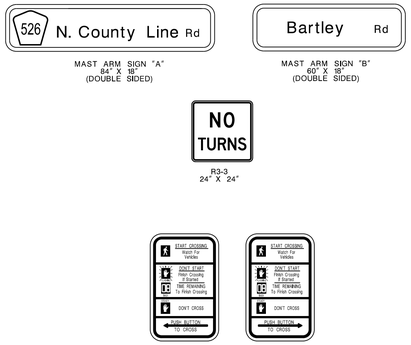


STATION & OFFSET LOCATION DETAIL
NOT TO SCALE

- X — STATION/OFFSET LOCATION CENTER
- SIGNAL FOUNDATION
- JUNCTION BOX
- CONTROLLER

NOTE:
ALL SIGNAL POLE, JUNCTION BOXES, AND CONTROLLER FOUNDATIONS SHALL CONFORM TO NJDOT DETAILS

SIGN LEGEND



NOTE:

1. ALL R10-3i SIGNS SHALL BE MOUNTED TO AN ALUMINUM PANEL PER NJDOT SPECIFICATION 911.01.02A.

GENERAL NOTES:

1. THE DESIGN AND INSTALLATION OF THIS SIGNAL HAS BEEN ACCOMPLISHED IN ACCORDANCE WITH THE CURRENT MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS (MUTCD).

2. THE ELECTRICAL SERVICE FOR THE INTERSECTION IS IN THE NAME OF THE MUNICIPALITY OF JACKSON TOWNSHIP.

3. PROPOSED FOUNDATIONS AND JUNCTION BOXES TO BE CONSTRUCTED OR RESET SHALL BE SET UTILIZING THE PROPOSED GRADES AS SHOWN IN THE CONSTRUCTION / GRADING PLANS. THE PROPOSED FOUNDATIONS AND JUNCTION BOXES SHALL CONFORM TO THE N.J.D.O.T DETAILS. ANY FOUNDATION OR JUNCTION BOX THAT IS NOT CONSTRUCTED OR RESET IN ACCORDANCE WITH THE N.J.D.O.T DETAILS WILL BE RESET OR RECONSTRUCTED TO THE PROPER GRADES AT THE CONTRACTOR'S EXPENSE.

4. THIS PLAN SUPERSEDES PREVIOUS PLAN: 12-009-0121, 2-04, REVISION DATE, RONALD LOTRECCHIO

5. THE MANUAL CONTROL IS TO BE DISCONNECTED.

6. THE RIGHT OF WAY IS CALCULATED OR BASED ON PROVIDED AS-BUILT PLANS AND PUBLICLY AVAILABLE TAX MAP AND DEED INFORMATION.

7. THE GENERATOR TRANSFER SWITCH SHALL BE SET TO MANUAL. *FOR NEW SIGNALS ONLY*

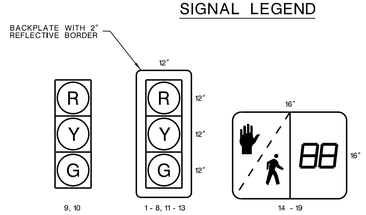
8. THERE IS TO BE ONE GROUND WIRE PER CONDUIT, GROUND WIRE SHALL BE GROUNDED IN EACH JUNCTION BOX AND FOUNDATION VIA THE GROUNDING ROD.

9. THE WIRE SHALL BE THE SAME WIRE SHOWN IN THE BLOCK WIRING DIAGRAM FOR EACH SIGNAL COMPONENT. NO REDUCTION IN SIZE SHALL BE ALLOWED IN THE TRAFFIC SIGNAL STANDARD AND MAST ARM.

10. THE IMAGE DETECTION MUST BE INSTALLED AND OPERATIONAL PRIOR TO ANY MILLING OPERATIONS.

11. THE CONTRACTOR SHALL SUBMIT A COMPLETED OC DC-14 FOR EACH INTERSECTION IN THE PROJECT PRIOR TO COMMENCING WORK AT THE SIGNAL THAT WILL DIRECTLY AFFECT THE OPERATING TRAFFIC SIGNAL EQUIPMENT. THE CONTRACTOR IS RESPONSIBLE FOR THE TRAFFIC SIGNAL MAINTENANCE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS SECTION 702.03 CONSTRUCTION.

SIGNAL LEGEND



NOTES:

1. HEADS #9 AND #10 ARE MOUNTED AT 12".

TO BE CONSTRUCTED

ITEM	DESCRIPTION	QUANTITIES	
55	MAST ARM STREET NAME SIGNS	18	S.F.
56	3" RIGID METALLIC CONDUIT	45	L.F.
57	FOUNDATION, TYPE SPF	3	UN.
59	GROUND WIRE, NO. 8 AWG	45	L.F.
61	PEDESTRIAN SIGNAL STANDARD	3	UN.
63	TRAFFIC SIGNAL CABLE, 2 CONDUCTOR	1,110	L.F.
64	TRAFFIC SIGNAL CABLE, 5 CONDUCTOR	480	L.F.
67	BACKPLATE	10	UN.
68	PEDESTRIAN SIGNAL HEAD	3	UN.
69	PUSH BUTTON	6	UN.
70	CONTROLLER TURN-ON	1	UN.
71	REGULATORY AND WARNING SIGN, SIGNAL MOUNTED	4	S.F.
72	CONTROLLER, 8 PHASE, REPLACEMENT	1	UN.

PLAN

20' 10' 0 20' 40'

SCALE 1" = 20'

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

STATE OF NEW JERSEY — BETH-ANN M. GRASSO — No. GE 43121 — LICENSED PROFESSIONAL ENGINEER — E06

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

TRAFFIC SIGNAL JK-30

C.R.#9 (ROUTE 526 NORTH COUNTY LINE ROAD)
AND
C.R.#16 (BARTLEY ROAD)

JACKSON TOWNSHIP

ELECTRICAL PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M	CHECKED B.M.G
	SCALE 1" = 20'	DRAWN R.M.E	APPROVED B.M.G
FILE NO. 12 - 009- 0128			SHEET 57 OF 84

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JK-29

C.R. #9 (ROUTE 526/NORTH COUNTY LINE ROAD & JUGHANDLE A-B

JACKSON TOWNSHIP

JK-29
CR#9 (Route 526/ N. County Line Road)
Jughandle A-B (Station 186+00)
Jackson Township

90 SECOND BACKGROUND AND 41-49 SECOND VARIABLE CYCLE LENGTH

	Phase	Signal Faces 1-3	4-6	7-9	10-12	Time (Seconds) Plan I 90 Sec Cycle	Plan II 90 Sec Cycle	Plan III 90 Sec Cycle	Plan IV FREE Cycle
	NO PEDESTRIAN ACTUATION								
A)	N. County Line Road ROW	G	R	G	R	69-56	69-56	69-56	20MIN.
	Change	Y	R	Y	R	5*	5*	5*	5
	Clearance	R	R	R	R	3	3	3	3
B)	Jughandle A-B ROW	R	G	R	G	7-20	7-20	7-20	7-15
	Change	R	Y	R	Y	3	3	3	3
	Clear	R	R	R	R	3	3	3	3
Emergency Flash		Y	R	Y	R	-	-	-	-
Offset						62	63	33	-

Notes:

1. The signal shall rest in Phase A - North County Line Road ROW.
2. The vehicle interval is to be set at 2 seconds for Phase B.
3. The manual control is to be disconnected.
4. *Offsets (in seconds) are measured from the beginning of yellow to North County Line Road ROW at Jughandle "A" - Shop Rite to the beginning of yellow to North County Line Road ROW at this intersection.
5. The following is the Time-Of-Day schedule:

6:00 AM to 10:30 AM	Monday to Friday	Plan I
10:30 AM to 1:00 PM	Monday to Friday	Plan II
1:00 PM to 11:00 PM	Monday to Friday	Plan III
6:00 AM to 11:00 PM	Saturday and Sunday	Plan III
11:00 PM to 6:00 AM	Daily	Plan IV

JK-33

C.R. #9 (ROUTE 526/NORTH COUNTY LINE ROAD & HUNTINGTON DRIVE

JACKSON TOWNSHIP

JK-33
C.R. #9 (Route 526/ N. County Line Road)
Huntington Drive
Jackson Township

90 SECOND BACKGROUND & 55-100 VARIABLE CYCLE LENGTHS

	Phase	Signal Faces 1-3	4-6	7-8	9, 10	11	12-13	14,15, 19,20	16,17	Time (Seconds) Plan I 90 sec Cycle	Plan II 90 sec Cycle	Plan III 90 sec Cycle	Plan IV FREE Cycle
	NO PEDESTRIAN ACTUATION												
A)	N. County Line Road NB Lead	R	<G-	G	R	R,-G>	R	DW	DW	7-12	7-16	7-15	7-10
	Change	R	<Y-	G	R	R,-Y>	R	DW	DW	3	3	3	3
	Clearance	R	<R-	G	R	R	R	DW	DW	3	3	3	3
B)	N. County Line Road ROW	G	<R-	G	R	R	R	DW	DW	55-38	55-34	55-35	20 MIN.
	Change	Y	<R-	Y[6]	R	R	R	DW	DW	5*	5*	5*	5
	Clearance	R	<R-	R[6]	R	R	R	DW	DW	3	3	3	3
C)	Huntington Drive ROW	R	<R-	R	G	G	G	DW	DW	7-19	7-19	7-19	7-10
	Change	R	<R-	R	Y	Y	Y	DW	DW	3	3	3	3
	Clearance	R	<R-	R	R	R	R	DW	DW	4	4	4	4
	PEDESTRIAN ACTUATION												
A)	N. County Line Road NB Lead	R	<G-	G	R	R,-G>	R	DW	DW	7-12	7-16	7-15	7-10
	Change	R	<Y-	G	R	R,-Y>	R	DW	DW	3	3	3	3
	Clearance	R	<R-	G	R	R	R	DW	DW	3	3	3	3
B)	N. County Line Road ROW	G	<R-	G	R	R	R	W	DW	7	7	7	10 MIN.
	Pedestrian Clearance	G	<R-	G	R	R	R	FDW	DW	22	22	22	22
	Change	Y	<R-	Y[6]	R	R	R	DW	DW	5*	5*	5*	5
	Clearance	R	<R-	R[6]	R	R	R	DW	DW	3	3	3	3
C)	Huntington Drive ROW	R	<R-	R	G	G	G	DW	W	11	11	11	11
	Pedestrian Clearance	R	<R-	R	G	G	G	DW	FDW	26**	26**	26**	26
	Change	R	<R-	R	Y	Y	Y	DW	DW	3	3	3	3
	Clearance	R	<R-	R	R	R	R	DW	DW	3	3	3	3
Emergency Flash		Y	<R-	Y	R	R	R	DARK	DARK	-	-	-	-
Offset										52	61	11	-

Notes:

1. The signal shall rest in Phase B - North County Line Road ROW
3. The vehicle interval is to be set at 2 seconds for Phases A and C.
4. The manual control is to be disconnected.
5. *Offsets (in seconds) are measured from the beginning of yellow to North County Line Road ROW at Jughandle "A" - Shop Rite to the beginning of yellow to North County Line Road ROW at this intersection.
6. If Phase C is skipped, signal shall display G.
7. ** When pedestrian actution cause the timing to extend the background cycle, the local cycle counter for the controller shall be frozen at the force off point of that phase until the end of the pedestrian clearance interval. The cycle counter shall then resume timing from where it left off and the controller will immediately begin offset seeking until it gets back into step.
8. Pedestrian recycle is on.
9. The following is the Time-Of-Day schedule:

6:00 AM to 10:30 AM	Monday to Friday	Plan I
10:30 AM to 1:00 PM	Monday to Friday	Plan II
1:00 PM to 11:00 PM	Monday to Friday	Plan III
6:00 AM to 11:00 PM	Saturday and Sunday	Plan III
11:00 PM to 6:00 AM	Daily	Plan IV

Emergency Pre-emption

- The intersection shall have a controller with internal fire pre-emption features.
- Push button controll pre-emption is permittd from the Firehouse to this intersection.
- The device shall guarantee 17 seconds of green to Phase B North County Line Road ROW in the normal operation.
- The controller shall guarantee 7 seconds of minimum green time to Phase A North County Line Road WB Lead Left , 10 seconds of minimum green time to Phase C Huntington Drive, pedestrian clearances, yellow change and all red clearance times before the pre-emption sequence is initiated in the normal operation.
- If the controller is replaced with one without internal pre-emption, the pre-emption device shall be disconnected.
- Normal operation shall commence to Phase B - North County Line ROW once pre-emption has terminated and coordination shall be re-established.
- Pre-emption of an approach shall provide ROW to both directions of traffic associated with that approach.

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

STATE OF NEW JERSEY BETH-ANN M. GRASSO No. GE 43121 LICENSED PROFESSIONAL ENGINEER E06

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

TRAFFIC SIGNAL

C.R. 526 COUNTY LINE ROAD AND HUNTINGTON DRIVE

JACKSON TOWNSHIP

TIMING DIRECTIVES

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M	CHECKED B.M.G
	SCALE 1" = 20'	DRAWN R.M.E	APPROVED B.M.G
FILE NO. 12 - 009- 0128			SHEET 58 OF 84

4/27/2026
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JK-31

C.R. #9 (ROUTE 526/NORTH COUNTY LINE ROAD & JUGHANDLE "C-D" - SHOP RITE JACKSON TOWNSHIP

JK-31
CR#9 (Route 526/ N. County Line Road)
Jughandle "C-D" - Shop Rite (Station 246+00)
Jackson Township

90 SECOND BACKGROUND AND 41-54 VARIABLE CYCLE LENGTHS

Phase		Signal Faces 1-3	4-6	7-9	10-12	Time (Seconds) Plan I 90 Sec Cycle	Plan II 90 Sec Cycle	Plan III 90 Sec Cycle	Plan IV FREE Cycle
	NO PEDESTRIAN ACTUATION								
A)	N. County Line Road ROW	G	R	G	R	69-52	69-52	69-52	20 MIN.
	Change	Y	R	Y	R	5*	5*	5*	5
	Clearance	R	R	R	R	3	3	3	3
B)	Ramps JH-C & JH-D ROW	R	G	R	G	7-24	7-24	7-24	7-20
	Change	R	Y	R	Y	3	3	3	3
	Clear	R	R	R	R	3	3	3	3
Emergency Flash		Y	R	Y	R	-	-	-	-
Offset						0	0	0	-

Notes:

1. The signal shall rest in Phase A - North County Line Road ROW
2. The vehicle interval is to be set at 2 secondsfor Phase B.
3. The manual control is to be disconnected.
4. *Offsets (in seconds) are measured from the beginning of yellow to North County Line Road ROW at this intersection.
5. The following is the Time-Of-Day schedule:

6:00 AM to 10:30 AM	Monday to Friday	Plan I
10:30 AM to 1:00 PM	Monday to Friday	Plan II
1:00 PM to 11:00 PM	Monday to Friday	Plan III
6:00 AM to 11:00 PM	Saturday and Sunday	Plan III
11:00 PM to 6:00 AM	Daily	Plan IV

JK-30

C.R. #9 (ROUTE 526/NORTH COUNTY LINE ROAD & C.R. #16 (BARTLEY ROAD) JACKSON TOWNSHIP

JK-30
CR #9 (Route 526/ N. County Line Road)
CR #16 (Bartley Road)
Jackson Township

90 SECOND BACKGROUND CYCLE LENGTH & 42-76 VARIABLE CYCLE LENGTHS

Phase		Signal Faces 1-3	4-6	7-10	11-13	14-17	18,19	Time (Seconds) Plan I 90 sec Cycle	Plan II 90 sec Cycle	Plan III 90 sec Cycle	Plan IV FREE Cycle
	NO PEDESTRIAN ACTUATION										
A)	N. County Line Rd. ROW	G	G	R	R	DW	DW	68-34	68-41	68-34	20
	Change	Y	Y	R	R	DW	DW	5*	5*	5*	5
	Clearance	R	R	R	R	DW	DW	3	3	3	3
B)	Bartley Road ROW	R	R	G	G	DW	DW	7-41	7-34	7-41	7-20
	Change	R	R	Y	Y	DW	DW	3	3	3	3
	Clearance	R	R	R	R	DW	DW	4	4	4	4
	PEDESTRIAN ACTUATION										
A)	N. County Line Rd. ROW	G	G	R	R	W	DW	24-20	24	24-20	10 MIN.
	Pedestrian Clearance	G	G	R	R	FDW	DW	14	14	14	14
	Change	Y	Y	R	R	DW	DW	5	5	5	5
	Clearance	R	R	R	R	DW	DW	3	3	3	3
B)	Bartley Road ROW	R	R	G	G	DW	W	9	9	9	9
	Pedestrian Clearance	R	R	G	G	DW	FDW	28**	28**	28**	28
	Vehicle extensione	R	R	G	G	DW	FDW	0-4	0	0-4	0
	Change	R	R	Y	Y	DW	DW	3	3	3	3
	Clearance	R	R	R	R	DW	DW	4	4	4	4
Emergency		Y	Y	R	R	DARK	DARK	-	-	-	-
Offset								52	62	89	-

Notes:

1. The signal shall rest in Phase A - North County Line Road ROW
2. The vehicle interval is to be set at 2 seconds for Phase B.
3. The manual control is to be disconnected.
4. *Offsets (in seconds) are measured from the beginning of yellow to North County Line Road ROW at Jughandle "A" - Shop Rite to the beginning of yellow to North County Line Road ROW at this intersection.
4. Pedestrian recycle is on.
5. The following is the Time-Of-Day schedule:

6:00 AM to 10:30 AM	Monday to Friday	Plan I
10:30 AM to 1:00 PM	Monday to Friday	Plan II
1:00 PM to 11:00 PM	Monday to Friday	Plan III
6:00 AM to 11:00 PM	Saturday and Sunday	Plan III
11:00 PM to 6:00 AM	Daily	Plan IV

6. ** When pedestrian actution cause the timing to extend the background cycle, the local cycle counter for the controller shall be frozen at the force off point of that phase until the end of the pedestrian clearance interval. The cycle counter shall then resume timing from where it left off and the controller will immediately begin offset seeking until it gets back into step.

GENERAL NOTES:
1. NO TIMING CHANGES ARE PROPOPSED FOR JK-29 CR-9 (ROUTE 526/NORTH COUNTY LINE ROAD) & RAMPS JH-B/JH-B2
2. NO TIMING CHANGES ARE PROPOPSED FOR JK-31 CR-9 (ROUTE 526/NORTH COUNTY LINE ROAD) & RAMPS JH-C/JH-D

STATE OF NEW JERSEY BETH-ANN M. GRASSO No. GE 43121 LICENSED PROFESSIONAL ENGINEER E06

Pennoni
NJ COA NO. GA28033300
BETH-ANN M. GRASSO, P.E.
4/28/2026
PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

DATE	REVISIONS	BY	CHK.

TRAFFIC SIGNAL
C.R. 526 COUNTY LINE ROAD AND HUNTINGTON DRIVE
JACKSON TOWNSHIP

TIMING DIRECTIVES

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M	CHECKED B.M.G
	SCALE 1" = 20'	DRAWN R.M.E	APPROVED B.M.G
FILE NO. 12 - 009- 0128			SHEET 59 OF 84

4/27/2026
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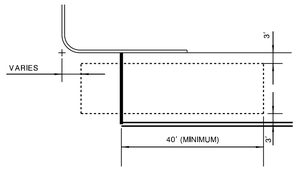
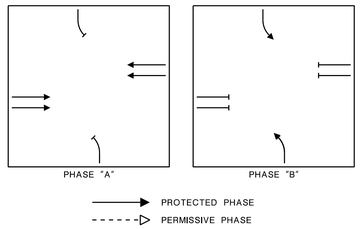
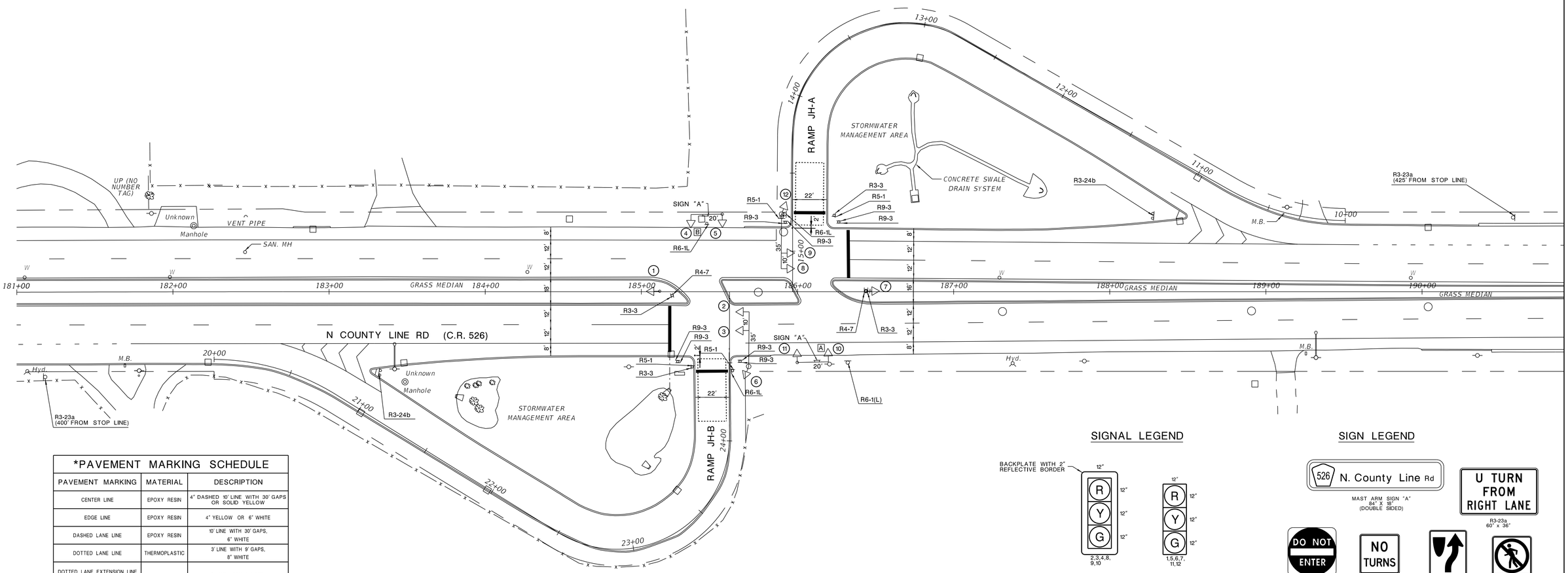
TYPICAL AREA OF DETECTION



PHASING DIAGRAM

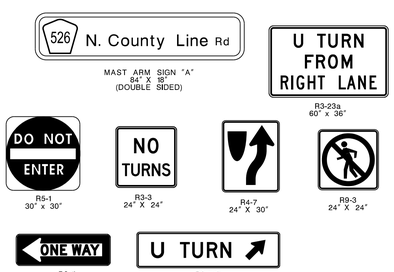




SIGNAL LEGEND

NOTES:
1. HEADS 1, 5, 6, 7, 11 & 12 ARE MOUNTED AT 10'.

SIGN LEGEND



*PAVEMENT MARKING SCHEDULE

PAVEMENT MARKING	MATERIAL	DESCRIPTION
CENTER LINE	EPOXY RESIN	4" DASHED 10' LINE WITH 30' GAPS OR SOLID YELLOW
EDGE LINE	EPOXY RESIN	4" YELLOW OR 6" WHITE
DASHED LANE LINE	EPOXY RESIN	10' LINE WITH 30' GAPS, 6" WHITE
DOTTED LANE LINE	THERMOPLASTIC	3' LINE WITH 9' GAPS, 8" WHITE
DOTTED LANE EXTENSION LINE WITHIN AN INTERSECTION "CAT TRACKS"	THERMOPLASTIC	2' LINE WITH 4' GAPS, 4" YELLOW OR 8" WHITE
SOLID LANE LINE	THERMOPLASTIC	8" WHITE
CROSSWALK LINE	THERMOPLASTIC	24" WHITE, 6' LONG SPACED AT 4' ON CENTER OR 12" WHITE, SPACED AT 7' ON CENTER
CROSSHATCH LINE	THERMOPLASTIC	24" YELLOW OR 24" WHITE SPACED 12' ON CENTER AT 45°
GORE LINE	THERMOPLASTIC	12" WHITE
STOP LINE	THERMOPLASTIC	24" WHITE
SYMBOL - MARKING	THERMOPLASTIC	WHITE

*UNLESS OTHERWISE NOTED

-THE MINIMUM DISTANCE BETWEEN STOP LINE AND CROSSWALK SHALL BE 4'.
-ALL LINES AND SIGNS SHALL CONFORM TO THE CURRENT "MUTCD".
-ALL SIGNS AND MARKINGS IN CONFLICT WITH PLAN SHALL BE REMOVED.

GENERAL NOTES:
1. THE DESIGN AND INSTALLATION OF THIS SIGNAL HAS BEEN ACCOMPLISHED IN ACCORDANCE WITH THE CURRENT MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS (MUTCD).
2. THIS PLAN SUPERSEDES PREVIOUS PLAN: 12-009-0120, 2-04, REVISION DATE, RONALD LOTRECCHIO
3. THE RIGHT OF WAY IS CALCULATED BASED ON PROVIDED AS-BUILT PLANS AND PUBLICLY AVAILABLE TAX MAP AND DEED INFORMATION.

PLAN

30' 20' 10' 0 30'

SCALE 1" = 30'

STATE OF NEW JERSEY
BETH-ANN M. GRASSO
No. GE 43121
LICENSED PROFESSIONAL ENGINEER
TSP04

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

TRAFFIC SIGNAL JK-29
C.R.#9 (ROUTE 526 NORTH COUNTY LINE ROAD)
AND
RAMP AT STA 186+00
JACKSON TOWNSHIP

TRAFFIC SIGNAL PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M	CHECKED B.M.G
	SCALE 1" = 30'	DRAWN R.M.E	APPROVED B.M.G
FILE NO. 12 - 009- 0120			SHEET 60 OF 84

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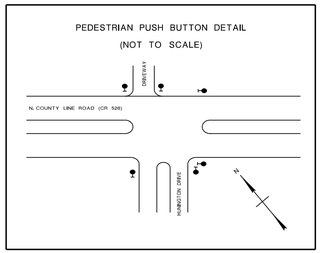
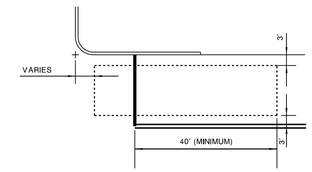
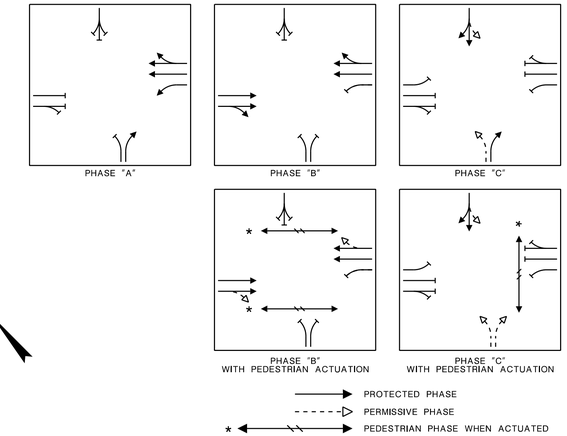
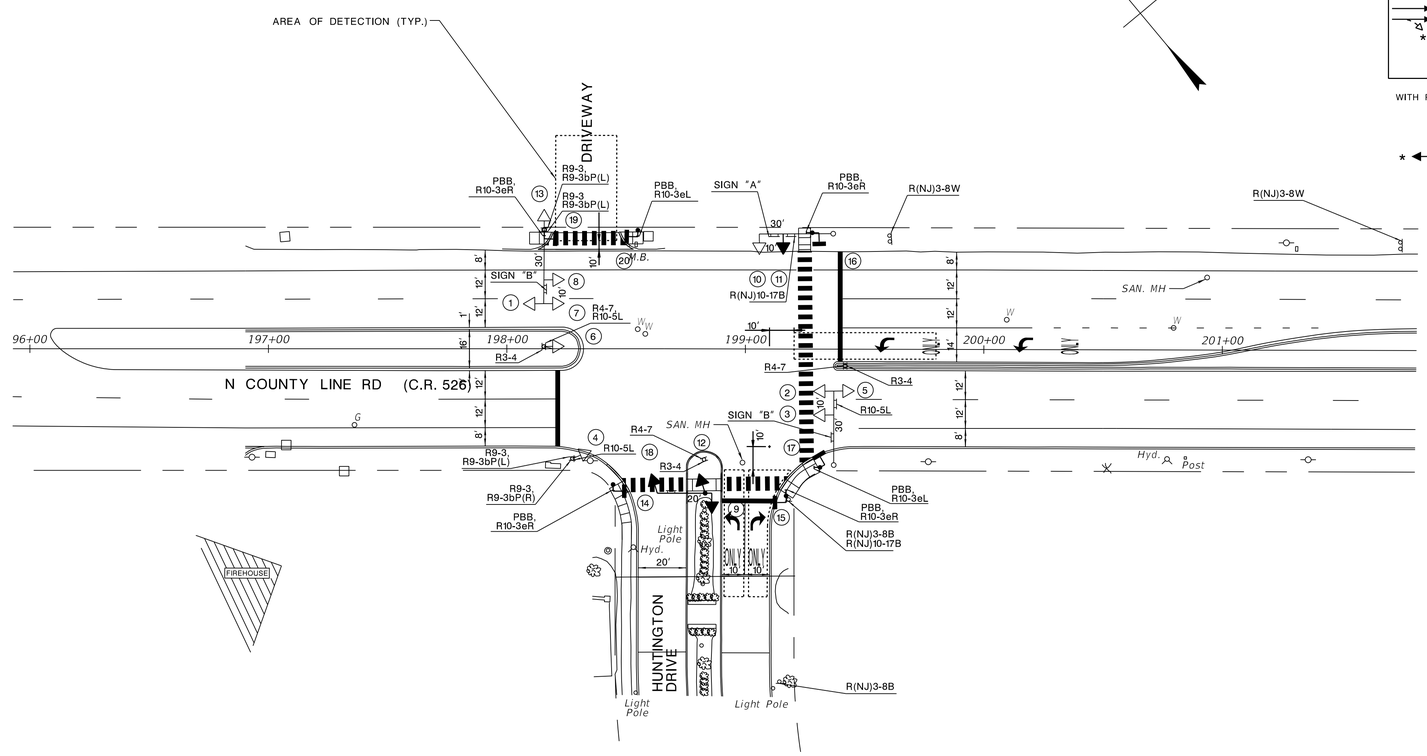


TYPICAL AREA OF DETECTION

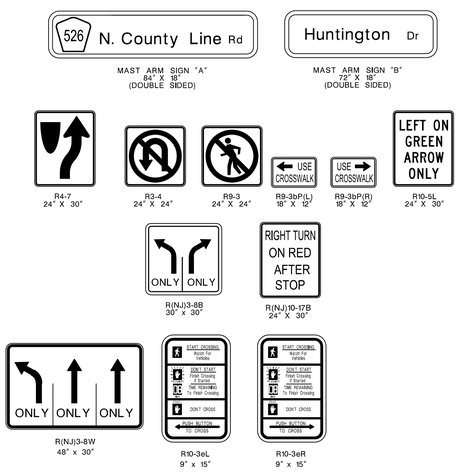


PHASING DIAGRAM





SIGN LEGEND



NOTE:

1. ALL R10-3I SIGNS SHALL BE MOUNTED TO AN ALUMINUM PANEL PER NJDOT SPECIFICATION 911.01.02A.

*PAVEMENT MARKING SCHEDULE

PAVEMENT MARKING	MATERIAL	DESCRIPTION
CENTER LINE	EPOXY RESIN	4" DASHED 10' LINE WITH 30' GAPS OR SOLID YELLOW
EDGE LINE	EPOXY RESIN	4" YELLOW OR 6" WHITE
DASHED LANE LINE	EPOXY RESIN	10' LINE WITH 30' GAPS, 6" WHITE
DOTTED LANE LINE	THERMOPLASTIC	3' LINE WITH 9' GAPS, 8" WHITE
DOTTED LANE EXTENSION LINE WITHIN AN INTERSECTION "CAT TRACKS"	THERMOPLASTIC	2' LINE WITH 4' GAPS, 4" YELLOW OR 8" WHITE
SOLID LANE LINE	THERMOPLASTIC	8" WHITE
CROSSWALK LINE	THERMOPLASTIC	24" WHITE, 6' LONG SPACED AT 4' ON CENTER OR 12" WHITE, SPACED AT 7' ON CENTER
CROSSHATCH LINE	THERMOPLASTIC	24" YELLOW OR 24" WHITE SPACED 12' ON CENTER AT 45°
GORE LINE	THERMOPLASTIC	12" WHITE
STOP LINE	THERMOPLASTIC	24" WHITE
SYMBOL - MARKING	THERMOPLASTIC	WHITE

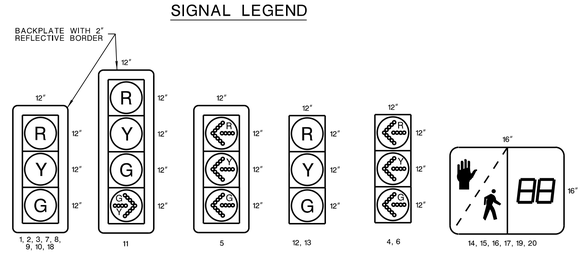
*UNLESS OTHERWISE NOTED

-ALL LINES AND SIGNS SHALL CONFORM TO THE CURRENT "MUTCD".
-ALL SIGNS AND MARKINGS IN CONFLICT WITH PLAN SHALL BE REMOVED.

GENERAL NOTES:

1. THE DESIGN AND INSTALLATION OF THIS SIGNAL HAS BEEN ACCOMPLISHED IN ACCORDANCE WITH THE CURRENT MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS (MUTCD).
2. THIS PLAN SUPERSEDES PREVIOUS PLAN: 12-009-092, 2-11, REVISION DATE, RONALD LOTRECCHIO
3. THE RIGHT OF WAY IS CALCULATED BASED ON PROVIDED AS-BUILT PLANS AND PUBLICLY AVAILABLE TAX MAP AND DEED INFORMATION.

SIGNAL LEGEND



NOTES:

1. HEADS #4, #6, #9, #12 AND #13 ARE MOUNTED AT 12'.

PLAN

30' 20' 10' 0 30'

SCALE 1" = 30'

STATE OF NEW JERSEY
BETH-ANN M. GRASSO
No. GE 43121
LICENSED PROFESSIONAL ENGINEER
TSP04

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

DATE	REVISIONS	BY	CHK.

TRAFFIC SIGNAL JK-33

C.R.#9 (ROUTE 526 NORTH COUNTY LINE ROAD)
AND
HUNTINGTON DRIVE

JACKSON TOWNSHIP

TRAFFIC SIGNAL PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M	CHECKED B.M.G
	SCALE 1" = 30'	DRAWN R.M.E	APPROVED B.M.G
FILE NO.	12 - 009- 0122		SHEET 61 OF 84

4/27/2026
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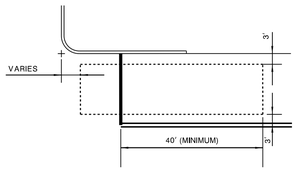
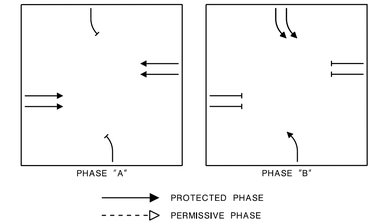
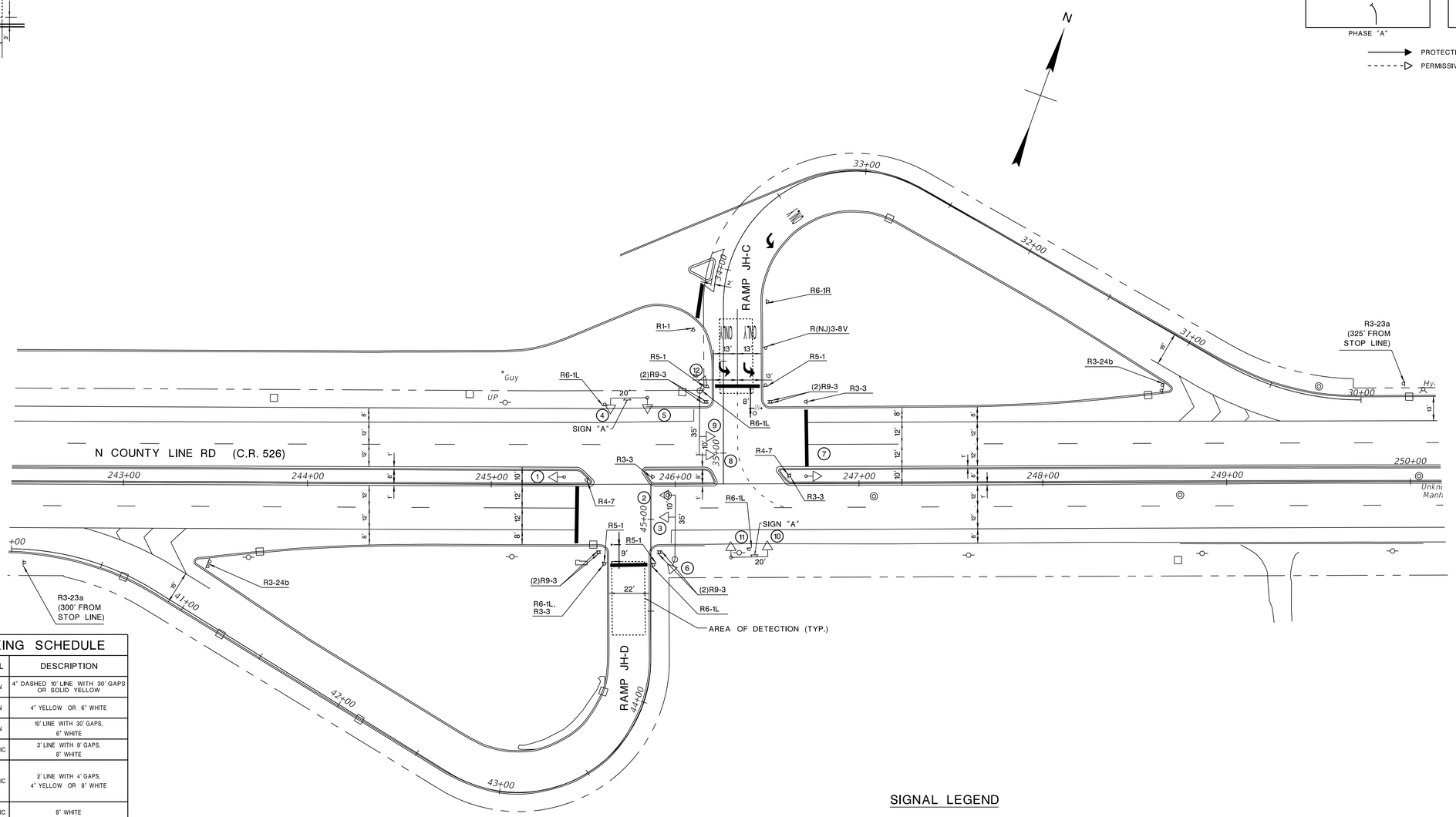
TYPICAL AREA OF DETECTION



PHASING DIAGRAM





*PAVEMENT MARKING SCHEDULE

PAVEMENT MARKING	MATERIAL	DESCRIPTION
CENTER LINE	EPOXY RESIN	4" DASHED 10' LINE WITH 30' GAPS OR SOLID YELLOW
EDGE LINE	EPOXY RESIN	4" YELLOW OR 6" WHITE
DASHED LANE LINE	EPOXY RESIN	10' LINE WITH 30' GAPS, 6" WHITE
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DOTTED LANE EXTENSION LINE WITHIN AN INTERSECTION "CAT TRACKS"	THERMOPLASTIC	2' LINE WITH 4' GAPS, 4" YELLOW OR 8" WHITE
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CROSSWALK LINE	THERMOPLASTIC	24" WHITE, 6' LONG SPACED AT 4' ON CENTER OR 12" WHITE, SPACED AT 7' ON CENTER
CROSSHATCH LINE	THERMOPLASTIC	24" YELLOW OR 24" WHITE SPACED 12' ON CENTER AT 45°
GORE LINE	THERMOPLASTIC	12" WHITE
STOP LINE	THERMOPLASTIC	24" WHITE
SYMBOL - MARKING	THERMOPLASTIC	WHITE

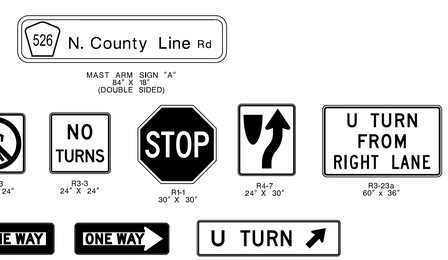
*UNLESS OTHERWISE NOTED

-THE MINIMUM DISTANCE BETWEEN STOP LINE AND CROSSWALK SHALL BE 4'.
-ALL LINES AND SIGNS SHALL CONFORM TO THE CURRENT "MUTCD".
-ALL SIGNS AND MARKINGS IN CONFLICT WITH PLAN SHALL BE REMOVED.

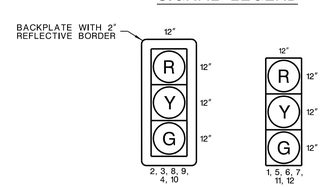
GENERAL NOTES:

1. THE DESIGN AND INSTALLATION OF THIS SIGNAL HAS BEEN ACCOMPLISHED IN ACCORDANCE WITH THE CURRENT MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS (MUTCD).
2. THIS PLAN SUPERSEDES PREVIOUS PLAN: 12-009-119, 2-04, REVISION DATE, RONALD LOTRECCHIO.
3. THE RIGHT OF WAY IS CALCULATED BASED ON PROVIDED AS-BUILT PLANS AND PUBLICLY AVAILABLE TAX MAP AND DEED INFORMATION.

SIGN LEGEND



SIGNAL LEGEND



NOTES:
1. HEADS 1, 5, 6, 7, 11, and 12 ARE MOUNTED AT 10'.

PLAN

30' 20' 10' 0 30' 60'

SCALE 1" = 30'

STATE OF NEW JERSEY BETH-ANN M. GRASSO No. GE 43121 LICENSED PROFESSIONAL ENGINEER TSP04

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

DATE	REVISIONS	BY	CHK.

TRAFFIC SIGNAL
C.R.#9 (ROUTE 526 NORTH COUNTY LINE ROAD)
AND
RAMP AT STA. 246+00
JACKSON TOWNSHIP

TRAFFIC SIGNAL PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M	CHECKED B.M.G
	SCALE 1" = 30'	DRAWN R.M.E	APPROVED B.M.G
FILE NO. 12 - 009- 0128			SHEET 62 OF 84

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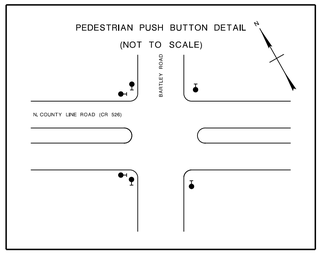
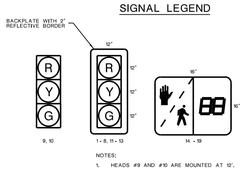
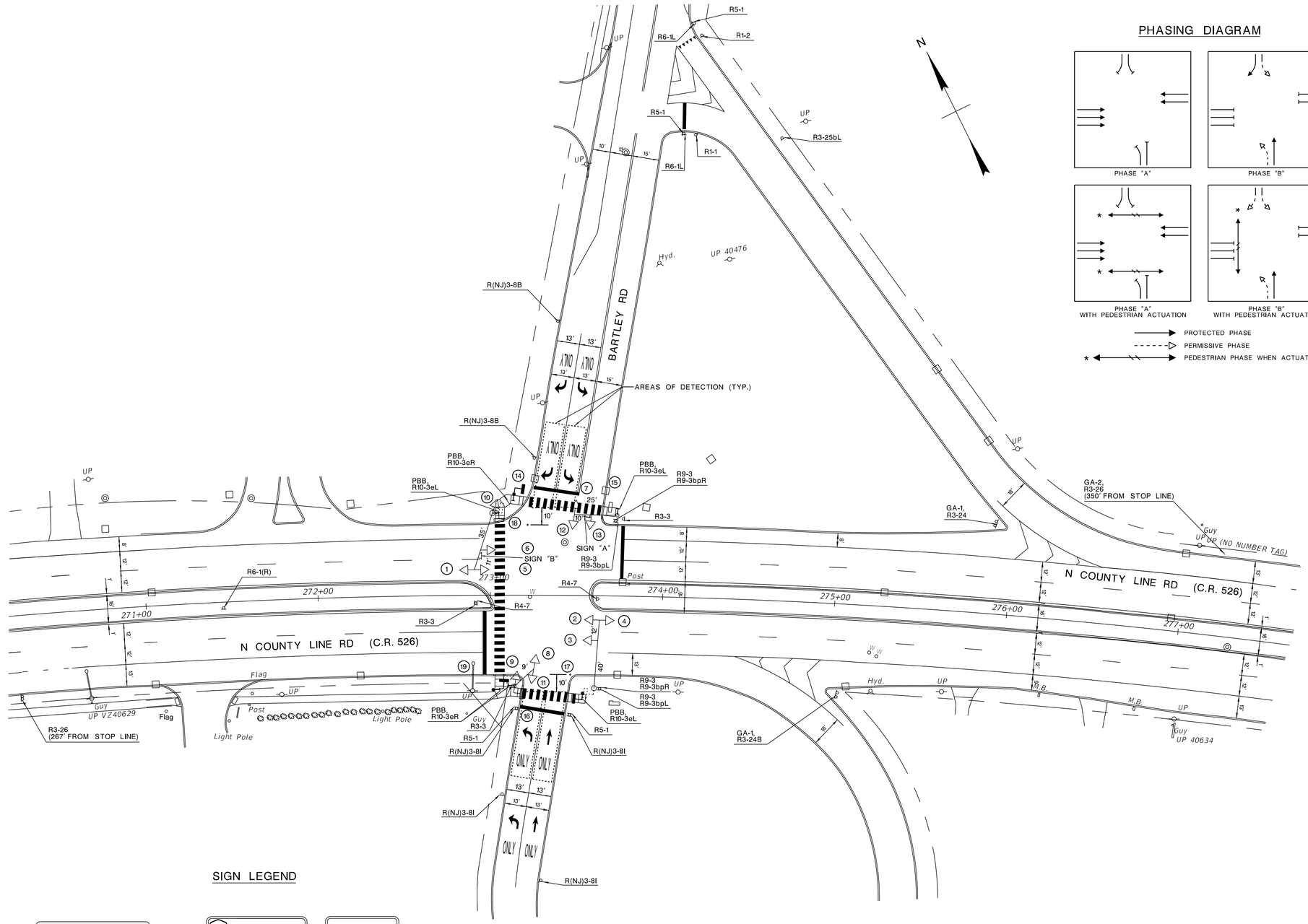


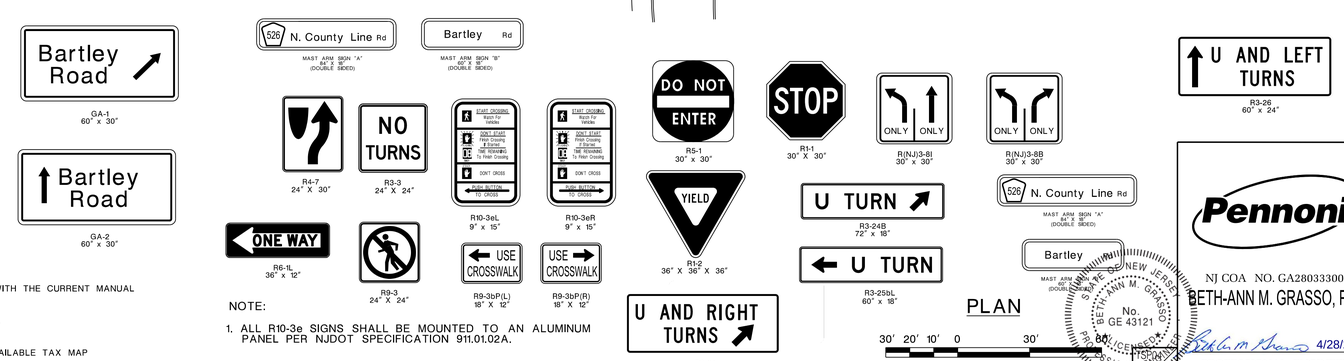
PHASING DIAGRAM

SIGNAL LEGEND





SIGN LEGEND



NOTE:

1. ALL R10-3e SIGNS SHALL BE MOUNTED TO AN ALUMINUM PANEL PER NJDOT SPECIFICATION 911.01.02A.

*PAVEMENT MARKING SCHEDULE

PAVEMENT MARKING	MATERIAL	DESCRIPTION
CENTER LINE	EPOXY RESIN	4" DASHED 10' LINE WITH 30' GAPS OR SOLID YELLOW
EDGE LINE	EPOXY RESIN	4" YELLOW OR 6" WHITE
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STOP LINE	THERMOPLASTIC	24" WHITE
SYMBOL - MARKING	THERMOPLASTIC	WHITE

*UNLESS OTHERWISE NOTED

-THE MINIMUM DISTANCE BETWEEN STOP LINE AND CROSSWALK SHALL BE 4'.

-ALL LINES AND SIGNS SHALL CONFORM TO THE CURRENT "MUTCD".

-ALL SIGNS AND MARKINGS IN CONFLICT WITH PLAN SHALL BE REMOVED.

GENERAL NOTES:

1. THE DESIGN AND INSTALLATION OF THIS SIGNAL HAS BEEN ACCOMPLISHED IN ACCORDANCE WITH THE CURRENT MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS (MUTCD).
2. THIS PLAN SUPERSEDES PREVIOUS PLAN: 12-009-0921, 2-04, REVISION DATE, RONALD LOTRECCHIO.
3. THE RIGHT OF WAY IS CALCULATED BASED ON PROVIDED AS-BUILT PLANS AND PUBLICLY AVAILABLE TAX MAP AND DEED INFORMATION.

PLAN

30' 20' 10' 0 30' 60'

SCALE 1" = 30'

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

No. GE 43121

PROFESSIONAL ENGINEER

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

DATE	REVISIONS	BY	CHK.

OCEAN COUNTY NEW JERSEY

TRAFFIC SIGNAL JK-30

C.R.#9 (ROUTE 526 NORTH COUNTY LINE ROAD)

AND

C.R.#16 (BARTLEY ROAD)

JACKSON TOWNSHIP

TRAFFIC SIGNAL PLAN

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M	CHECKED B.M.G
	SCALE 1" = 30'	DRAWN R.M.E	APPROVED B.M.G
FILE NO.	12 - 009- 0128		SHEET 63 OF 84

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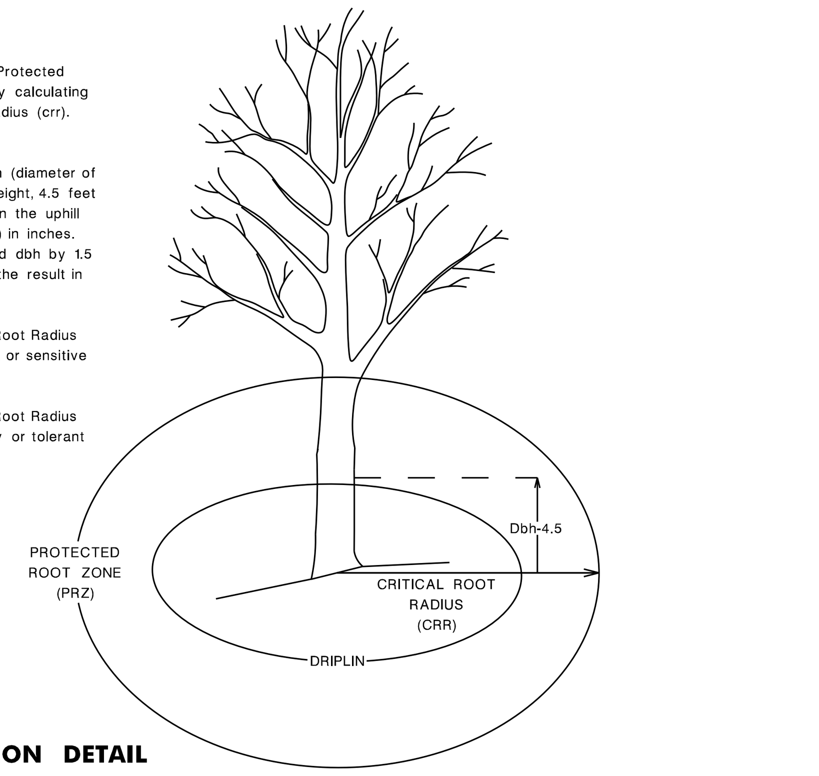
Estimate a tree's Protected Root Zone (PRZ) by calculating the Critical Root Radius (crr).

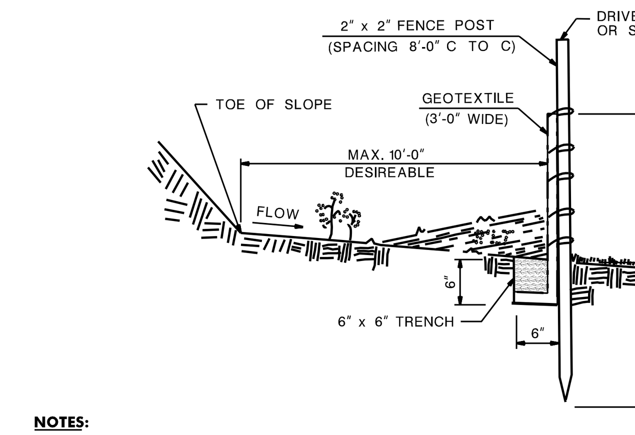
1. Measure the dbh (diameter of tree at breast height, 4.5 feet above ground on the uphill side of the tree) in inches.
2. Multiply measured dbh by 1.5 or 1.0. Express the result in feet.

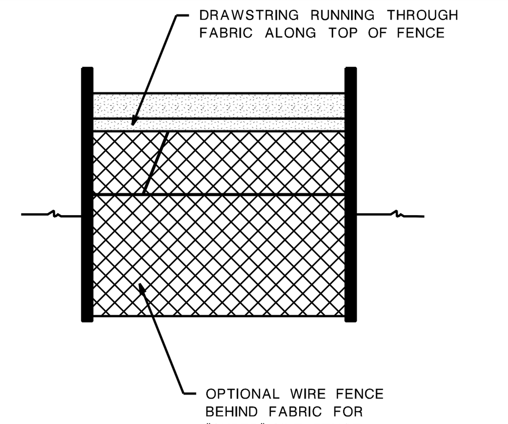
Dbh x 1.5: Critical Root Radius for older, unhealthy, or sensitive species.

Dbh x 1.0: Critical Root Radius for younger, healthy or tolerant species.



TREE PROTECTION DETAIL





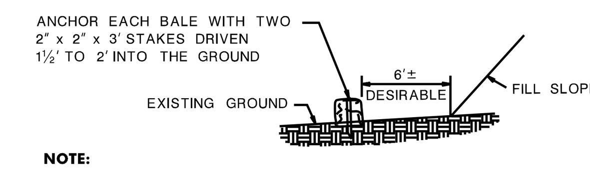
NOTES:

1. SECURELY FASTEN GEOTEXTILE TO FENCE POST BY USE OF WIRE TIES, HOG RINGS, STAPLES OR POCKETS. FOUR TO SIX FASTENERS PER POST.
2. BURY BOTTOM 1'-0" OF GEOTEXTILE AND TAMP IN PLACE.
3. SECURELY FASTEN ENDS OF INDIVIDUAL ROLLS OF GEOTEXTILE TO A POST BY WRAPPING EACH END OF THE GEOTEXTILE AROUND THE POST TWICE AND ATTACHING AS SPECIFIED IN NOTE 1 ABOVE. SPLICING OF INDIVIDUAL ROLLS SHALL NOT OCCUR AT LOW POINTS.
4. SET SILT FENCE WITHIN PROJECT LIMITS. 10'-0" IS DESIRABLE.
5. POSTS SHALL BE CONSTRUCTED OF HARDWOOD WITH A MINIMUM DIAMETER THICKNESS OF 1½ INCHES.
6. THE GEOTEXTILE FABRIC MUST BE SECURELY FASTENED TO THE POSTS USING A SYSTEM CONSISTING OF METAL FASTENERS (NAILS OR STAPLES) AND A HIGH STRENGTH REINFORCEMENT MATERIAL (NYLON WEBBING, GROMMETS, WASHERS, ETC.) PLACED BETWEEN THE FASTENER AND THE GEOTEXTILE FABRIC. THE FASTENING SYSTEM SHALL RESIST TEARING AWAY FROM THE POST.
7. "SUPER" SILT FENCE - A METAL FENCE WITH 6IN OR SMALLER MESH OPENINGS AND AT LEAST 2FT HIGH MAY BE UTILIZED, FASTENED TO THE FENCE POSTS, TO PROVIDE REINFORCEMENT AND SUPPORT TO THE GEOTEXTILE FABRIC. POSTS MAY BE SPACED LESS THAN 8FT ON CENTER AND MAY BE CONSTRUCTED OF HEAVIER WOOD OR METAL AS NEEDED TO WITHSTAND HEAVIER SEDIMENT LOADING. THIS PRACTICE IS APPROPRIATE WHERE SPACE FOR OTHER PRACTICES IS LIMITED AND HEAVY SEDIMENT LOADING IS EXPECTED. "SUPER" SILT FENCE IS NOT TO BE USED IN PLACE OF PROPERLY DESIGNED DIVERSIONS (PG 15-1) WHICH MAY BE NEEDED TO CONTROL SURFACE RUNOFF RATES AND VELOCITIES.

SILT FENCE MAINTENANCE:

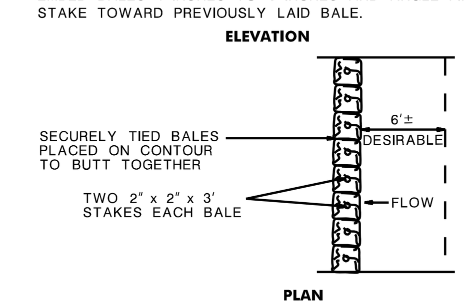
1. SEDIMENT SHALL BE REMOVED FROM THE UPSTREAM FACE OF THE BARRIER WHEN IT HAS REACHED A DEPTH OF ½ THE BARRIER HEIGHT.
2. REPAIR OR REPLACE BARRIER (FABRIC, POSTS, BAILES, ETC.) WHEN DAMAGED.
3. BARRIERS SHALL BE INSPECTED DAILY FOR SIGNS OF DETERIORATION AND SEDIMENT REMOVAL.

SILT FENCE



NOTE:
EMBED BALES 4 INCHES TO 6 INCHES AND ANGLE FIRST STAKE TOWARD PREVIOUSLY LAID BALE.

ELEVATION

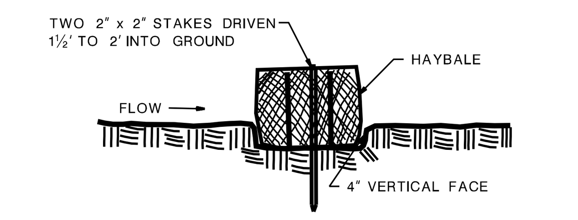


PLAN

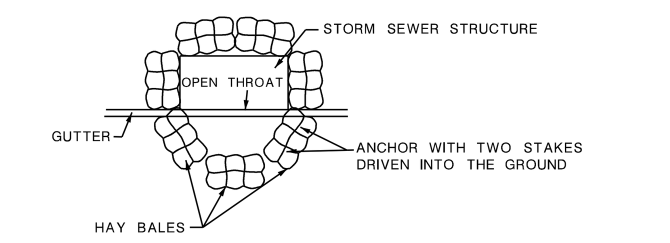
NOTE:

1. TO BE USED WHERE THE EXISTING GROUND SLOPES AWAY FROM THE HIGHWAY EMBANKMENT AS CALLED FOR ON PLANS.
2. ALL BALES SHALL BE SECURELY TIED AND STAKED ON CONTOUR.
3. BALES SHALL BE PLACED IN A ROW WITH ENDS TIGHTLY ABUTTING THE ADJACENT BALES.
4. BALES SHALL BE SECURELY ANCHORED IN PLACE BY TWO STAKES OR RE-BARS DRIVEN THROUGH EACH BALE. THE FIRST STAKE IN EACH BALE SHALL BE DRIVEN TOWARD PREVIOUSLY LAID BALE TO FORCE BALES TOGETHER.

HAYBALES



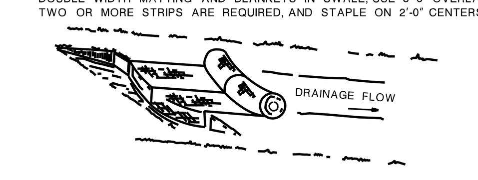
EMBEDDING DETAIL



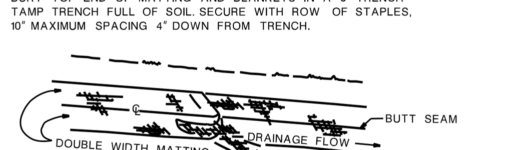
TEMPORARY HAY BALE BARRIER AROUND DRAINAGE STRUCTURE

SWALE OR DITCH

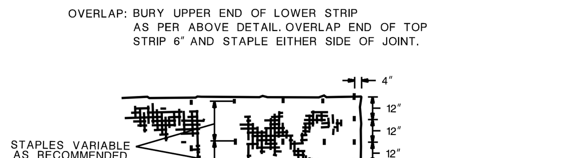
DOUBLE WIDTH MATTING AND BLANKETS IN SWALE, USE 3'-6" OVERLAP WHERE TWO OR MORE STRIPS ARE REQUIRED, AND STAPLE ON 2'-0" CENTERS



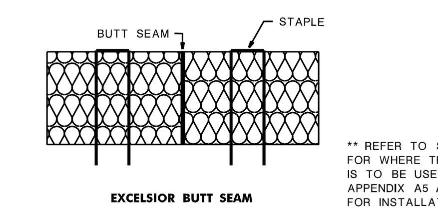
BURY TOP END OF MATTING AND BLANKETS IN A 6" TRENCH TAMP TRENCH FULL OF SOIL. SECURE WITH ROW OF STAPLES, 10" MAXIMUM SPACING 4" DOWN FROM TRENCH.



OVERLAP: BURY UPPER END OF LOWER STRIP AS PER ABOVE DETAIL. OVERLAP END OF TOP STRIP 6" AND STAPLE EITHER SIDE OF JOINT.



SECURE MATTING AND BLANKETS WITH STAPLES SPACED 24" APART ALONG THE SIDES AND DOWN THE CENTER. AT THE ENDS OF THE MATTING AND AT 50 FOOT INTERVALS STAPLES SHALL BE PLACED 12" APART ACROSS THE WIDTH.



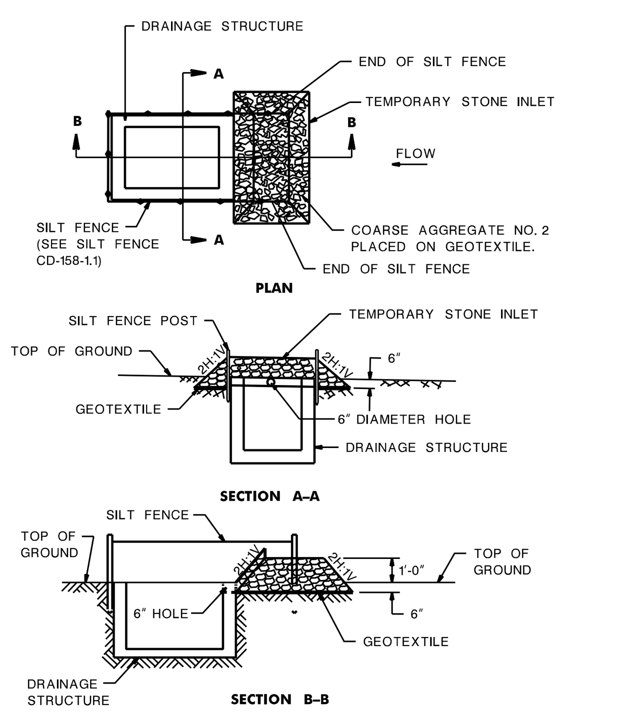
EXCELSIOR BUTT SEAM

** REFER TO SE&SC MANUAL FOR WHERE THIS TREATMENT IS TO BE USED AND USE APPENDIX A5 AS A GUIDE FOR INSTALLATION.

NOTES:

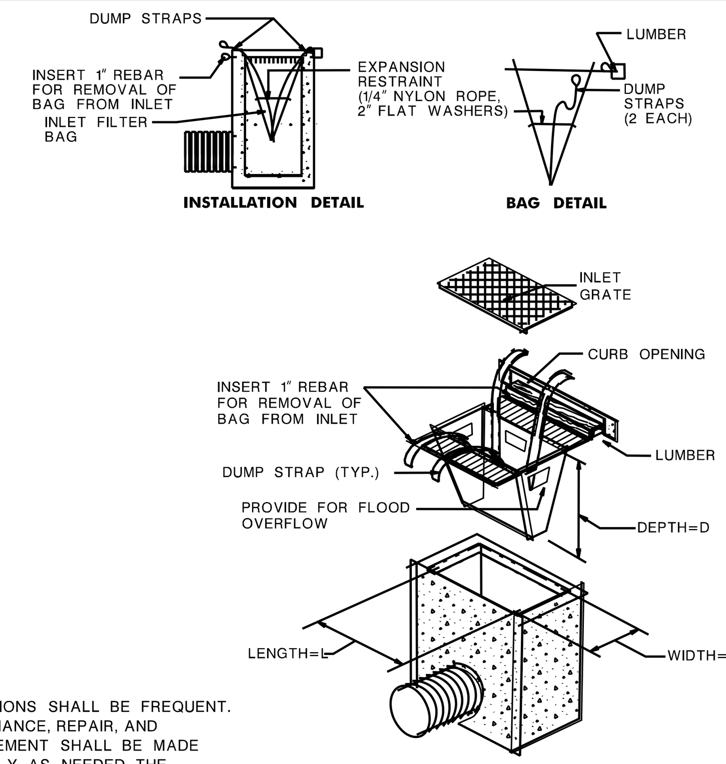
1. EROSION STOP - FOLD OF JUTE BURIED IN SLIT TRENCH AND TAMPED, DOUBLE ROW OF STAPLES

TOPSOIL STABILIZATION MATTING



NOTE:
REFER TO CD-158-2.4 SECTION A-A FOR TREATMENT OF 6" HOLE.

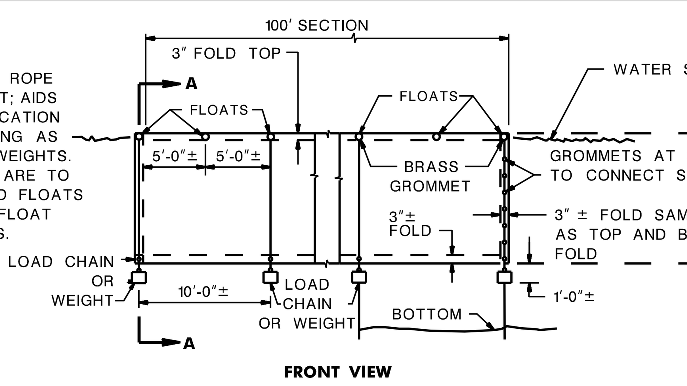
INLET SEDIMENT TRAP



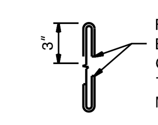
NOTE:
INSPECTIONS SHALL BE FREQUENT. MAINTENANCE, REPAIR, AND REPLACEMENT SHALL BE MADE PROMPTLY, AS NEEDED. THE FILTER SHALL BE REMOVED WHEN THE AREA DRAINING TOWARD THE INLET HAS BEEN STABILIZED.

INLET FILTER

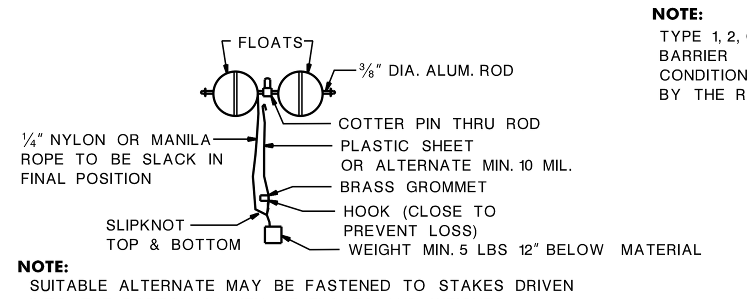
NOTE:
¼" NYLON OR MANILA ROPE FORMS REINFORCEMENT; AIDS IN REMOVAL OR RELOCATION OF BARRIER BY SERVING AS A PICK-UP LINE FOR WEIGHTS. ROPES AND WEIGHTS ARE TO BE ATTACHED TO END FLOATS AND EVERY SECOND FLOAT BETWEEN END FLOATS.



FRONT VIEW



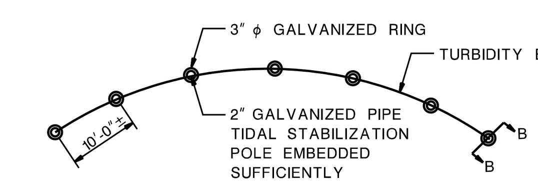
SIDE VIEW
FOLDING DETAIL

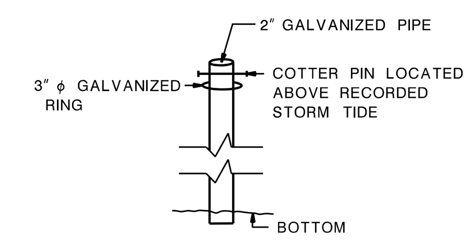


NOTE:
SUITABLE ALTERNATE MAY BE FASTENED TO STAKES DRIVEN INTO THE BOTTOM IN LIEU OF FLOATS AND WEIGHTS

SECTION A–A

NOTE:
TYPE 1, 2, OR 3 TURBIDITY BARRIER TO FIT EXISTING CONDITION AS APPROVED BY THE R.E..

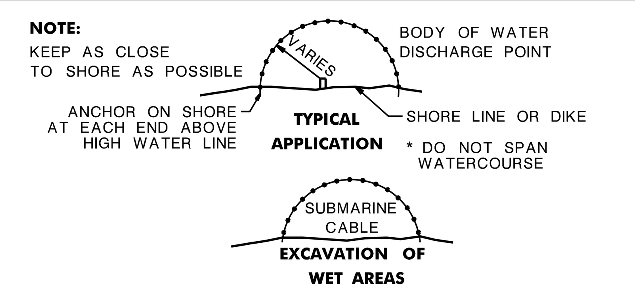




SECTION B–B

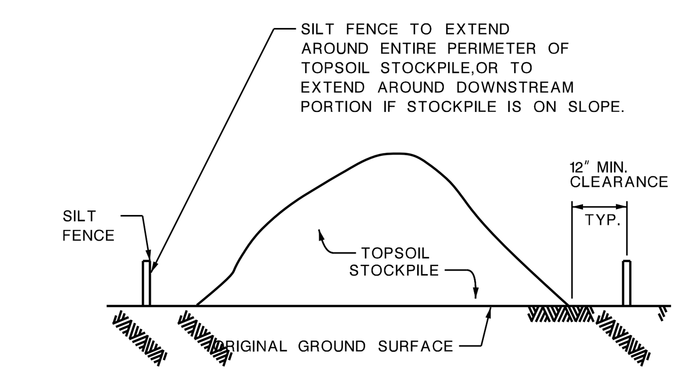
FLOATING TURBIDITY BARRIER

NOTE:
KEEP AS CLOSE TO SHORE AS POSSIBLE



GENERAL NOTES:

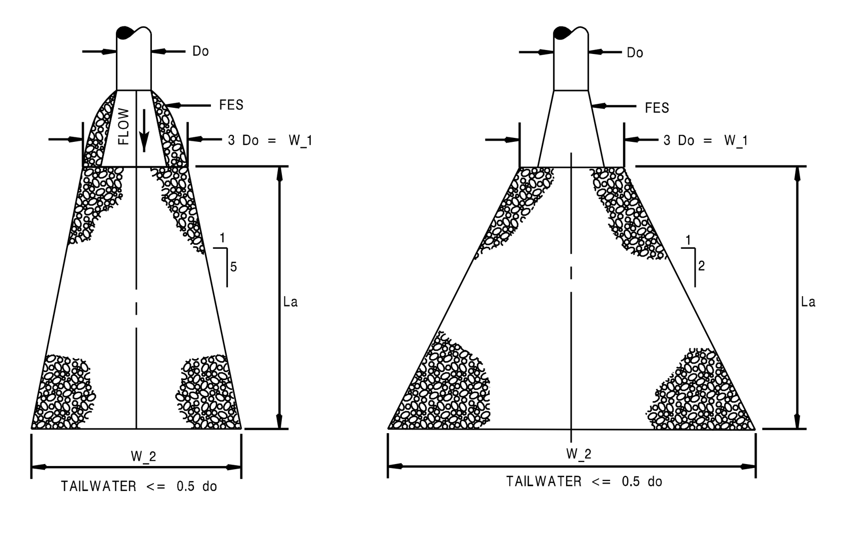
1. INSTALL TURBIDITY BARRIER TO PREVENT DRIFTING OF SILT CAUSED BY DISCHARGE OF STORM SEWERS, DEWATERING BASINS, CONSTRUCTION, DREDGING OR FILLING OPERATIONS, OR OTHER ACTIVITIES THAT COULD CAUSE TURBIDITY.
2. EXACT PLACEMENT OF TURBIDITY BARRIER SHALL BE SO AS TO EFFECTIVELY CONTROL SILT DISPERSION UNDER THE CONDITIONS PRESENT ON A PARTICULAR PROJECT.
3. THE DETAILS SHOWN ON THIS SHEET ARE SUGGESTED METHODS ONLY. ALTERNATE SOLUTION AND USAGE OF MATERIALS MAY BE USED AS APPROVED.
4. USE APPROPRIATE NAVIGATIONAL WARNING LIGHTS WHEN USED NEXT TO NAVIGATIONAL CHANNEL.
5. TYPE 1 IS FOR PONDS, SHALLOW LAKES, SMALL STREAMS AND MARSHES WITH CURRENT VELOCITIES LESS THAN 1 FT./SEC AND SHELTERED FROM WINDS.
6. TYPE 2 IS FOR LAKES, STREAMS, INTERCOASTAL & TIDAL AREAS WITH CURRENT VELOCITIES UP TO 5 FT./SEC.
7. TYPE 3 IS FOR LAKES, STREAMS, INTERCOASTAL AND TIDAL AREAS WITH CONSIDERABLE CURRENT VELOCITIES UP TO 5FT./SEC., TIDAL ACTION AND SUBJECT TO WIND AND WAVES.
8. AT A MINIMUM BARRIER MATERIAL SHALL BE A POLYETHYLENE PLASTIC SHEET, 10 MIL., OR SUITABLE ALTERNATE TO FIT THE EXISTING CONDITIONS. SEE PROJECT SPECIFICATIONS TO CONFIRM MINIMUM MATERIAL REQUIREMENTS.
9. AT A MINIMUM WIGHTS SHALL BE 5 POUNDS AND EXTEND 12" BELOW THE BOTTOM OF THE MATERIAL. SEE PROJECT SPECIFICATIONS TO CONFIRM MINUMUM REQUIREMENTS.

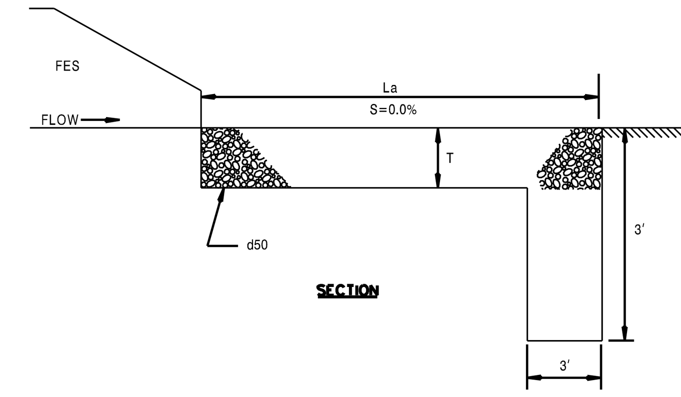


NOTE:
STOCK PILES ARE NOT TO BE PLACED IN AN AREA WITH CONCENTRATED FLOW, WETLANDS, EXTREME SLOPE, OR WITHIN 100' OF A NATURAL STREAM.

TEMPORARY TOPSOIL STOCKPILE

SUBSTITUTE HAY BALES FOR SILT FENCE IF STOCKPILE IS LOCATED ON A PAVED SURFACE.





NOTES:

STONE SHALL BE REASONABLY WELL GRADED. 50% BY WEIGHT SHALL BE SMALLER THEN THE MEDIAN SIZE STONE DESIGNATE AS d50. LARGEST STONE SIZE SHALL BE 1.5 d50. THE d75 SHOULD BE 1.25 TIMES THE d50 AND THE d15 SHOULD BE 0.5 TIMES THE d50.

FLARED END SECTION NUMBER	L (FEET)	W_1 (FEET)	W_2 (FEET)	d50 (INCHES)	d50 THICKNESS (INCHES)

NOTE: THICKNESS OF d 50 INCLUDES THE USE OF FILTER FABRIC

RIP–RAP APRON

U:\Accounts\OCPBX\OCPBX24001 - QP2023-163 Reconst & Resurf CR 526\DESIGN_SHEETS\DETAIL-CD-3.dgn
PENNONI-HDH-OCEAN-CO-NJ-FULL-PDF_rme.PLTCFG
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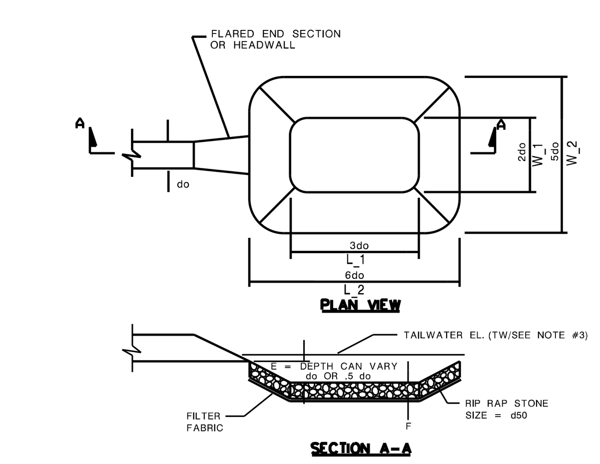


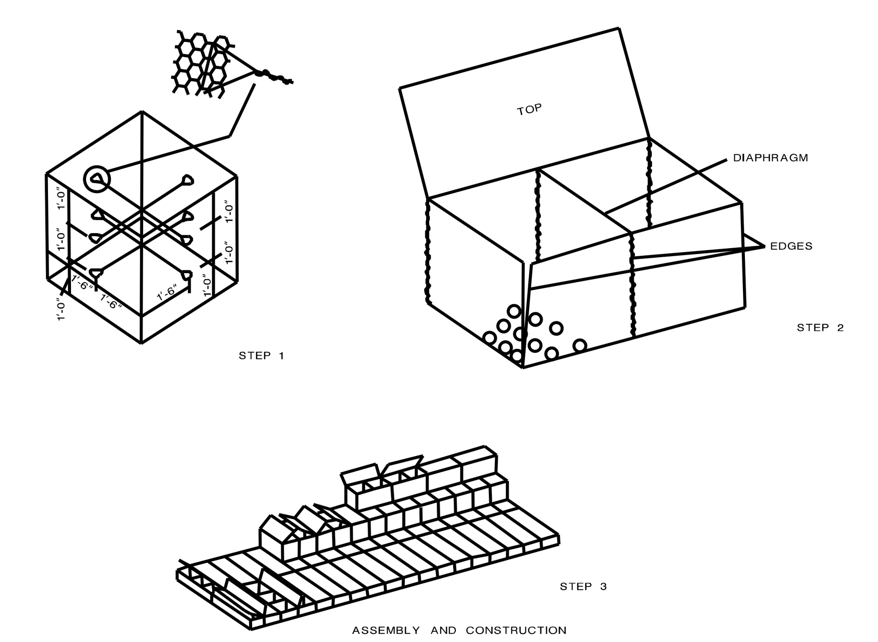
NOTES:

1. FOR DESIGN INFORMATION, DIMENSIONS AND RIP RAP SIZE SEE CONSTRUCTION PLANS.
2. STONES FOR RIP RAP SHALL BE COMPOSED OF A WELL GRADED MIXTURE OF DURABLE ROCK SUCH THAT 50% OF THE MIXTURE BY WEIGHT SHALL BE LARGER THAN A d50 STONE DIAMETER THE MIXTURE SHALL CONTAIN A SUFFICIENT MIXTURE OF OTHER STONES TO FILL VOIDS. THE DIAMETER OF THE LARGEST STONE SIZE SHALL BE 1.5 d50.
3. TAILWATER (TW) SHALL BE EQUAL TO THE 2-YR STORM DEPTH OF RECEIVING WATER AT THE END OF THE APRON OR 0.2 x Do FOR AREAS WHERE TAIL WATER CAN NOT BE COMPUTED.

FLARED END SECTION NUMBER	L_1 (FEET)	L_2 (FEET)	W_1 (FEET)	W_2 (FEET)	d50 (INCHES)	d50 THICKNESS (INCHES)	E DEPTH (INCHES)

NOTE: THE THICKNESS OF d50 IS TO BE UNIFORM AND INCLUDES THE USE OF FILTER FABRIC UNDER THE RIP RAP

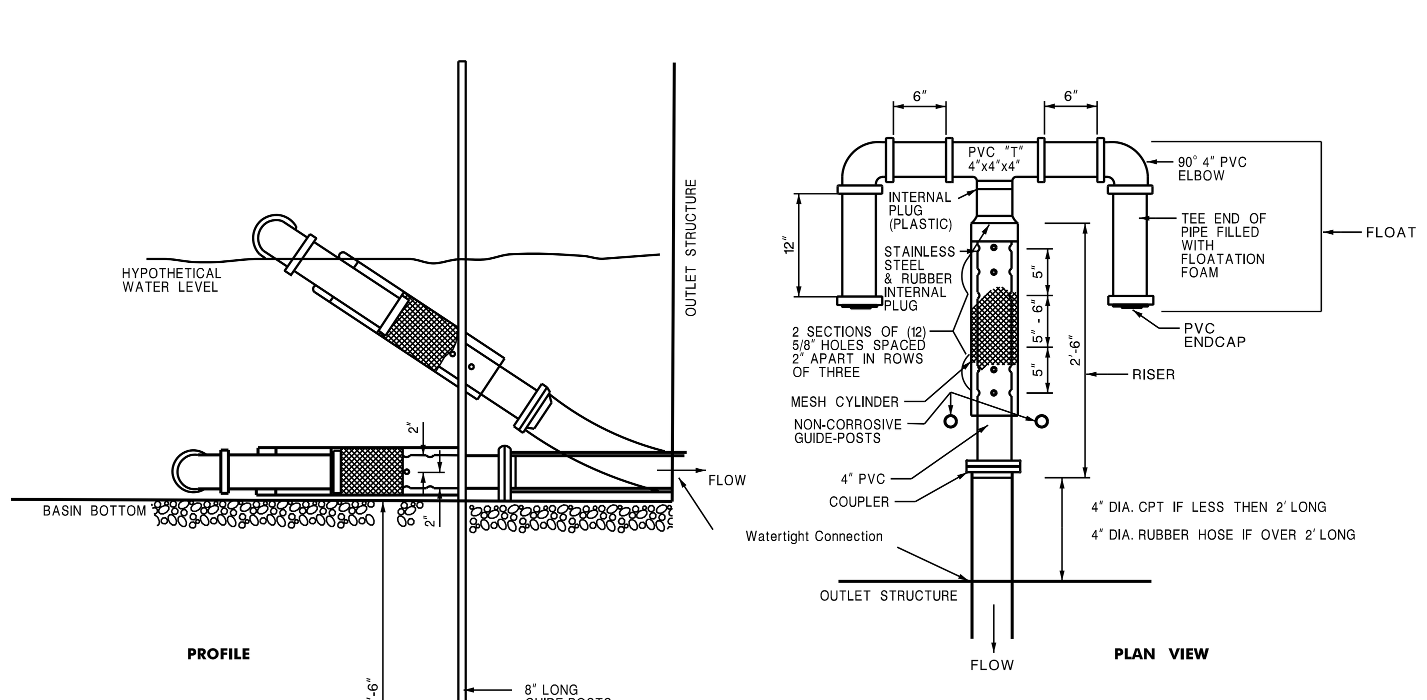
RIPRAP CHANNEL PROTECTION (SCOUR HOLE)



1. FOR EASY HANDLING AND SHIPPING, GABIONS ARE SUPPLIED FOLDED INTO A FLAT POSITION. THEY ARE READILY ASSEMBLED BY UNFOLDING AND BY SIMPLY WIRING THE EDGES TOGETHER AND THE DIAPHRAGMS TO THE SIDES.
2. THE GABIONS ARE FILLED TO A DEPTH OF ONE FOOT, THEN ONE CONNECTING WIRE IS PLACED IN EACH DIRECTION AND LOOPED AROUND TWO MESHES OF THE GABION WALL. THIS OPERATION IS REPEATED UNTIL THE GABION IS FILLED.
3. ADJOINING GABIONS ARE WIRED TOGETHER BY THEIR VERTICAL EDGES. EMPTY GABIONS, STACKED ON FILLED GABIONS, ARE WIRED TO THE FILLED GABIONS AT FRONT AND BACK.
4. AFTER THE GABION IS FILLED THE TOP IS FOLDED SHUT AND WIRED TO THE ENDS, SIDES AND DIAPHRAGMS.
5. PLASTIC-COATED WIRE MESH BASKETS SHALL BE USED.
6. ALL WIRE MESH STRUCTURES PLACED AGAINST THE BOTTOM OF A CHANNEL SHALL BE UNDERLAIN BY GEOTEXTILE OR A GRAVEL FILTER DESIGNED ACCORDING TO THE LIMITS OUTLINED IN TABLE 22-1
7. THE ROCK USED TO FILL BASKET STRUCTURES SHALL BE 4" TO 7" ANGULAR, BLOCK-SHAPED ROCK. FOR WIRE MESH "MATTRESS" STRUCTURES, 3" TO 4" STONE MAY BE USED PROVIDED THE MESH OPENING IS SMALL ENOUGH TO CONTAIN THE STONE. SMALLER STONE WILL PROVIDE MORE STONE LAYERS IN THE MATTRESS WHERE LARGER STONE WOULD NOT SUFFICIENTLY FILL THE STRUCTURE'S VOID SPACE

GABION AND MATTRESS

NOTE: ALL PIPE AND TUBING IS SIZED AS PER ORFICE DIAMETER. (EXAMPLE SHOWN IS FOR A FOUR INCH DIAMETER ORFICE)



NOTES:

ALL PIPES TO BE SCHD 40 PVC

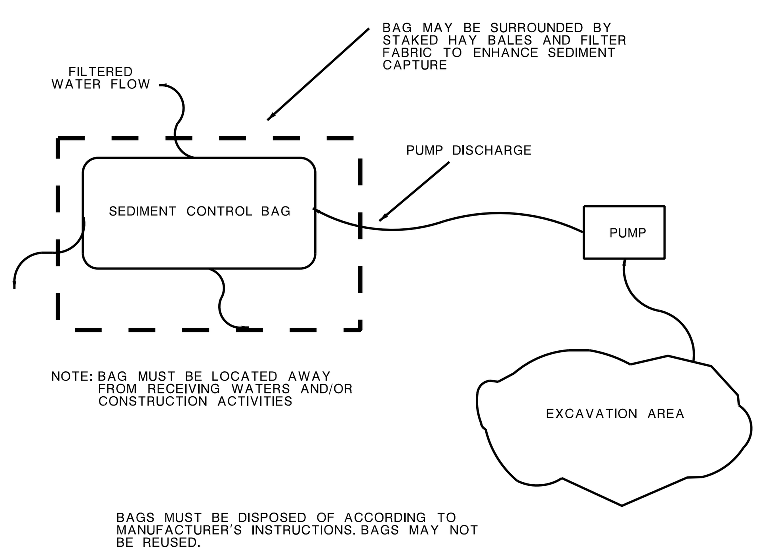
TAPE OR CEMENT ALL WATERTIGHT CONNECTIONS

CONTRACTOR IS TO CONSTRUCT A CYLINDER MADE OF GALVANIZED HARDWARE CLOTH OVER THE PERFORATIONS IN THE RISER TO PREVENT BLOCKAGE OF THE HOLES FROM DEBRIS USE 1/2" X 1/2" 19 GUAGE MESH OR APPROVED EQUAL. PROVIDE 2" CLEAR BETWEEN PERFORATED RISER AND MESH ITSELF.

FLOATING RISER IS TO BE REMOVED AFTER CONSTRUCTION OF PROJECT SUCH THAT BASIN CAN FUNCTION PROPERLY FOR DETENTION PURPOSES.

PROVISION MUST BE MADE TO PREVENT THE SKIMMER FROM RESTING ON THE BASIN FLOOR WHEN THE POOL HAS BEEN COMPLETELY DRAINED

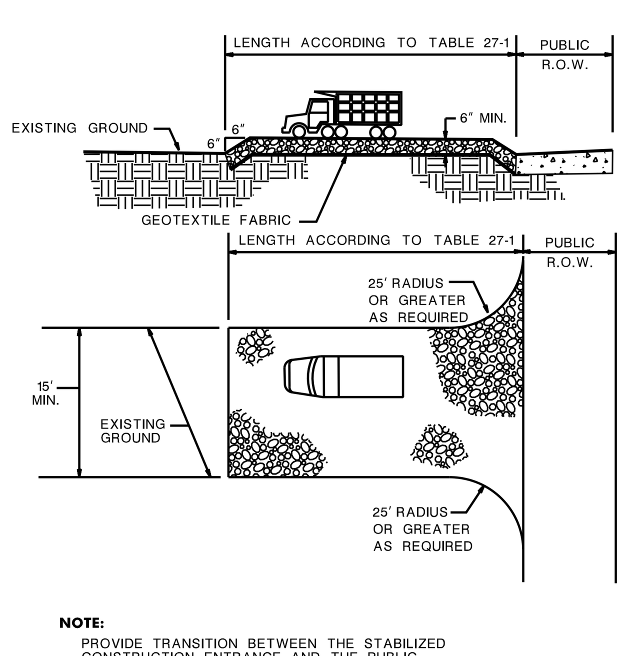
FLOATING RISER DETAIL
DEWATERING SEDIMENT BASIN WITH SKIMMER



DEWATERING

NOTE:

1. BAGS SHALL BE LOCATED FOR EASE OF CLEAN-OUT AND DISPOSAL OF TRAPPED SEDIMENT AND TO MINIMIZE INTERFERENCE WITH CONSTRUCTION ACTIVITIES AND PEDESTRIAN TRAFFIC. BAGS SHALL BE NOT PLACED DIRECTLY INTO RECEIVING WATERS.
2. SEDIMENT CONTROL BAGS MUST BE LOCATED AWAY FROM RECEIVING WATERS AND DISPOSED OF ACCORDING TO MANUFACTURER'S INSTRUCTIONS. SEE DETAIL. BAGS MAY BE COMBINED WITH TEMPORARY FILTERS (ITEM 3. BELOW) FOR ENHANCED FILTRATION.
3. FOR SMALL QUANTITIES OF PONDED WATER SUCH AS MAY BE FOUND IN SHALLOW EXCAVATIONS (SMALL TERNCHES, MANHOLE INSTALLATIONS ETC.) A SEDIMENT FILTER MAY BE CONSTRUCTED USING COMBINATIONS OF HAY BALES, SMALL CLEAN STONE AND FILTER FABRIC. THIS METHOD IS LIMITED TO SMALL QUANTITIES OF TRAPPED SURFACE WATER (PUMPING OF WELL POINTS IS EXCLUDED FROM THIS STANDARD) AND WHERE SEDIMENTS ARE NOT HIGHLY COLLOICAL IN NATURE.



NOTE:
PROVIDE TRANSITION BETWEEN THE STABILIZED CONSTRUCTION ENTRANCE AND THE PUBLIC RIGHT OF WAY.

PROFILE AND PLAN VIEW

STABILIZED CONSTRUCTION DRIVEWAY

1. STABILIZE CONSTRUCTION ENTRANCE NEED TO SHOW THE STONE SIZE & MAINTENANCE:
2. STONE SIZE - USE ASTM C-33, SIZE NO. 2 (2-1/2 TO 1-1/2 IN) OR 3 (2 TO 1 IN). USE CLEAN CRUSHED ANGULAR STONE. CRUSHED CONCRETE OF SIMILAR SIZE MAY BE SUBSTITUTED BUT WILL REQUIRE MORE FREQUENT UPGRADING AND MAINTENANCE.
3. THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO ROADWAYS. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE OR ADDITIONAL LENGTH AS CONDITIONS DEMAND AND REPAIR AND/OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT.
4. ALL SEDIMENT SPILLED, DROPPED, WASHED, OR TRACKED ONTO ROADWAYS (PUBLIC OR PRIVATE) OR OTHER IMPERVIOUS SURFACES MUST BE REMOVED IMMEDIATELY.
5. INDIVIDUAL LOT ENTRANCE AND EGRESS- AFTER INTERIOR ROADWAYS ARE PAVED, INDIVIDUAL LOT INGRESS/EGRESS POINTS MAY REQUIRE A STABILIZED CONSTRUCTION ENTRANCE CONSISTING OF NO. 3 STONE (1" TO 2") TO PREVENT OR MINIMIZE TRACKING OF SEDIMENTS. WIDTH OF THE STONE INGRESS/EGRESS SHALL BE EQUAL TO LOT ENTRANCE WIDTH AND SHALL BE A MINIMUM OF TEN FEET IN LENGTH.
6. TIRE WASHING - IF SPACE IS LIMITED, VEHICLE TIRES MAY BE WASHED WITH CLEAN WATER BEFORE ENTERING A PAVED AREA. A WASH STATION MUST BE LOCATED SUCH THAT WASH WATER WILL NOT FLOW ONTO PAVED ROADWAYS OR INTO UNPROTECTED STORM DRAINAGE SYSTEMS.
7. WHEN THE CONSTRUCTION ACCESS EXITS ONTO A MAJOR ROADWAY, A PAVED TRANSITION AREA MAY BE INSTALLED BETWEEN THE MAJOR ROADWAY AND THE STONED ENTRANCE TO PREVENT LOOSE STONES FROM BEING TRANSPORTED OUT ONTO THE ROADWAY BY HEAVY EQUIPMENT ENTERING OR LEAVING THE SITE.
8. THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO ROADWAYS. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE OR ADDITIONAL LENGTH AS CONDITIONS DEMAND AND REPAIR AND/OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT. ALL SEDIMENT SPILLED, DROPPED, WASHED, OR TRACKED ONTO ROADWAYS (PUBLIC OR PRIVATE) OR OTHER IMPERVIOUS SURFACES MUST BE REMOVED IMMEDIATELY.
9. WHERE ACCUMULATION OF DUST/SEDIMENT IS INADEQUATELY CLEANED OR REMOVED BY CONVENTIONAL METHODS, A POWER BROOM OR STREET SWEEPER WILL BE REQUIRED TO CLEAN PAVED OR IMPERVIOUS SURFACES. ALL OTHER ACCESS POINTS WHICH ARE NOT STABILIZED SHALL BE BLOCKED OFF.
10. WHERE A STABILIZED CONSTRUCTION EXIT TRAVERSES BETWEEN TWO BUILDINGS, IT SHALL BE STONED THE ENTIRE LENGTH OF THE R.O.W. MOUNTABLE STONE BERMS PLACED ACROSS THE WIDTH OF THE EXIT MAY ALSO BE REQUIRED AT THE TRANSITION POINT BETWEEN PAVED AND NON-PAVED AREAS TO TRAP SEDIMENTS WHICH ARE CARRIED BY STORMWATER FLOWING ALONG THE CURBLINE.

TABLE 27-1: LENGTHS OF CONSTRUCTION EXITS ON SLOPING ROADBEDS

PERCENT SLOPE OF ROADWAY	LENGTH OF STONE REQUIRED	
	COARSE GRAINED SOILS	FINE GRAINED SOILS
0 to 2%	50 ft.	100 ft.
2 to 5%	100 ft.	200 ft.
>5%	Entire surface stabilized with Hot Mix Asphalt Base Course, Mix I-2*	

*AS PRESCRIBED BY LOCAL ORDINANCE OR OTHER GOVERNING AUTHORITY.

NOTE:

1. THESE ARE FOR THE CONTRACTOR'S REFERENCE WHEN INSTALLING SEDIMENT CONTROL DEVICES IN ACCORDANCE WITH THE STATE STANDARDS FOR SOIL EROSION AND SEDIMENT CONTROL OF NEW JERSEY. SOME DETAILS MAY OR MAY NOT BE APPLICABLE.
2. THE COST OF INSTALLING SEDIMENT CONTROL DEVICES IS PAID FOR UNDER THE VARIOUS ITEMS IN THIS CONTRACT.
3. THE STAGING AREA IS NOT PROPOSED. IF THE CONTRACTOR IDENTIFIES A STAGING OR STOCKPILE AREA OUTSIDE THE PROJECT AREA, THE CONTRACTOR SHALL COMPLY WITH THE STANDARDS FOR SOIL EROSION AND SEDIMENT CONTROL IN N.J. 7TH EDITION, JANUARY 2014, REVISED JULY 2017 AND OBTAIN THE NECESSARY SOIL CONSERVATION DISTRICT PERMITS AND APPROVALS.

STATE OF NEW JERSEY · BETH-ANN M. GRASSO · No. GE 43121 · LICENSED PROFESSIONAL ENGINEER

CD1
CD15

Pennoni

NJ COA NO. GA28033300
BETH-ANN M. GRASSO, P.E.
4/28/2026
PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

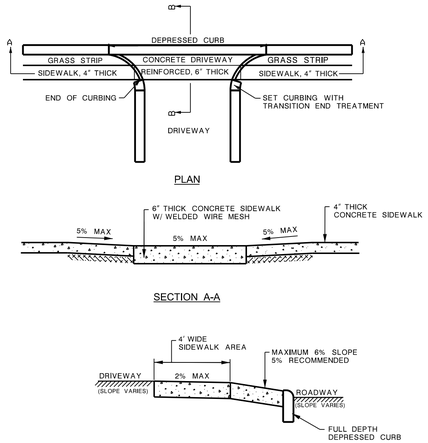
OCEAN COUNTY NEW JERSEY

DATE	REVISIONS	BY	CHK.

RECONSTRUCTION OF
NORTH COUNTY LINE RD
FROM JACKSON MILLS RD TO BENNETTS MILLS RD
JACKSON TOWNSHIP

CONSTRUCTION DETAILS & SOIL EROSION DETAILS

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE	DRAWN C.D.	APPROVED B.M.G.
FILE NO.	12 - 009 - 0128		SHEET 64 OF 84



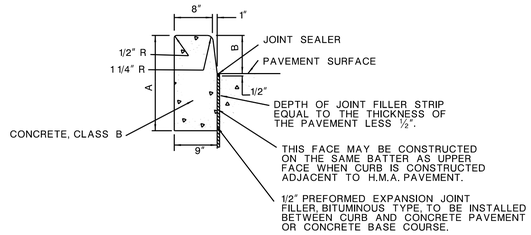
NOTES:

1. AN ACCESSIBLE PEDESTRIAN ROUTE MUST CONTINUE THROUGH THE DRIVEWAY IN LINE WITH THE EXISTING SIDEWALK.
2. DRIVEWAY WIDTH AND CURB RADII SHALL BE AS SHOWN ON PLANS AND SHALL CONFORM WITH LOCAL MUNICIPAL ORDINANCE OR 1989 SUBDIVISION AND SITE PLAN RESOLUTION.

CONCRETE APRON WITH SIDEWALK (COMMERCIAL PROPERTIES)

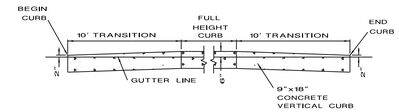
NOT TO SCALE

CURB SIZE	DIM. A	DIM. B
9" X 14"	14"	2"
9" X 16"	16"	4"
9" x 18"	18"	6"
9" x 20"	20"	8"
9" x 22"	22"	10"

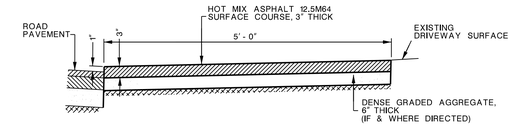


TRANSVERSE JOINTS 1/2" WIDE SHALL BE INSTALLED IN THE CURB 20' APART AND SHALL BE FILLED WITH PREFORMED BITUMINOUS IMPREGNATED FIBER JOINT FILLER RECESSED ¼" IN FROM FRONT FACE AND TOP OF CURB. EXPANSION JOINTS THROUGH AND ADJACENT TO THE CURB SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE CURB.

CONCRETE VERTICAL CURB

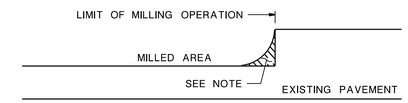


CURB TRANSITION "BEGIN CURB" AND "END CURB"



HOT MIX ASPHALT DRIVEWAY, 3" THICK WITHOUT CURBING

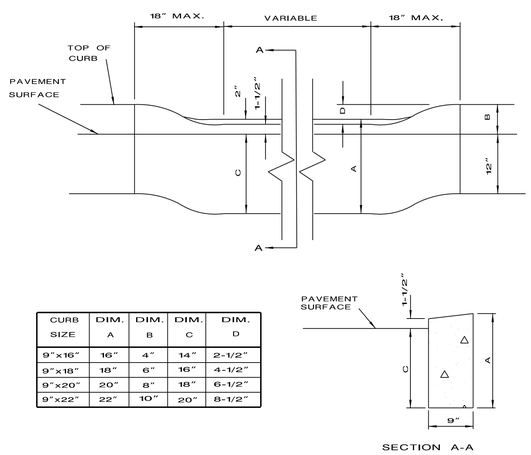
NOT TO SCALE



NOTE:
REMOVE THE HMA MATERIAL LEFT BY THE DRUM RADIUS AT THE LIMITS OF THE MILLING OPERATION. ENSURE THAT THE FACE IS CLEAN AND VERTICAL BY SAWCUTTING OR TRANSVERSE MILLING. THIS END TREATMENT IS NOT APPLICABLE TO TEMPORARY LIMITS OF MILLING (i.e. END OF WORKDAY). IT IS APPLICABLE TO ALL AREAS WHERE THE COMPLETED MILLING OPERATION MATCHES ANY EXISTING PAVEMENT INCLUDING BRIDGES.

END TREATMENT FOR MILLING OPERATIONS

CD-401-1.2



CURB SIZE	DIM. A	DIM. B	DIM. C	DIM. D
9"x16"	16"	4"	14"	2-1/2"
9"x18"	18"	6"	16"	4-1/2"
9"x20"	20"	8"	18"	6-1/2"
9"x22"	22"	10"	20"	8-1/2"

METHOD OF DEPRESSING CURB AT DRIVEWAYS

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

4/28/2026

PROFESSIONAL ENGINEER
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OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

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CONSTRUCTION DETAILS

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE NOT TO SCALE	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 65 OF 84

CD15

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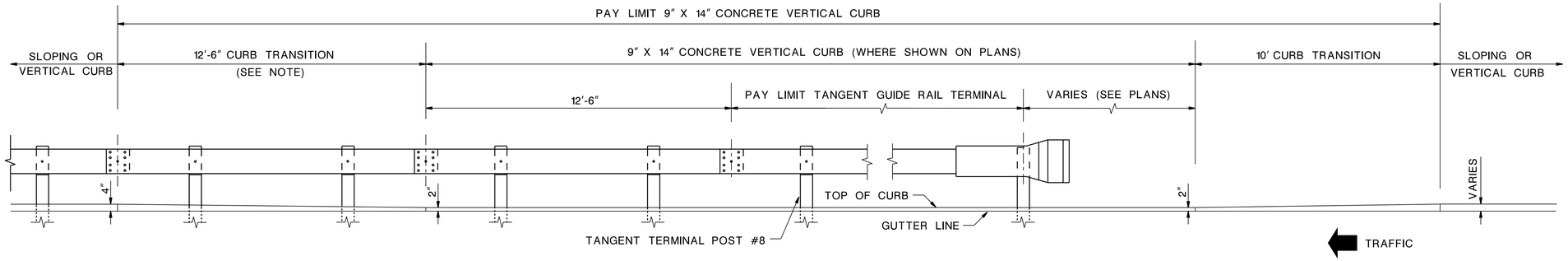
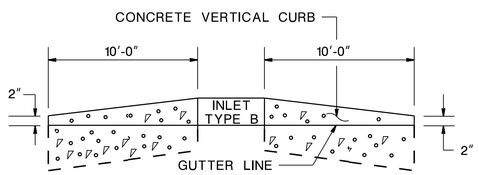


NOTE: A RAIL HEIGHT TRANSITION MAY ALSO BE REQUIRED. SEE CD-609-5.

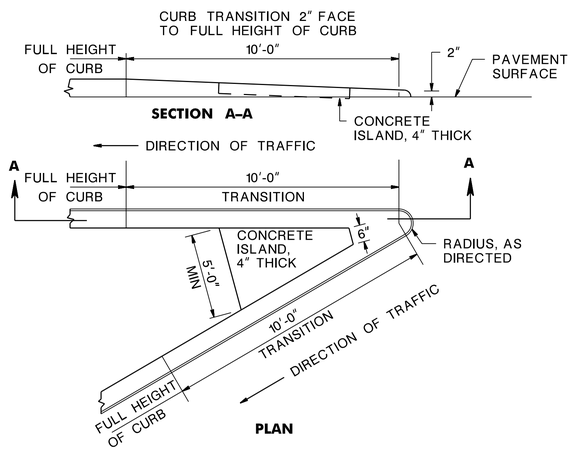
METHOD OF TRANSITIONING TO 2″ VERTICAL CURB AT A TANGENT GUIDE RAIL TERMINAL

CD-607-2.1



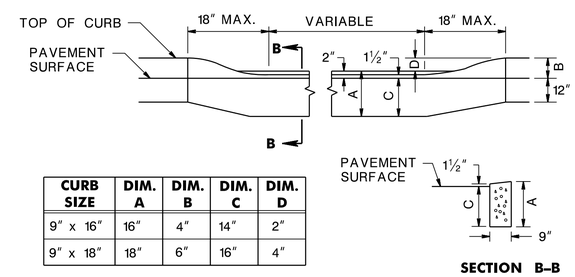
CURB TREATMENT AT BERM SECTION AND ALL CURB ENDS

CD-607-2.2



APPROACH CURBED GORE AREA TREATMENT

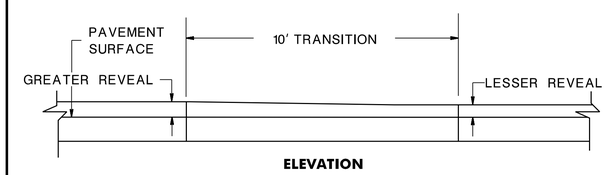
CD-607-2.3



CURB SIZE	DIM. A	DIM. B	DIM. C	DIM. D
9″ x 16″	16″	4″	14″	2″
9″ x 18″	18″	6″	16″	4″

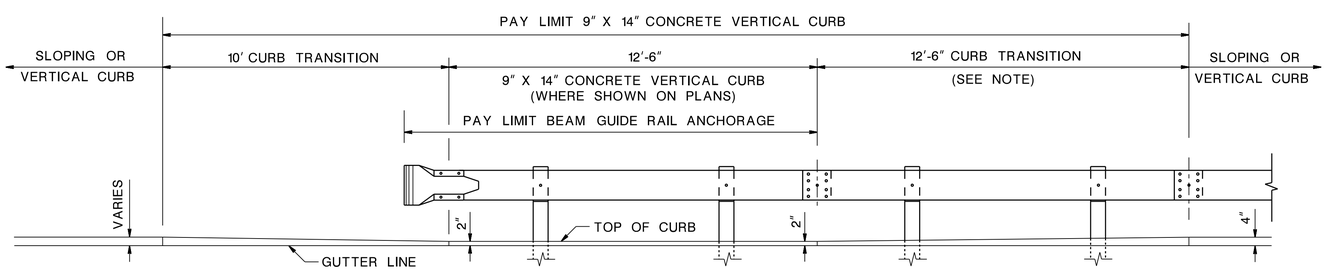
METHOD OF DEPRESSING CURB AT DRIVEWAYS

CD-607-2.4



LINEAR CURB TRANSITION

CD-607-2.5



NOTE: A RAIL HEIGHT TRANSITION MAY ALSO BE REQUIRED. SEE CD-609-4.

METHOD OF TRANSITIONING TO 2″ VERTICAL CURB AT A BEAM GUIDE RAIL ANCHORAGE

CD-607-2.6

CURB TRANSITIONS

N.T.S.

CD-607-2

NEW JERSEY DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS

CD3 / CD15

BDC16D-07-TRANSITION WITH GUIDE RAIL REVISED
BDC17D-00-TRANSITION WITH GUIDE RAIL ADDED
BDC17D-02-MASH TITLE ADDED
BDC16D-01-ORIGINAL SHEET

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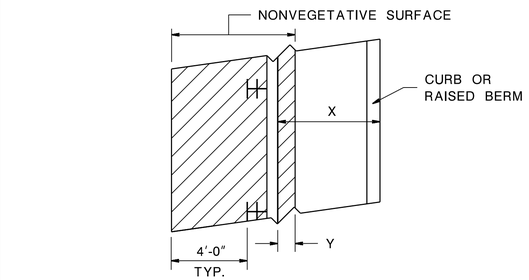
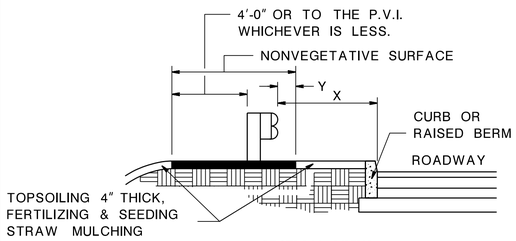


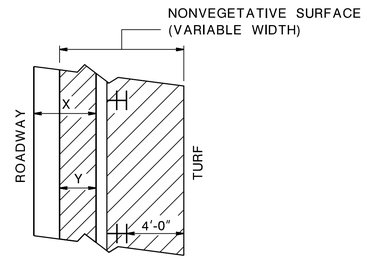
PLAN VIEW



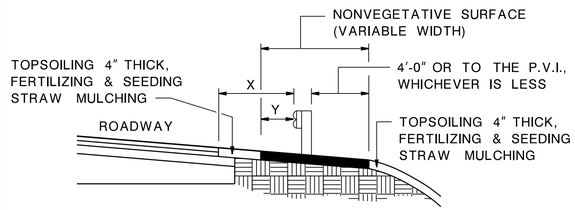
SECTION VIEW

NONVEGETATIVE SURFACES AROUND GUIDE RAIL BEHIND CURB OR RAISED BERM

CD-608-1.1



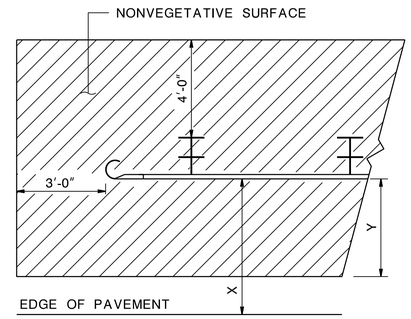
PLAN VIEW



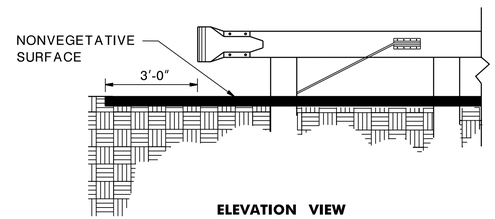
SECTION VIEW

NONVEGETATIVE SURFACE AT EDGE OF PAVEMENT ON UMBRELLA SECTION WHERE GUIDE RAIL IS USED

CD-608-1.2



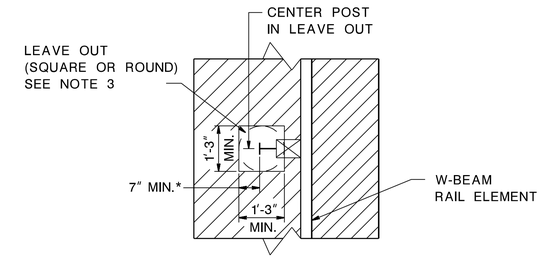
PLAN VIEW



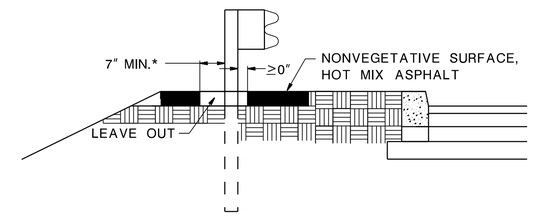
ELEVATION VIEW

NONVEGETATIVE SURFACES AROUND BEAM GUIDE RAIL ANCHORAGE

CD-608-1.3



PLAN VIEW

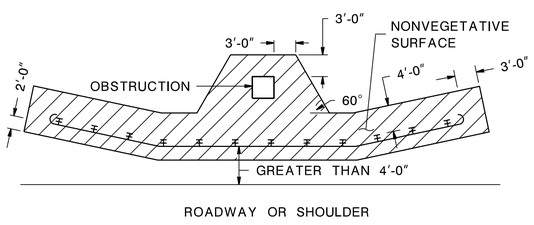


SECTION VIEW

* FOR DUAL FACED GUIDE RAIL, THE 7" MINIMUM APPLIES TO BOTH SIDES OF THE POST.

LEAVE OUT FOR STANDARD ITEM "NONVEGETATIVE SURFACE, HOT MIX ASPHALT" ONLY

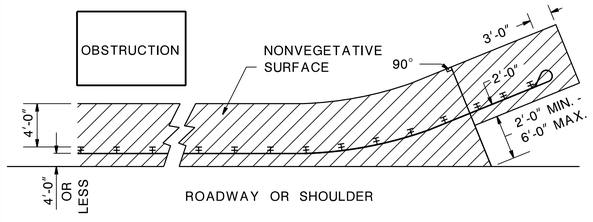
CD-608-1.4



PLAN VIEW

NONVEGETATIVE SURFACE AROUND FLARED GUIDE RAIL WHERE GUIDE RAIL OFFSET FROM EDGE OF PAVEMENT IS GREATER THAN 4'-0"

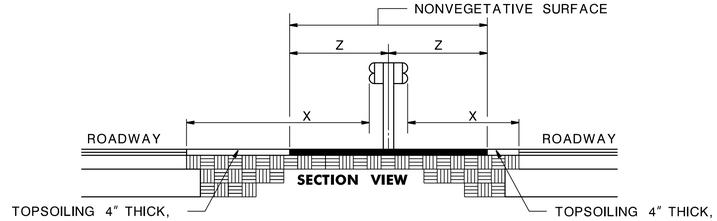
CD-608-1.5



PLAN VIEW

NONVEGETATIVE SURFACE AROUND FLARED GUIDE RAIL WHERE GUIDE RAIL OFFSET FROM EDGE OF PAVEMENT IS 4'-0" OR LESS

CD-608-1.6



Z = 3' FOR DUAL FACED BEAM GUIDE RAIL
4' FOR DUAL FACED MODIFIED THRIE BEAM

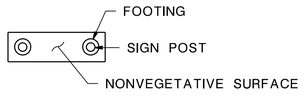
NOTE:
WHERE X IS LESS THAN OR EQUAL TO 4', USE Y TO DETERMINE NONVEGETATIVE SURFACE TREATMENT FOR THAT SIDE OF GUIDE RAIL.

NONVEGETATIVE SURFACE UNDER MEDIAN GUIDE RAIL

CD-608-1.7

X	Y
GUIDE RAIL OFFSET FROM EDGE OF PAVEMENT	**WIDTH OF NONVEGETATIVE SURFACE IN FRONT OF GUIDE RAIL**
GREATER THAN 4'-0"	2'-0"
LESS THAN OR EQUAL TO 4'-0"	Y=X

CD-608-1.8



THE NONVEGETATIVE SURFACE IS TO FORM A RECTANGULAR PAD WHOSE OUTSIDE LIMITS EXTEND A MINIMUM OF 3'-0" BEYOND THE POST FOOTING.

PLAN VIEW

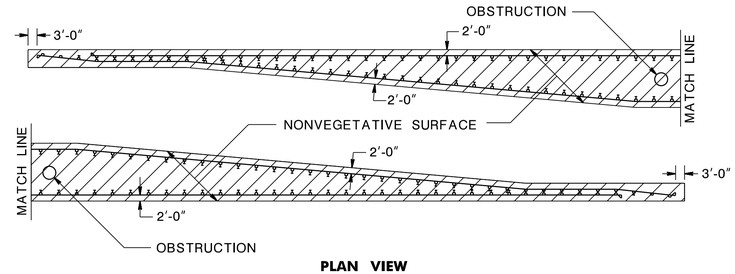
NONVEGETATIVE SURFACE AROUND OVERHEAD SIGN FOUNDATIONS AND UNDER LARGE GROUND MOUNTED SIGNS

CD-608-1.9

GENERAL NOTES:

1. IF THE END OF THE GUIDE RAIL IS BURIED IN THE SLOPE, THE LIMIT OF NONVEGETATIVE SURFACE RELATIVE TO THE BURIED GUIDE RAIL WILL BE DETERMINED BY THE RE.
2. SEE TYPICAL SECTIONS FOR CROSS SLOPES IN ROADSIDE (BORDER OR SIDEWALK AREA).
3. LEAVE OUTS CAN BE FILLED WITH:
   (a) COARSE AGGREGATE, SIZE NO. 57 TO BE HAND TAMPED, THEN SEAL SURFACE WITH EMULSIFIED ASPHALT AT 0.35 GAL/SY ±0.05 AS PER STANDARD SPECIFICATIONS SECTION 902; OR
   (b) COARSE AGGREGATE, SIZE NO. 57 IN BASE OF LEAVE OUT AND TOP WITH NONVEGETATIVE SURFACE, HMA, 2" THICK. GRADE TO DRAIN AND HAND TAMP LEAVE OUT SURFACE.

CD-608-1.10



PLAN VIEW

NONVEGETATIVE SURFACE AT MEDIAN GUIDE RAIL

CD-608-1.11

CD4
CD15

NONVEGETATIVE SURFACE

N.T.S.

CD-608-1

NEW JERSEY DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS

BDC18D-07-REVISIONS TO CD-608-1.4 & CD-608-1.11
BDC17D-02-REVISIONS TO CD-608-1
BDC16D-01-ORIGINAL SHEET

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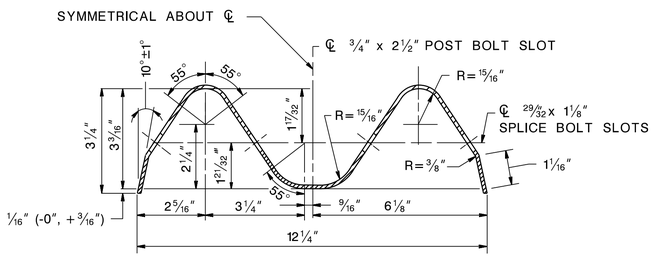
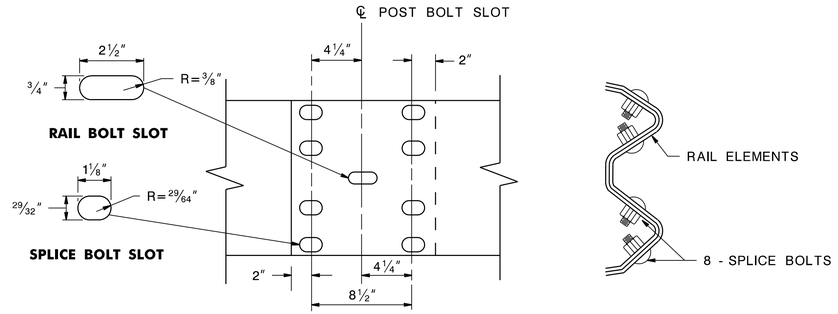
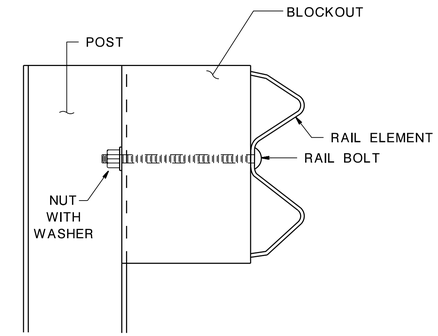


RAIL ELEMENT TO BE SUPPLIED IN LENGTHS OF 13'-6 1/2" OR 26'-0 1/2"

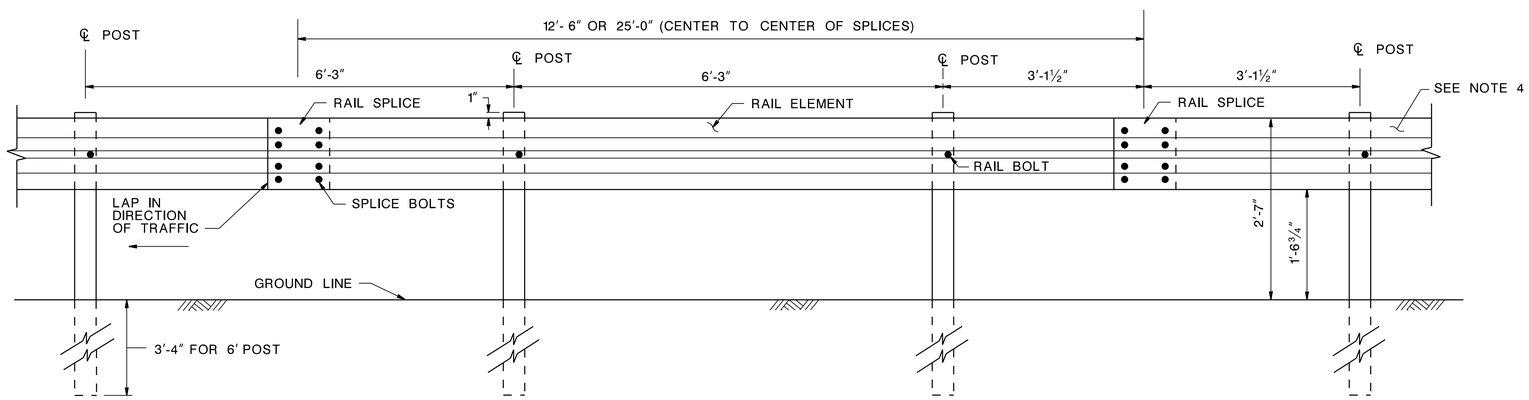
W-BEAM RAIL ELEMENT



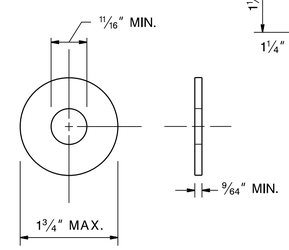
RAIL SPLICE



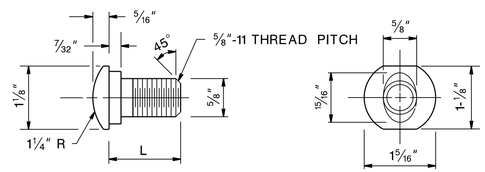
BEAM GUIDE RAIL POST ASSEMBLY



BEAM GUIDE RAIL

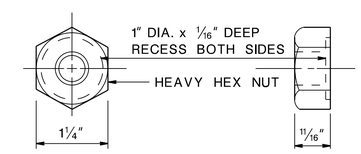


STEEL WASHER



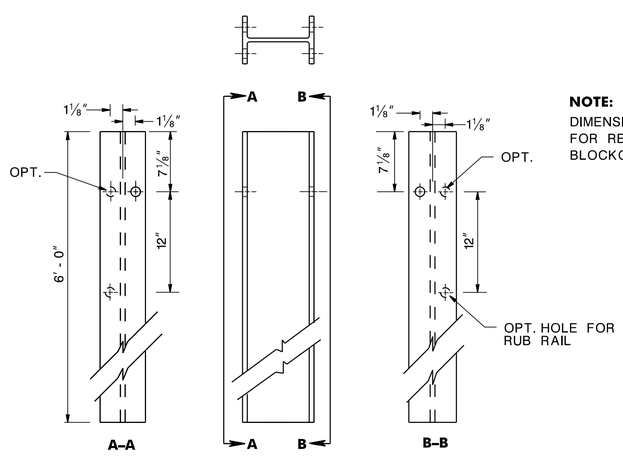
TYPE	L	MIN. THREAD LENGTH
SPLICE	1 1/4"	FULL LENGTH THREAD
RAIL	9 1/2"	1 3/4"

5/8" DIA. BUTTON HEAD BOLT



5/8" DIA. RECESS NUT

SPLICE & RAIL NUT & BOLT

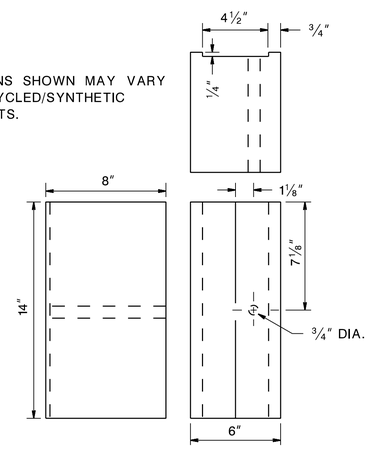


W6x8.5 OR W6x9 STEEL POST

NOTE:
ALL HOLES 13/16" DIA.

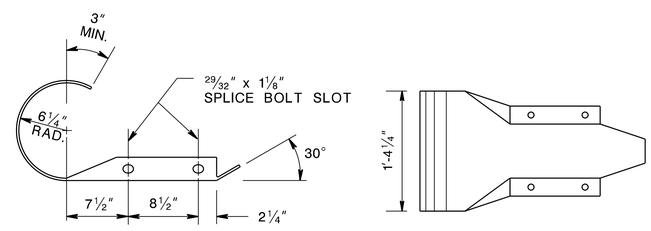
6' POST

NOTE:
DIMENSIONS SHOWN MAY VARY FOR RECYCLED/SYNTHETIC BLOCKOUTS.



APPROVED RECYCLED/ SYNTHETIC MATERIALS

6"x8"x14" BLOCKOUT



END SECTION (ROUNDED)

NOTES:

1. ALL DIMENSIONS ARE SUBJECT TO MANUFACTURING TOLERANCES.
2. FURNISH RAIL ELEMENTS SHOPCURVED, CONCAVE OR CONVEX, FOR RADII BETWEEN 20 AND 150 FEET.
3. WHERE TRANSITIONING TO EXISTING GUIDE RAIL, AN END TERMINAL, OR A CRASH CUSHION MOUNTED AT A HEIGHT OTHER THAN 2'-7", THE VERTICAL TRANSITION TO BE ACCOMPLISHED IN A MINIMUM LENGTH OF 12'-6" FOR EACH 2" OF VERTICAL CHANGE. SEE CD-609-8.
4. INSTALL AN END TERMINAL AS SHOWN ON THE PLANS. USE THE END SECTION (ROUNDED) ON THE END OF THE RAIL ELEMENT WHERE DUAL FACED BEAM GUIDE RAIL ENDS AND SINGLE FACED BEAM GUIDE RAIL BEGINS.

BEAM GUIDE RAIL (MASH TL-3)

N.T.S.

CD-609-1

NEW JERSEY DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS

CD-609-1.1

CD5 / CD15

BDC17D-02-REVISIONS TO CD-609-1
BDC16D-01-ORIGINAL SHEET

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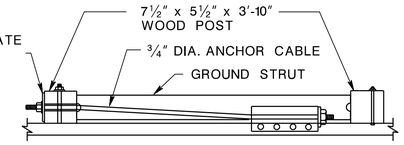
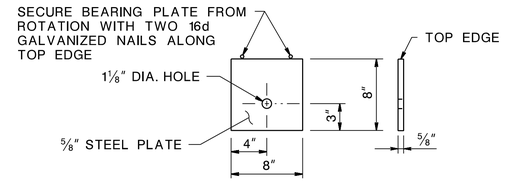
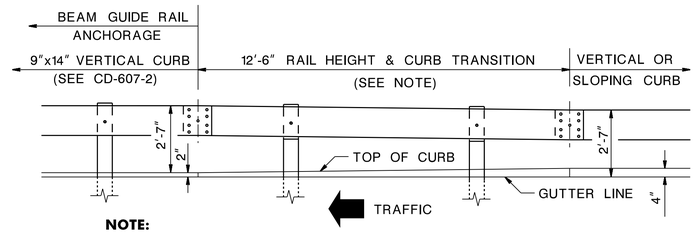


PLAN

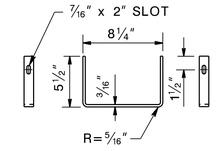


BEARING PLATE

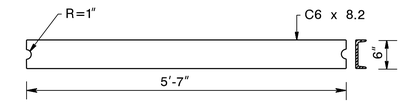


NOTE:
WHERE GUIDE RAIL IS OFFSET 4 FEET OR MORE FROM THE GUTTER LINE, RAIL HEIGHT IS MEASURE FROM THE GROUND LINE (CD-609-8A). A RAIL HEIGHT TRANSITION IS NOT REQUIRED.

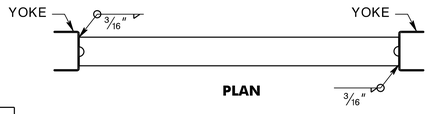
RAIL HEIGHT TRANSITION FOR GUIDE RAIL ANCHORAGE WITH 2" VERTICAL CURB WHERE GUIDE RAIL IS OFFSET LESS THAN 4 FEET FROM THE GUTTER LINE



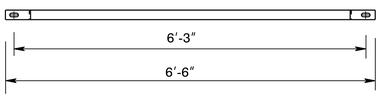
YOKE DETAIL



STRUT DETAIL

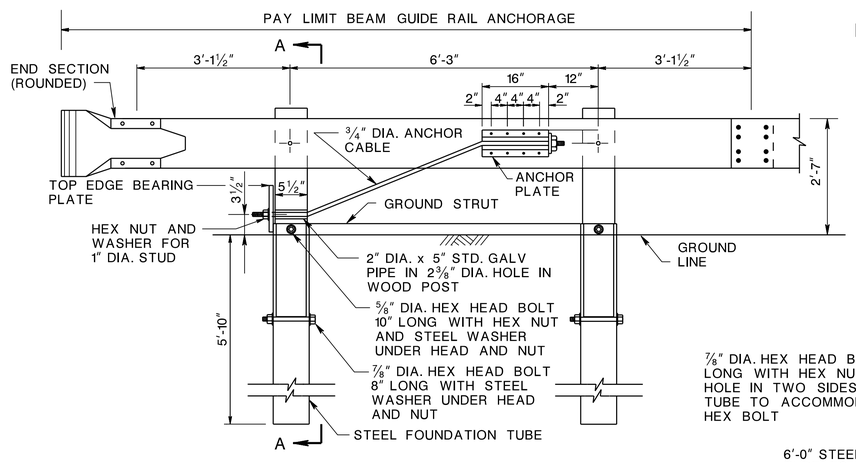


PLAN



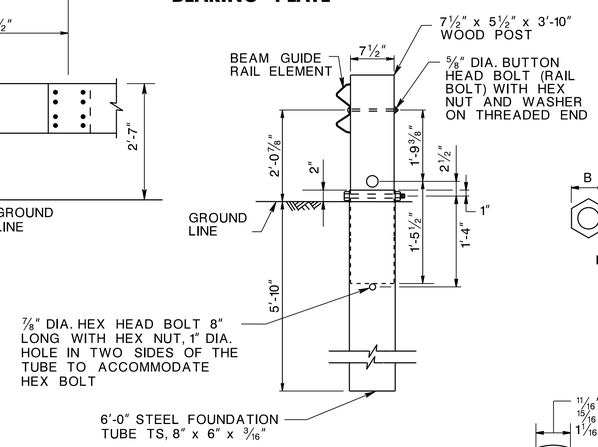
ELEVATION

GROUND STRUT

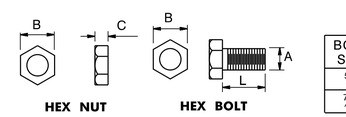


ELEVATION

BEAM GUIDE RAIL ANCHORAGE

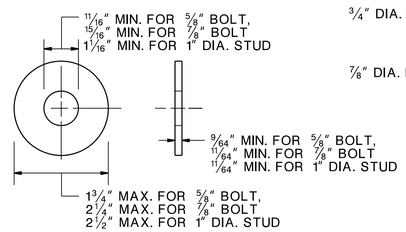


SECTION A–A

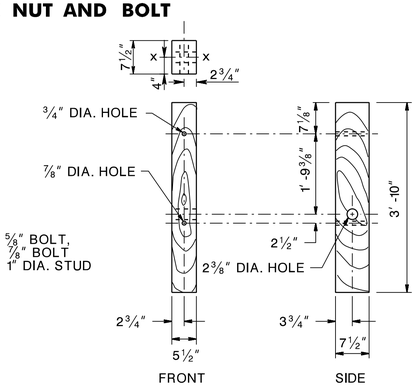


BOLT SIZE	THREAD PITCH	A	B	C	L
⅝"	⅝-11	⅝"	15/16"	35/64"	1½", 10"
⅞"	⅞-9	⅞"	1 5/16"	¾"	8"

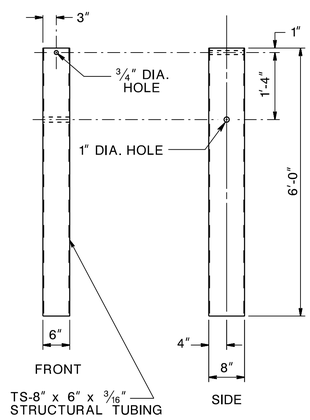
HEX NUT AND BOLT



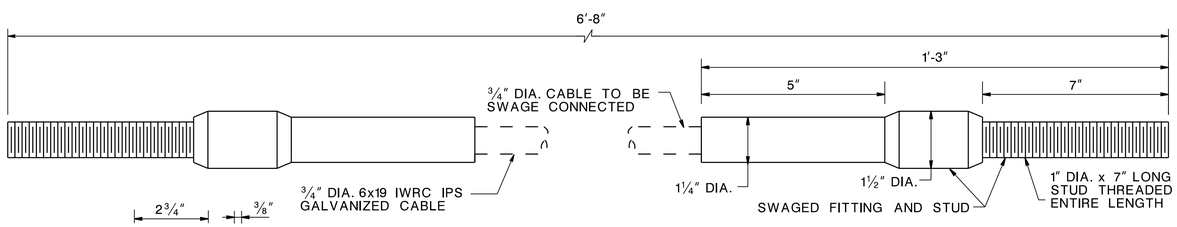
STEEL WASHER



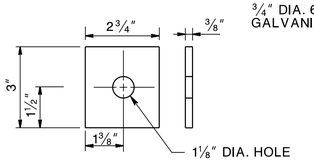
WOOD POST



STEEL FOUNDATION TUBE

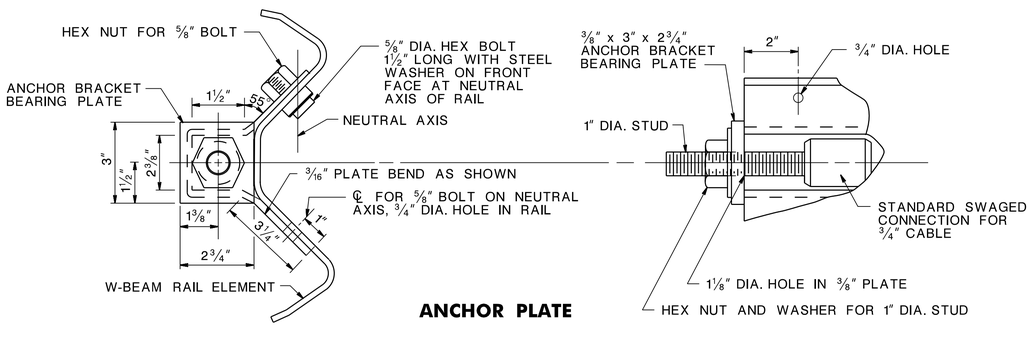


CABLE ASSEMBLY

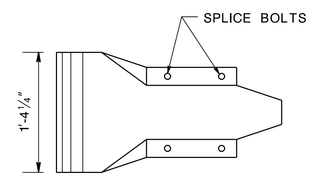


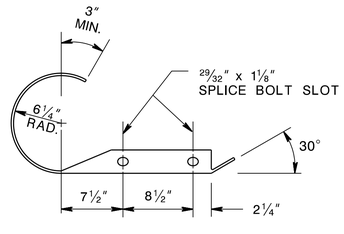
ANCHOR BRACKET BEARING PLATE

NOTE:
OTHER ANCHOR CABLE ASSEMBLIES MAY BE USED. MINIMUM BREAKING STRENGTH OF ASSEMBLY TO BE 42,700 LBS.

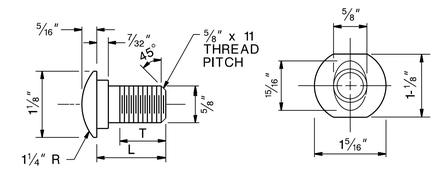


ANCHOR PLATE



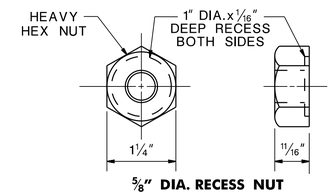


END SECTION (ROUNDED)



⅝" DIA. BUTTON HEAD BOLT

TYPE	L	MIN. THREAD LENGTH (T)
RAIL	10"	4"
SPLICE	1¼"	FULL LENGTH THREAD



⅝" DIA. RECESS NUT

RAIL NUT & BOLT

BEAM GUIDE RAIL ANCHORAGE (MASH TL–3)

N.T.S.

CD-609-4

NEW JERSEY DEPARTMENT OF TRANSPORTATION

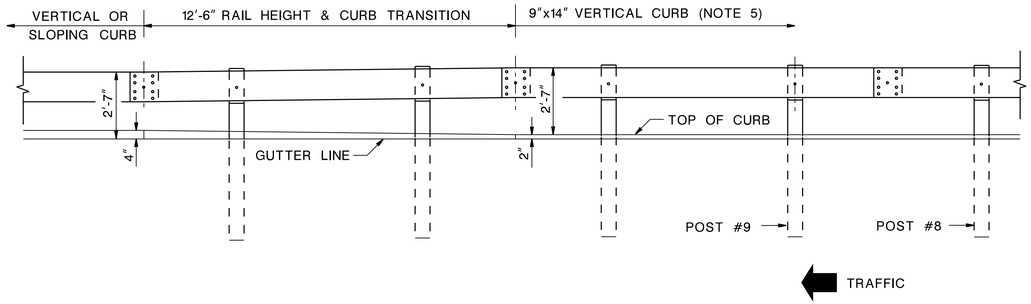
CONSTRUCTION DETAILS

CD-609-4.1

CD6 / CD15

BDC16D-07-ANCHORAGE WITH CURB REVISED	date=
BDC17D-10-ANCHORAGE WITH CURB ADDED	scale=
BDC17D-02-REVISIONS TO CD-609-4	pen table=
BDC16D-01-ORIGINAL SHEET	ID=

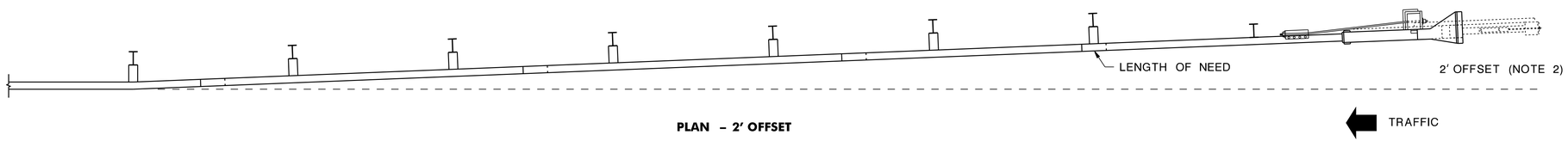
file=



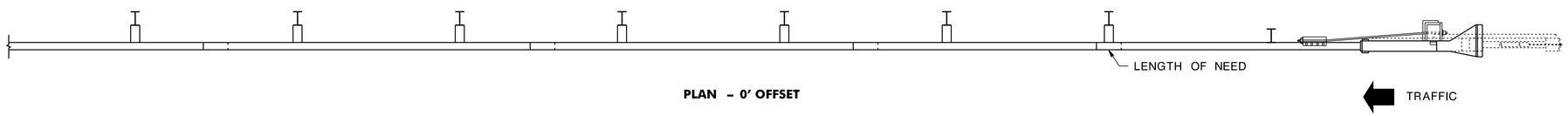
RAIL HEIGHT TRANSITION FOR TANGENT GUIDE RAIL TERMINAL WITH 2" VERTICAL CURB WHERE GUIDE RAIL IS OFFSET LESS THAN 4 FEET FROM THE GUTTER LINE (NOTE 6)

NOTES:

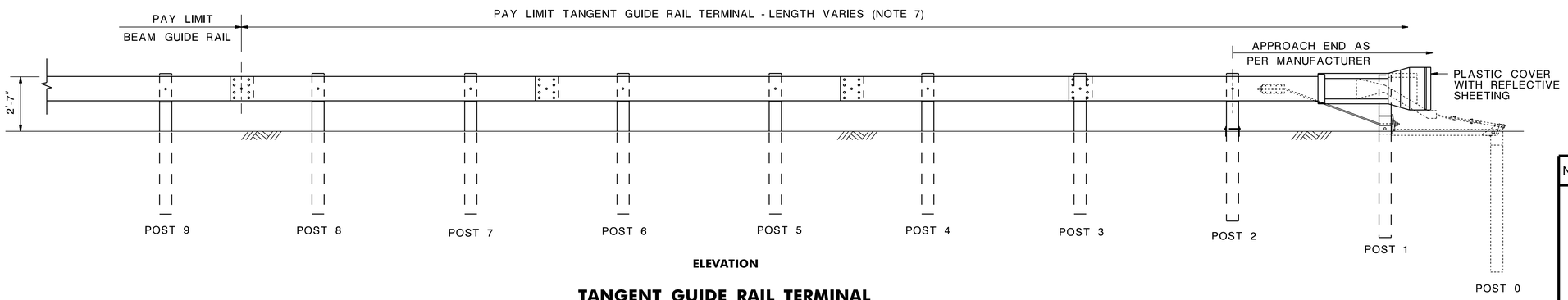
1. NUMBER OF POSTS, TYPE OF POST, POST SPACING, FLARE RATE, AND MATERIALS TO BE IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS AND THE DEPARTMENT'S QUALIFIED PRODUCTS LIST.
2. THE LOCATION OF THE 2 FOOT OFFSET VARIES WITH EACH MANUFACTURER. WHERE A 2 FOOT OFFSET IS SHOWN ON THE PLANS, CONSTRUCT THE TANGENT GUIDE RAIL TERMINAL WITH A STRAIGHT FLARE FOR ITS ENTIRE LENGTH AS PER THE MANUFACTURER.
3. WHERE GUIDE RAIL IS INSTALLED FLUSH WITH THE GUTTER LINE OR OFFSET 6 INCHES FROM THE GUTTER LINE, CONSTRUCT THE TANGENT GUIDE RAIL TERMINAL WITH A TWO FOOT OFFSET SO THAT THE TERMINAL END DOES NOT PROTRUDE INTO THE ROADWAY.
4. WHERE THE DOWNSTREAM GUIDE RAIL IS ON A HORIZONTAL CURVE, CONSTRUCT THE TANGENT GUIDE RAIL TERMINAL IN A STRAIGHT LINE AS SHOWN ON THIS DETAIL (DO NOT FOLLOW THE HORIZONTAL CURVE).
5. 9"x14" CONCRETE VERTICAL CURB WHERE SHOWN ON PLANS. SEE CD-607-2 FOR ADDITIONAL CURB TRANSITION DETAILS.
6. WHERE GUIDE RAIL IS OFFSET 4 FEET OR MORE FROM THE GUTTER LINE, RAIL HEIGHT OF THE GUIDE RAIL AND TANGENT GUIDE RAIL TERMINAL IS MEASURED FROM THE GROUND LINE (CD-609-8A). A RAIL HEIGHT TRANSITION IS NOT REQUIRED.
7. LENGTH OF TANGENT GUIDE RAIL TERMINAL AS PER MANUFACTURER. SEE QUALIFIED PRODUCTS LIST.
8. LOCATION OF POST #1 AS SHOWN ON THE PLANS.



PLAN – 2' OFFSET



PLAN – 0' OFFSET



ELEVATION

TANGENT GUIDE RAIL TERMINAL

TANGENT GUIDE RAIL TERMINAL (MASH TL-3)

N.T.S.

CD-609-5

NEW JERSEY DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS

CD-609-5.1

CD7 / CD15

70 / 84

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BDC17D-10-TERMINAL WITH CURB ADDED			
BDC17D-02-REVISIONS TO CD-609-5			
BDC16D-01-ORIGINAL SHEET			

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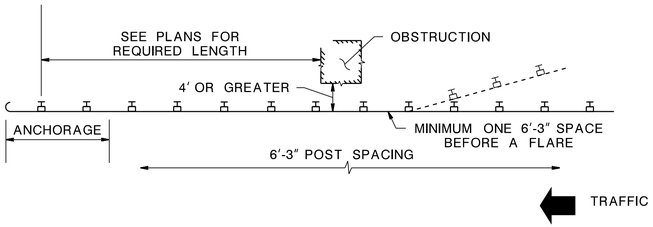
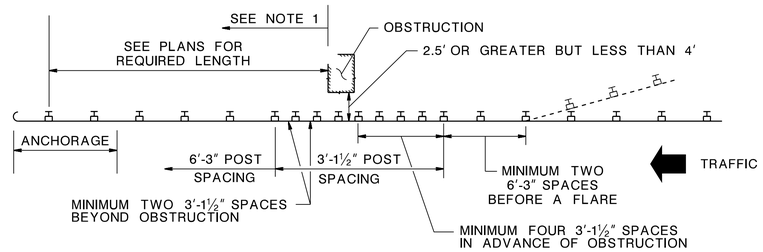
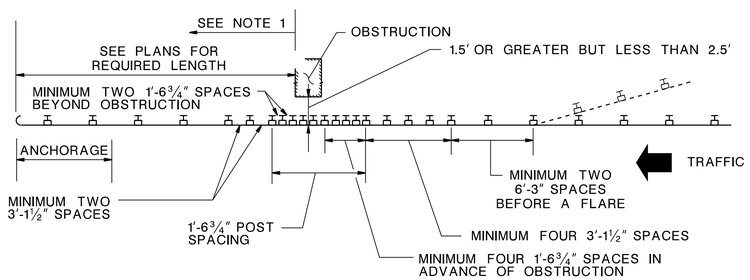


WHERE CLEARANCE FROM FACE OF RAIL TO OBSTRUCTION IS 4′ OR GREATER (SEE NOTE 2)



WHERE CLEARANCE FROM FACE OF RAIL TO OBSTRUCTION IS 2.5′ OR GREATER BUT LESS THAN 4′ (SEE NOTE 2)



WHERE CLEARANCE FROM FACE OF RAIL TO OBSTRUCTION IS 1.5′ OR GREATER BUT LESS THAN 2.5′ (SEE NOTE 2)

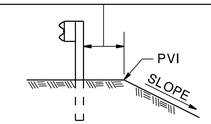
NOTES:

1. WHERE AN APPROACH END TREATMENT AT THE TRAILING END OF GUIDE RAIL IS SHOWN ON THE PLANS, THE POST SPACING REQUIREMENTS SHALL BE THE SAME AS THE APPROACH END.
2. IN A FILL SECTION WHERE THE DISTANCE FROM THE BACK OF THE POST TO THE PVI IS LESS THAN 1′ AND THE SLOPE IS STEEPER THAN 3:1, THE MINIMUM CLEARANCE FROM THE FACE OF THE RAIL TO AN OBSTRUCTION IS INCREASED BY 1′ DUE TO INCREASED POST DEFLECTION.
3. ADDITIONAL POSTS AND BLOCKOUTS WILL BE PAID FOR UNDER PAY ITEM "BEAM GUIDE RAIL POST".

CLEARANCE FROM FACE OF RAIL TO OBSTRUCTION

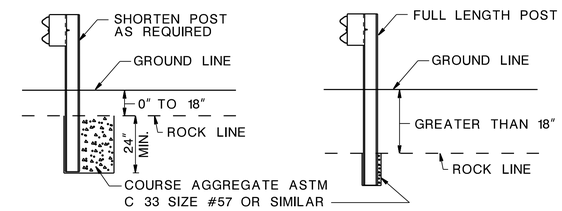
CD-609-8.1

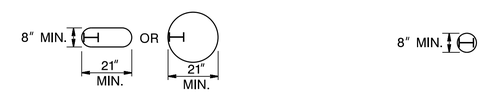
DISTANCE FROM BACK OF POST TO PVI	SLOPE	ADDITIONAL POST LENGTH
IF LESS THAN 2′ BUT GREATER OR EQUAL TO 1′	6:1 OR FLATTER	NO CHANGE
	STEEPER THAN 6:1 TO 3:1	1′
	STEEPER THAN 3:1 TO 2:1	2′
IF LESS THAN 1′	6:1 OR FLATTER	1′
	STEEPER THAN 6:1 TO 3:1	2′
	STEEPER THAN 3:1 TO 2:1	3′



ADDITIONAL LENGTH BEAM GUIDE RAIL POSTS

CD-609-8.2

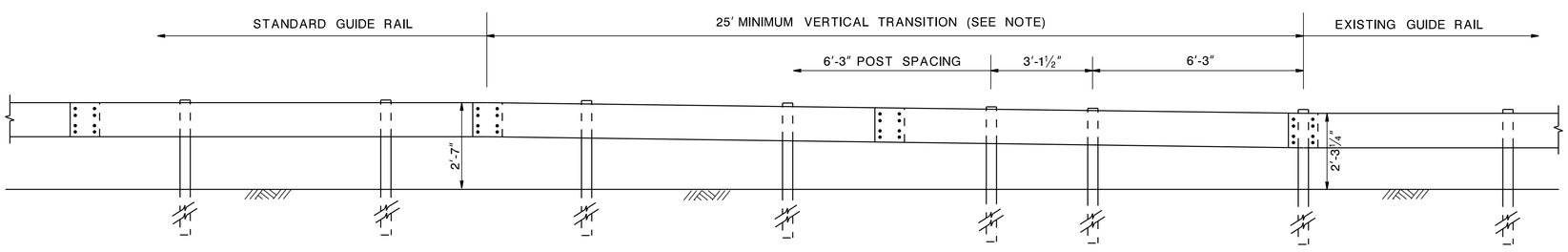




CASE 1 **CASE 2**

GUIDE RAIL POST INSTALLATION IN ROCK

CD-609-8.3



NOTE:
WHERE TRANSITIONING TO EXISTING GUIDE RAIL, AN END TERMINAL, OR A CRASH CUSHION MOUNTED AT A HEIGHT OTHER THAN 2′-7″, THE VERTICAL TRANSITION SHALL BE ACCOMPLISHED IN A MINIMUM LENGTH OF 12′-6″ FOR EACH 2″ OF VERTICAL CHANGE.

VERTICAL TRANSITION TO EXISTING 27¼″ HIGH GUIDE RAIL

CD-609-8.4 CD8 / CD15

BEAM GUIDE RAIL TREATMENTS

N.T.S.

CD-609-8

NEW JERSEY DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS

BDC18D-07-CLEARANCE DETAILS REVISED
BDC17D-02 REVISIONS TO CD-609-8
BDC16R-01-ORIGINAL SHEET

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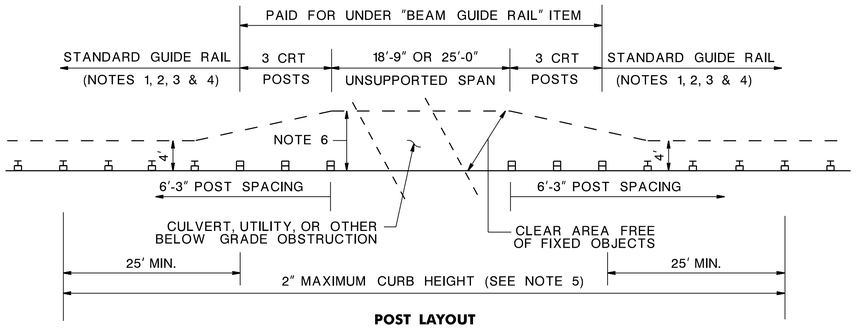
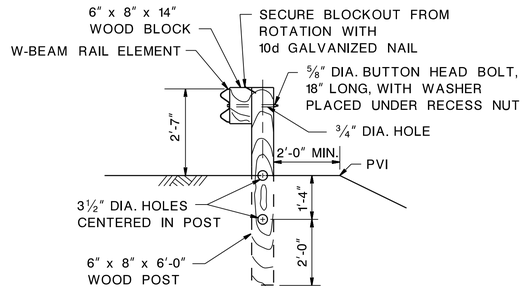


POST LAYOUT



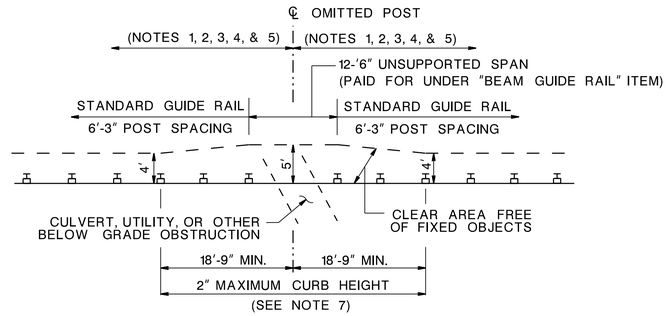
CRT POST FOR UNSUPPORTED SPAN

NOTES:

1. A MINIMUM OF TEN 6'-3" POST SPACES OF TANGENT GUIDE RAIL ARE REQUIRED BETWEEN THE OUTER CRT POSTS OF CONSECUTIVE 18-9" OR 25'-0" UNSUPPORTED SPANS.
2. THE OUTER CRT POSTS MUST BE A MINIMUM OF TEN 6'-3" POST SPACES FROM THE APPROACH END OF A TANGENT GUIDE RAIL TERMINAL AND EIGHT 6'-3" POST SPACES FROM THE BEGINNING OF A FLARE OR REDUCED POST SPACING.
3. THE OUTER CRT POSTS MUST BE A MINIMUM OF TEN 6'-3" POST SPACES FROM THE LAST POST OF AN END ANCHORAGE.
4. THE OUTER CRT POSTS MUST BE A MINIMUM OF SIX 6'-3" POST SPACES FROM A THRIE BEAM TO W-BEAM ASYMMETRICAL TRANSITION SECTION.
5. WHERE THERE IS CURB, THE MAXIMUM CURB HEIGHT IS 2" FROM 25' IN ADVANCE OF THE FIRST CRT POST ON THE APPROACH END TO 25' PAST THE LAST CRT POST ON THE TRAILING END.
6. THE REQUIRED CLEAR AREA FREE OF FIXED OBJECTS IS 7' FOR AN 18'-9" UNSUPPORTED SPAN AND 8' FOR A 25'-0" UNSUPPORTED SPAN.
7. IF THERE IS A VERTICAL DROPOFF BEHIND THE UNSUPPORTED SPAN, THE FACE OF RAIL MUST BE A MINIMUM OF 3' FROM THE DROPOFF.

18'–9" OR 25'–0' UNSUPPORTED SPAN

CD-609-8A.1

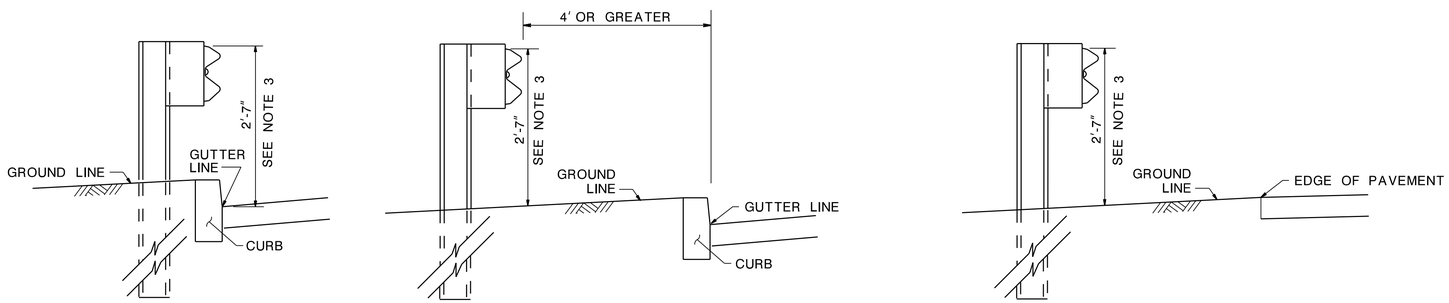


NOTES:

1. A MINIMUM OF NINE 6'-3" POST SPACES OF TANGENT GUIDE RAIL ARE REQUIRED BETWEEN TWO CONSECUTIVE SINGLE POST OMISSIONS.
2. THE OMITTED POST MUST BE A MINIMUM OF TEN 6'-3" POST SPACES FROM THE APPROACH END OF A TANGENT GUIDE RAIL TERMINAL AND FIVE 6'-3" POST SPACES FROM THE BEGINNING OF A FLARE OR REDUCED POST SPACING.
3. THE OMITTED POST MUST BE A MINIMUM OF TEN 6'-3" POST SPACES FROM THE LAST POST OF AN END ANCHORAGE.
4. THE OMITTED POST MUST BE A MINIMUM OF SIX 6'-3" POST SPACES FROM A THRIE BEAM TO W-BEAM ASYMMETRICAL TRANSITION SECTION.
5. THE OMITTED POST MUST BE A MINIMUM OF SEVEN 6'-3" POST SPACES FROM AN OUTER CRT POST OF AN 18'-9" OR 25'-0" UNSUPPORTED SPAN.
6. THE REQUIRED CLEAR AREA FREE OF FIXED OBJECTS IS 5' BEHIND A 12'-6" UNSUPPORTED SPAN.
7. WHERE THERE IS CURB, THE MAXIMUM CURB HEIGHT IS 2" FOR A MINIMUM LENGTH OF 18'-9" IN ADVANCE OF AND ON THE TRAILING END OF THE OMITTED POST.

12'–6" UNSUPPORTED SPAN

CD-609-8A.2



GUIDE RAIL FLUSH WITH GUTTER LINE (SEE NOTE 4)

GUIDE RAIL OFFSET 4' OR GREATER FROM GUTTER LINE

RAIL HEIGHT DETERMINATION WITH CURB

RAIL HEIGHT DETERMINATION WITHOUT CURB

NOTES:

1. WHERE GUIDERAIL ADJACENT TO CURB IS FLUSH WITH THE GUTTER LINE (6" OFFSET FOR SLOPING CURB) AND IS TAPERED TO AN OFFSET OF 4' OR GREATER, A VERTICAL TRANSITION IS REQUIRED. THE VERTICAL TRANSITION SHALL BE ACCOMPLISHED IN A MINIMUM LENGTH OF 12'-6" FOR EACH 2" OF VERTICAL CHANGE.
2. SEE PLANS FOR GUIDE RAIL OFFSET.
3. 2'-10" FOR MODIFIED THRIE BEAM GUIDE RAIL.
4. FOR SLOPING CURB, FACE OF RAIL IS OFFSET 6" FROM GUTTER LINE.

RAIL HEIGHT DETERMINATION

CD-609-8A.3

CD9 / CD15

BEAM GUIDE RAIL TREATMENTS (MASH TL–3)

N.T.S.

CD-609-8A

NEW JERSEY DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS

BDC18D-07-UNSUPPORTED SPAN NOTES REVISED
BDC17D-02-NEW SHEET

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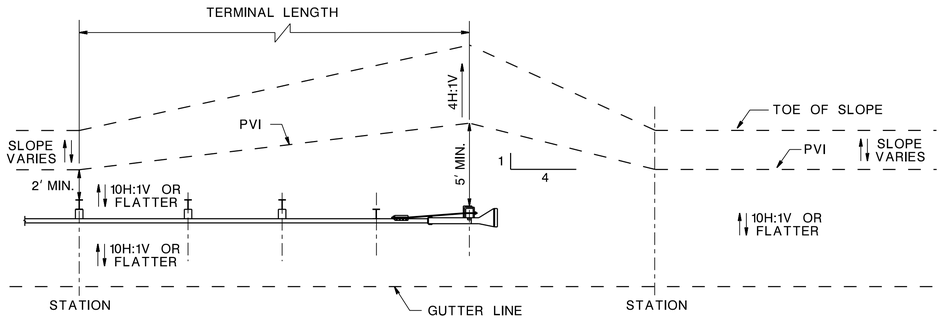
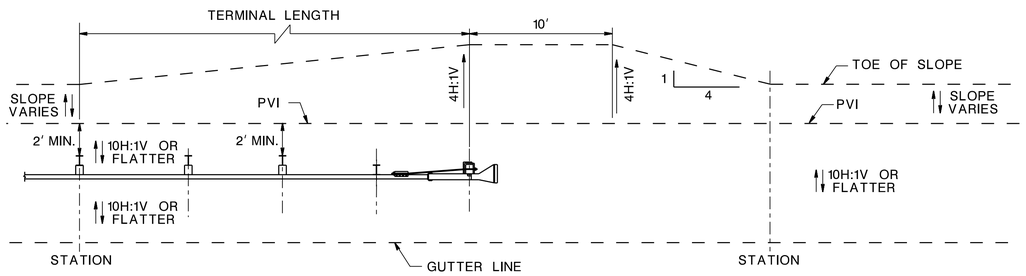


STANDARD GRADING



ALTERNATE GRADING

GRADING	STANDARD/ALTERNATE
STATION TO STATION	
STA. 161+30 TO STA. 161+95	STANDARD
STA. 187+84 TO STA. 188+62	STANDARD
STA. 188+49 TO STA. 187+86	STANDARD
STA. 189+32 TO STA. 189+98	STANDARD
STA. 193+83 TO STA. 193+17	ALTERNATIVE
STA. 205+05 TO STA. 206+02	STANDARD
STA. 210+17 TO STA. 209+23	STANDARD
STA. 209+62 TO STA. 210+32	STANDARD
STA. 212+70 TO STA. 212+10	ALTERNATIVE
STA. 275+60 TO STA. 275+15	STANDARD

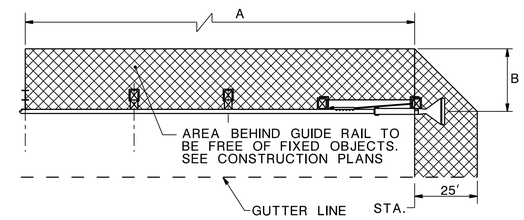
NOTE:

WHERE GUIDE RAIL IS INSTALLED FLUSH WITH THE GUTTER LINE OR OFFSET 6 INCHES FROM THE GUTTER LINE, THE TANGENT GUIDE RAIL TERMINAL IS TO BE CONSTRUCTED WITH A STRAIGHT FLARE FOR ITS ENTIRE LENGTH TO PROVIDE A TWO FOOT OFFSET SO THAT THE EXTRUDER HEAD DOES NOT PROTRUDE INTO THE ROADWAY.

GRADING TREATMENT AT TANGENT GUIDE RAIL TERMINALS

CD-609-10.1

ROADSIDE RECOVERY AREA		
STATION	**A**	**B**
STA. 161+45	65′	7′
STA. 188+12	65′	8′
STA. 188+36	65′	18′
STA. 189+48	65′	11′
STA. 193+67	2′	4′
STA. 205+52	33′	7′
STA. 209+73	65′	6′
STA. 209+82	65′	6′
STA. 212+60	25′	4′
STA. 275+47	65′	10′



NOTE:

NO FIXED OBJECTS IN FRONT OF THE GUIDE RAIL FOR ITS ENTIRE LENGTH ARE PERMITTED.

RECOVERY AREA AT TANGENT GUIDE RAIL TERMINALS

CD-609-10.2

CRASH CUSHION COMPRESSIVE BARRIER SUMMARY TABLE

ITEM NO.	PAY ITEM	DESIGN SPEED	BASELINE AND STATION	PRODUCTS AND FOUNDATION		
611312M	CRASH CUSHION, COMPRESSIVE BARRIER, TYPE 3, WIDTH NARROW	55 MPH	NORTH COUNTY LINE ROAD STA. 164+92	QUADGUARD M10 TL-3 WIDTH 24″ LENGTH 22′ 6″ REINFORCED PCC WITH ANCHOR BLOCK	SCI100GM SMART CUSHION TL-3 WIDTH 24″ LENGTH 21′-6″ 6″ REINFORCED PCC	UNIVERSAL TAU-M TL-3 WIDTH 30″ LENGTH 23′-11″ 6″ REINFORCED PCC
611312M	CRASH CUSHION, COMPRESSIVE BARRIER, TYPE 3, WIDTH NARROW	55 MPH	NORTH COUNTY LINE ROAD STA. 211+45	QUADGUARD M10 TL-3 WIDTH 24″ LENGTH 22′ 6″ REINFORCED PCC WITH ANCHOR BLOCK	SCI100GM SMART CUSHION TL-3 WIDTH 24″ LENGTH 21′-6″ 6″ REINFORCED PCC	UNIVERSAL TAU-M TL-3 WIDTH 30″ LENGTH 23′-11″ 6″ REINFORCED PCC

NOTES:

FOR EACH LOCATION SHOWN IN THE CRASH CUSHION, COMPRESSIVE BARRIER SUMMARY TABLE, INSTALL ONE (1) OF THE PRODUCTS LISTED FOR THAT LOCATION.

THE CONTRACTOR IS RESPONSIBLE FOR INSTALLING THE CRASH CUSHION IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS INCLUDING TRANSITIONS.

A CONCRETE PAD FOUNDATION SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS FOR ALL PERMANENT COMPRESSIVE CRASH CUSHION INSTALLATIONS, INCLUDING AN ANCHOR BLOCK WHERE APPLICABLE.

THE STATION SHOWN IS AN APPROXIMATE LOCATION. SEE PLANS FOR SPECIFIC LOCATION INFORMATION.

CD-611-1.1

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

STATE OF NEW JERSEY — BETH-ANN M. GRASSO — No. GE 43121 — LICENSED PROFESSIONAL ENGINEER

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

FROM JACKSON MILLS RD TO BENNETTS MILLS RD

JACKSON TOWNSHIP

CONSTRUCTION DETAILS

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE NOT TO SCALE	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 73 OF 84

CD15

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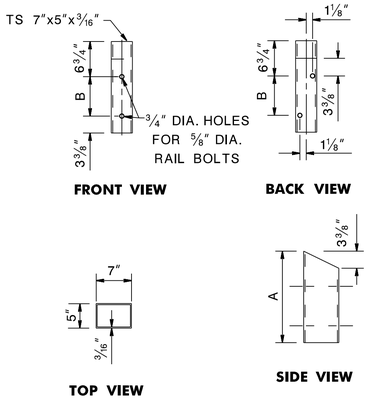


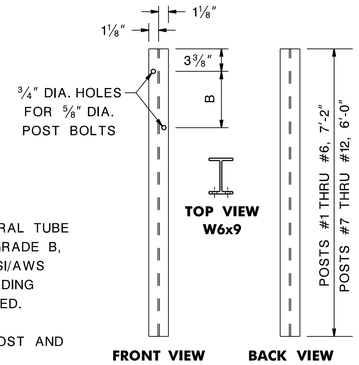
DIM.	POSTS #1 – #11	POST #12
A	1'-5 3/4"	1'-1 7/8"
B	7 5/8"	3 3/4"

BLOCKOUT DIMENSIONS

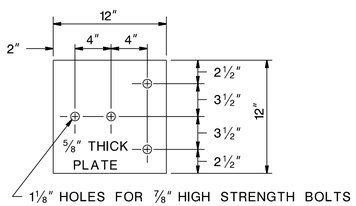
NOTES:

1. STEEL FOR STRUCTURAL TUBE TO BE ASTM A500 GRADE B, WELDED AS PER ANSI/AWS D1.1 STRUCTURAL WELDING CODE, AND GALVANIZED.
2. SEE CD-609-18 FOR POST AND RAIL BOLT DETAILS.

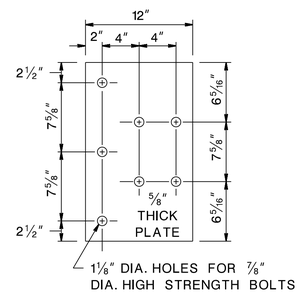
STRUCTURAL TUBE BLOCKOUTS TL–3 ATTACHMENTS (CD–609–14, 15, 16, 17, 17B, 17C)



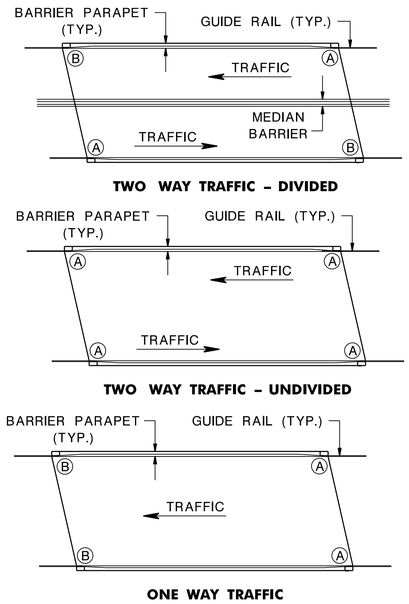
POSTS – TL–3 ATTACHMENTS (CD–609–14, 15, 16, 17, 17B, 17C)



BACKUP PLATE FOR W–BEAM TERMINAL CONNECTOR



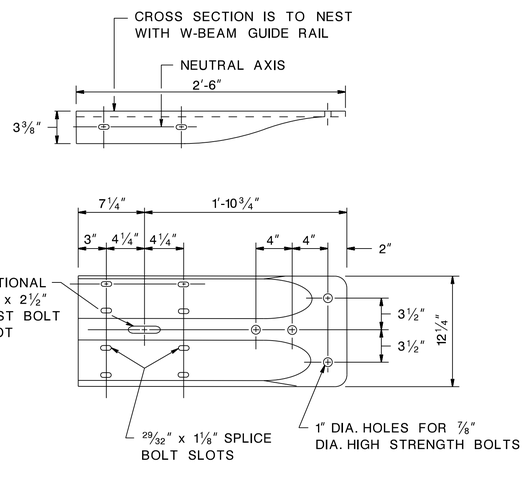
BACKUP PLATE FOR THRIE BEAM TERMINAL CONNECTOR



BRIDGE ATTACHMENT TYPES

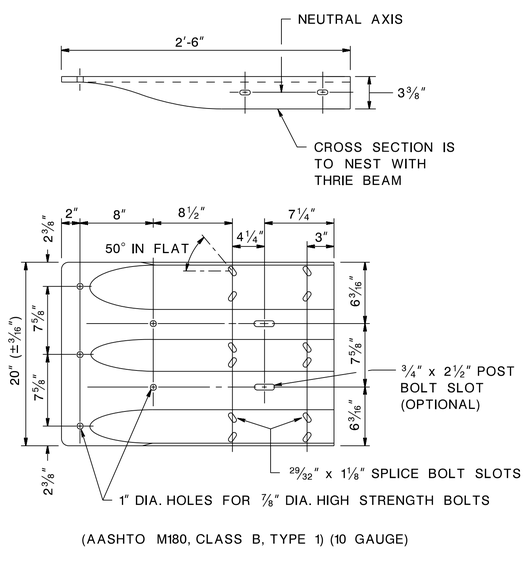
GENERAL NOTES FOR TL–2 & TL–3 ATTACHMENTS (CD–609–14 THRU CD–609–17E)

1. THIS GUIDE RAIL TRANSITION IS APPROPRIATE FOR CONNECTION TO A VERTICAL CONCRETE SHAPE AND SHOULD NOT BE CONNECTED DIRECTLY TO A CONCRETE SAFETY SHAPE. CONCRETE SAFETY BARRIER SHOULD BE TRANSITIONED TO A VERTICAL SHAPE AT THE GUIDE RAIL CONNECTION.
2. FOR RECOMMENDED ATTACHMENT, REFER TO "BRIDGE ATTACHMENT TYPES", THIS SHEET.
3. ALL CROSS SLOPES BETWEEN THE PAVEMENT EDGE AND POSTS TO BE 10H:1V OR FLATTER.
4. EMBANKMENT MATERIAL CONFORMING TO THE NJDOT STANDARD SPECIFICATIONS SECTION 203 TO EXTEND AT A 2% SLOPE FOR A MINIMUM OF 2'-0" BEHIND THE POSTS AT WHICH POINT A SLOPE OF NO STEEPER THAN 2H:1V SHOULD EXTEND A MINIMUM OF 4'-0" FURTHER.
5. LOCATE DRAINAGE INLETS AND ELECTRICAL JUNCTION BOXES ON APPROACHES SO AS NOT TO INTERFERE WITH GUIDE RAIL POST SPACING.
6. STRUCTURAL STEEL PLATES AND SHAPES TO CONFORM TO AASHTO M270 AND BE GALAVANIZED PER AASHTO M111.
7. HIGH STRENGTH STEEL BOLTS, NUTS AND WASHERS TO CONFORM TO AASHTO M164. ZINC COATED BOLTS, NUTS AND WASHERS TO BE TREATED ACCORDING TO AASHTO M232M.
8. THE THICKNESS OF THRIE-BEAM AND W-BEAM RAIL ELEMENTS IS 12-GAUGE UNLESS OTHERWISE NOTED.
9. FOR ADDITIONAL THRIE BEAM AND W-BEAM DETAILS REFER TO CD-609-1 AND CD-609-18.
10. CONCRETE LIP CURB TO BE PAID UNDER 9"X16" CONCRETE VERTICAL CURB (SEE CD-607-1.9).
11. W-BEAM AND THRIE BEAM TERMINAL CONNECTORS USE AASHTO M180 MECHANICAL PROPERTIES FOR BEAM & TRANSITION SECTIONS.



(AASHTO M180, CLASS B, TYPE 1) (10 GAUGE)

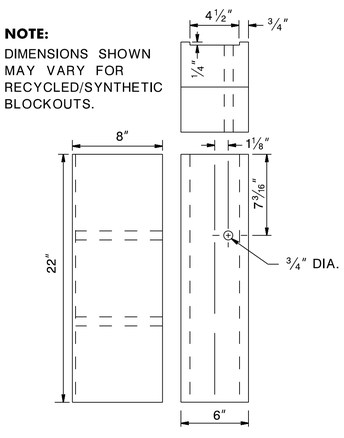
W–BEAM TERMINAL CONNECTOR



(AASHTO M180, CLASS B, TYPE 1) (10 GAUGE)

THRIE BEAM TERMINAL CONNECTOR

NOTE:
DIMENSIONS SHOWN MAY VARY FOR RECYCLED/SYNTHETIC BLOCKOUTS.

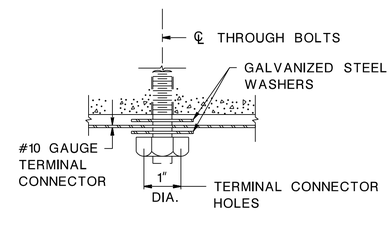


APPROVED RECYCLED/ SYNTHETIC MATERIALS

6"x8"x22" BLOCKOUT TL–2 ATTACHMENTS (CD–609–15A, 15B, 16A, 17A, 17D, 17E)

GENERAL NOTES FOR TL–3 ATTACHMENTS (CD–609–14, 15, 16, 17, 17B & 17C)

12. AT TYPE (A) ATTACHMENTS, THRIE BEAM RAIL ELEMENT WILL REQUIRE ADDITIONAL RAIL BOLT SLOTS FOR POST #1, #3, #5, #7 AND #9. HOLES ARE TO BE SHOP PUNCHED OR DRILLED BEFORE GALVANIZATION. NO FIELD DRILLING IS PERMITTED.
13. POSTS 1 THRU 6 TO BE 7'-2" LONG WITH 4'-10" POST EMBEDMENT. POSTS 7 THRU 12 TO BE 6'-0" LONG WITH 3'-8" POST EMBEDMENT.
14. WHEN THE CONFIGURATION OF BRIDGE ABUTMENTS AND WINGWALLS DO NOT ACCOMMODATE THE INSTALLATION OF POST 1, THE POST MAY BE ATTACHED TO THE ABUTMENT HEADER WITH THE USE OF A BASE PLATE.
15. STRUCTURAL TUBE BLOCKOUTS ARE TO BE USED FOR POSTS 1 THRU 12.



TERMINAL ANCHORAGE

BEAM GUIDE RAIL ATTACHMENTS (MASH TL–2 & TL–3)

N.T.S.

CD-609-13

NEW JERSEY DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS

CD-609-13.1

CD11 / CD15

BDC17D-02-REVISIONS TO CD-609-13
BDC16D-01-ORIGINAL SHEET

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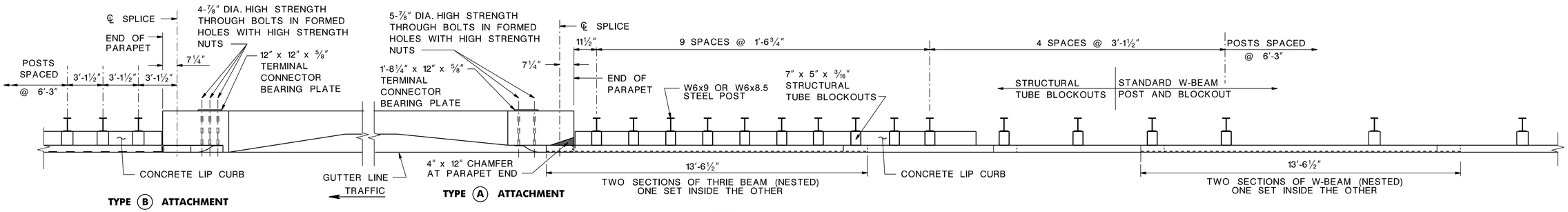
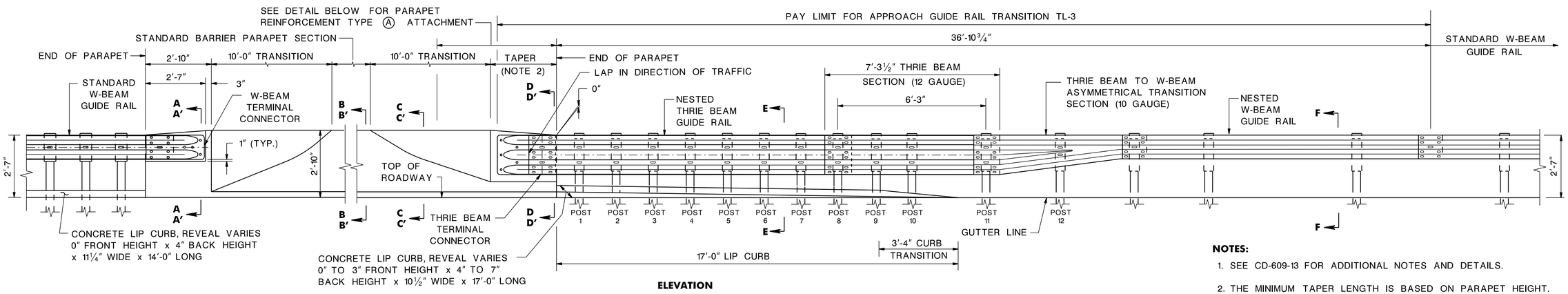


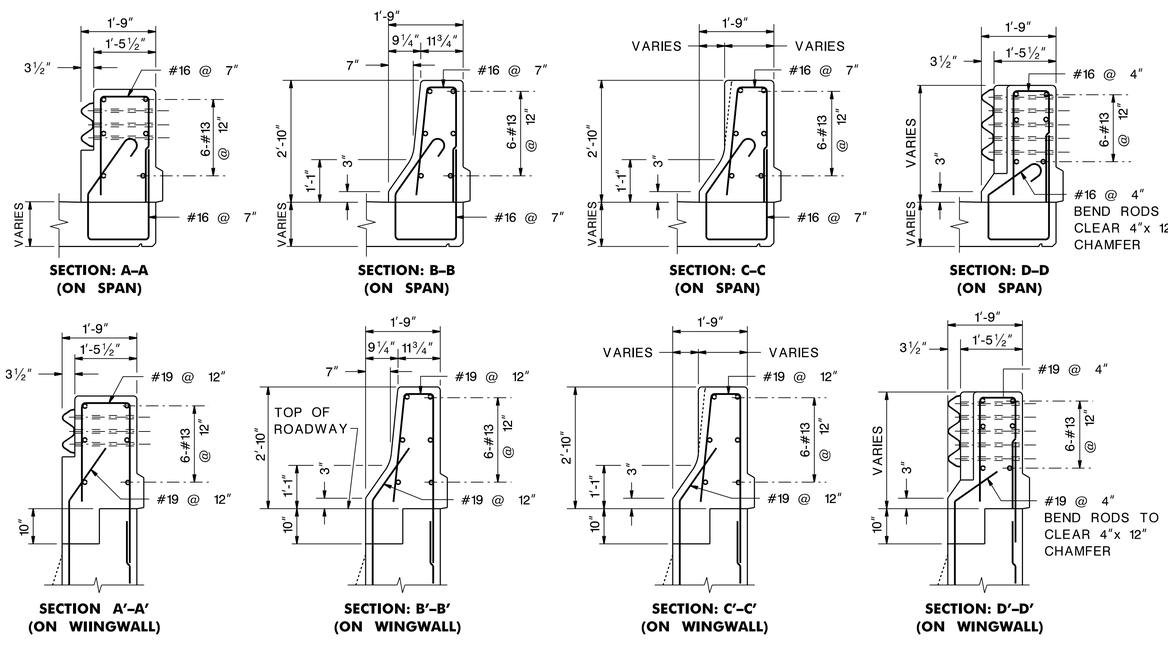
PLAN

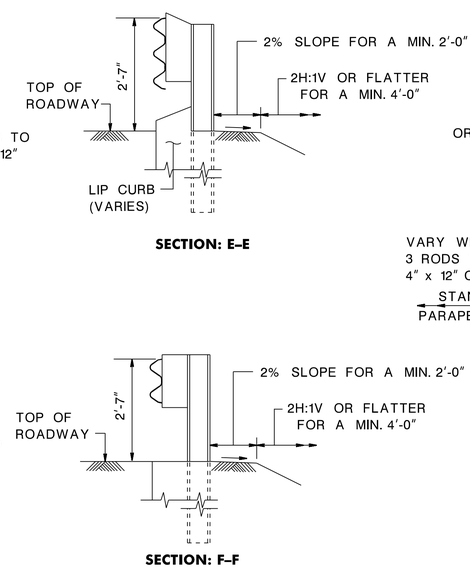


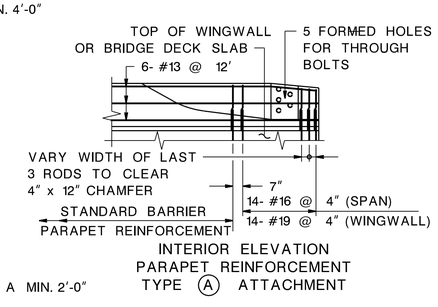
ELEVATION

NOTES:

1. SEE CD-609-13 FOR ADDITIONAL NOTES AND DETAILS.
2. THE MINIMUM TAPER LENGTH IS BASED ON PARAPET HEIGHT. THE TAPER MUST BE 5:1 OR FLATTER WITH 8:1 DESIRABLE. SEE BRIDGE PLANS FOR TAPER LENGTH.
3. REINFORCEMENT STEEL IS IN METRIC UNITS.
4. FOR ADDITIONAL PARAPET DETAILS & DIMENSIONS, SEE BCD-507-3.4.







BEAM GUIDE RAIL ATTACHMENTS

N.T.S.

CD-609-17B

NEW JERSEY DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS

GUIDE RAIL ATTACHMENT – DESIGN SPEED GREATER THAN 45 MPH (MASH TL-3)
EXISTING NJ BARRIER PARAPET (NO ROADWAY CURBING ON APPROACH)

CD-609-17B.1

CD12 / CD15

BDC17D-02-NEW SHEET

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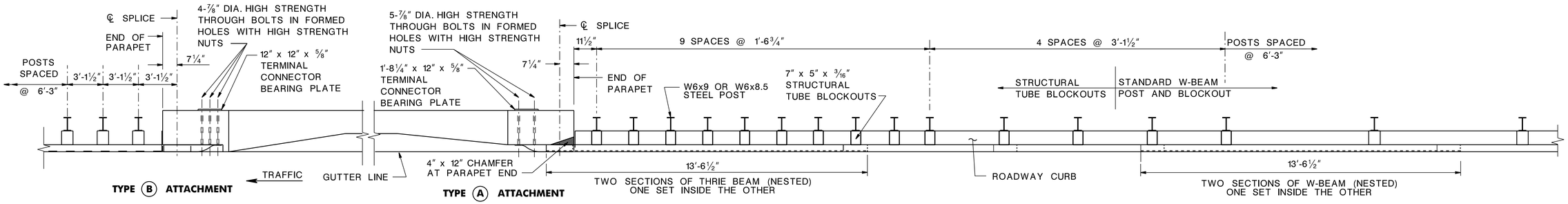
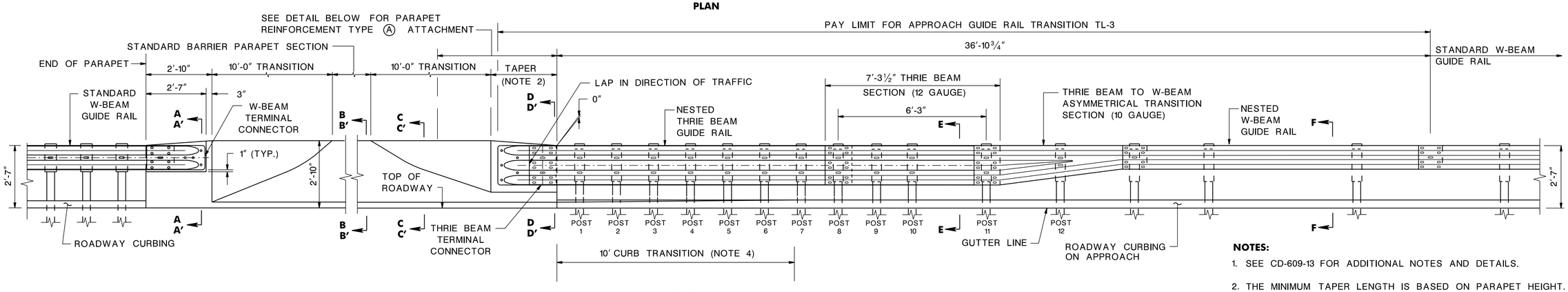
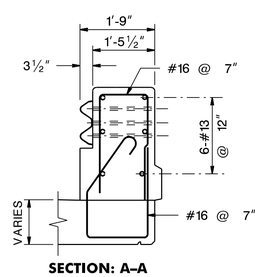


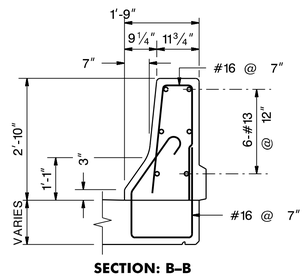
PLAN



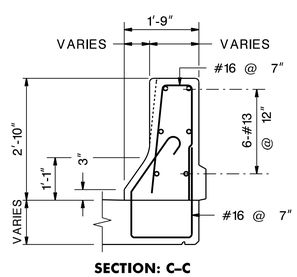
ELEVATION



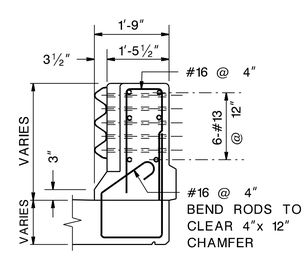
SECTION: A–A (ON SPAN)



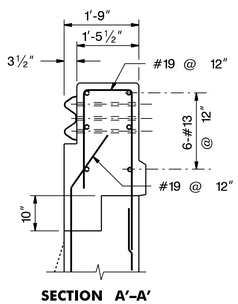
SECTION: B–B (ON SPAN)



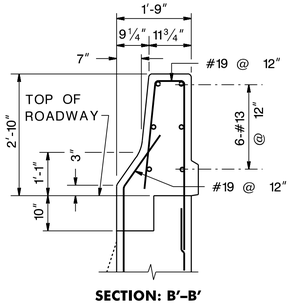
SECTION: C–C (ON SPAN)



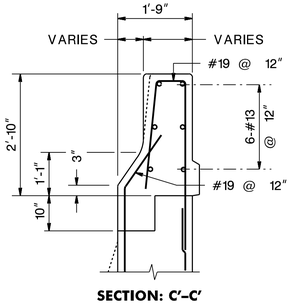
SECTION: D–D (ON SPAN)



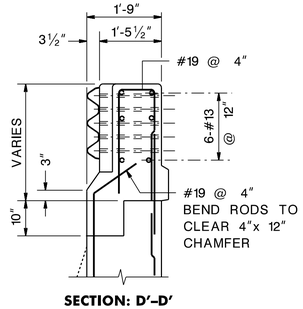
SECTION A'–A' (ON WIINGWALL)



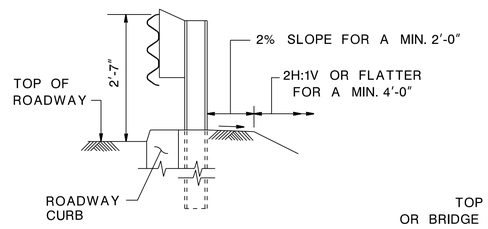
SECTION: B'–B' (ON WINGWALL)



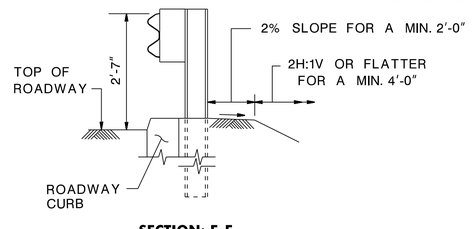
SECTION: C'–C' (ON WINGWALL)



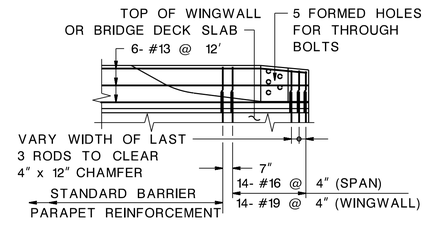
SECTION: D'–D' (ON WINGWALL)



SECTION: E–E



SECTION: F–F



INTERIOR ELEVATION PARAPET REINFORCEMENT TYPE (A) ATTACHMENT

NOTES:

1. SEE CD-609-13 FOR ADDITIONAL NOTES AND DETAILS.
2. THE MINIMUM TAPER LENGTH IS BASED ON PARAPET HEIGHT. THE TAPER MUST BE 5:1 OR FLATTER WITH 8:1 DESIRABLE. SEE BRIDGE PLANS FOR TAPER LENGTH.
3. REINFORCEMENT STEEL IS IN METRIC UNITS.
4. TRANSITION LAST 10 FEET OF ROADWAY CURBING TO MATCH BARRIER PARAPET SHAPE.
5. FOR ADDITIONAL PARAPET DETAILS & DIMENSIONS, SEE BCD-507-3.4.

BEAM GUIDE RAIL ATTACHMENTS

N.T.S.

CD-609-17C

NEW JERSEY DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS

GUIDE RAIL ATTACHMENT – DESIGN SPEED GREATER THAN 45 MPH (MASH TL-3)
EXISTING NJ BARRIER PARAPET (WITH ROADWAY CURBING ON APPROACH)

CD-609-17C.1

CD13 / CD15

BDC17D-02-NEW SHEET

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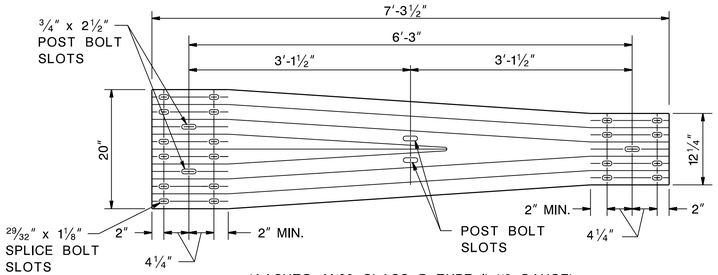
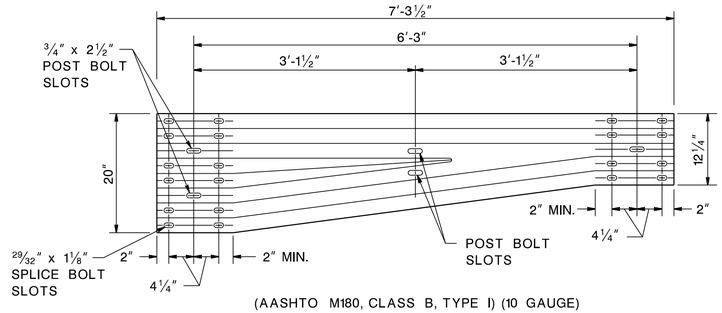


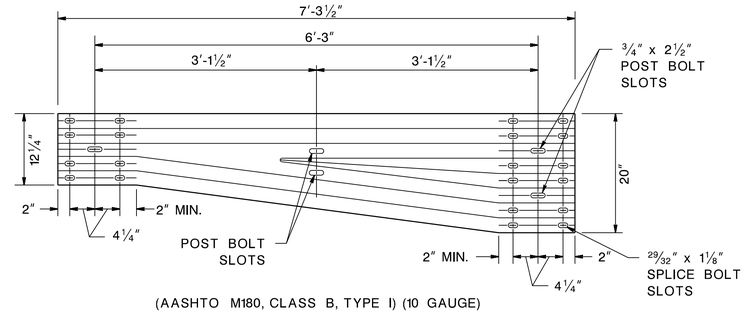
(AASHTO M180, CLASS B, TYPE I) (10 GAUGE)

THRIE BEAM TO W-BEAM SYMMETRICAL TRANSITION SECTION – SEE NOTE 1



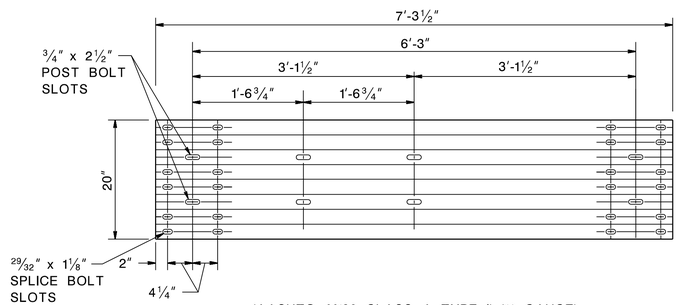
(AASHTO M180, CLASS B, TYPE I) (10 GAUGE)

THRIE BEAM TO W-BEAM ASYMMETRICAL TRANSITION SECTION – RIGHT SIDE APPROACH – SEE NOTE 2



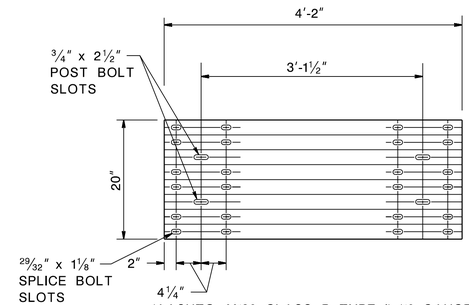
(AASHTO M180, CLASS B, TYPE I) (10 GAUGE)

THRIE BEAM TO W-BEAM ASYMMETRICAL TRANSITION SECTION – LEFT SIDE APPROACH – SEE NOTE 2



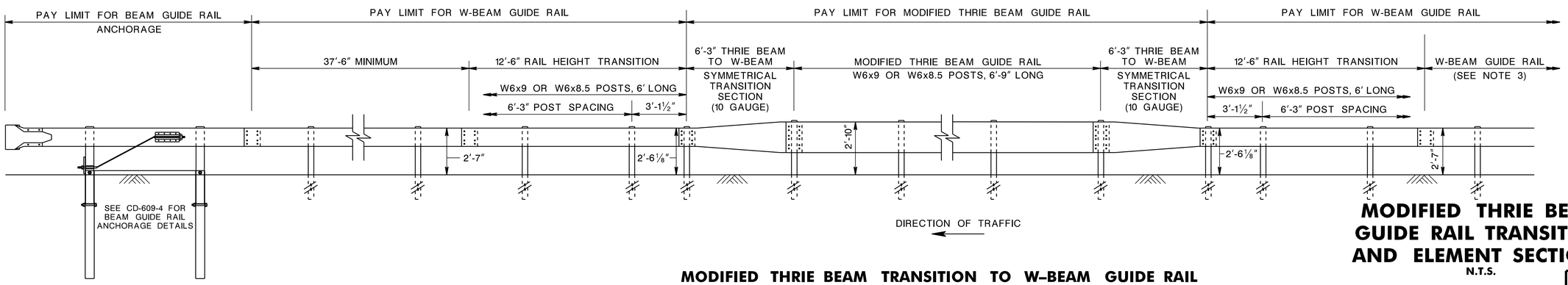
(AASHTO M180, CLASS A, TYPE I) (12 GAUGE)

7′–3½″ THRIE BEAM SECTION FOR TL–3 BRIDGE ATTACHMENTS



(AASHTO M180, CLASS B, TYPE I) (10 GAUGE)

4′–2″ THRIE BEAM SECTION FOR TL–2 BRIDGE ATTACHMENTS



MODIFIED THRIE BEAM TRANSITION TO W–BEAM GUIDE RAIL

MODIFIED THRIE BEAM GUIDE RAIL TRANSITION AND ELEMENT SECTIONS

N.T.S.

CD-609-20

NOTES:

1. A THRIE BEAM TO W-BEAM SYMMETRICAL TRANSITION SECTION IS USED WHERE A VERTICAL TRANSITION IS REQUIRED, SUCH AS A TRANSITION FROM MODIFIED THRIE BEAM TO W-BEAM GUIDE RAIL.
2. A THRIE BEAM TO W-BEAM ASYMMETRICAL TRANSITION SECTION IS USED WHERE A VERTICAL TRANSITION IS NOT REQUIRED, SUCH AS A TRANSITION FROM THRIE BEAM AT A BRIDGE ATTACHMENT TO W-BEAM GUIDE RAIL.
3. A MINIMUM 12′-6″ LENGTH OF BEAM GUIDE RAIL IS REQUIRED BETWEEN THE 12′-6″ RAIL HEIGHT TRANSITION AND A TANGENT GUIDE RAIL TERMINAL. A MINIMUM 25′ LENGTH OF BEAM GUIDE RAIL IS REQUIRED BETWEEN THE 12′-6″ RAIL HEIGHT TRANSITION AND A STRAIGHT FLARE OF BEAM GUIDE RAIL.

NEW JERSEY DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS

CD-609-20.1

CD14 / CD15

BDC23D-01-REVISED CD-609-18 THRU 20
BDC18D-07-NOTE 3 REVISED
BDC17D-02-NEW SHEET

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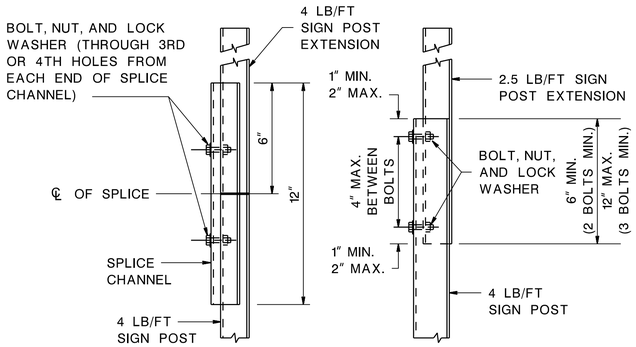
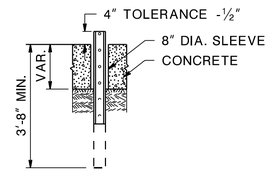
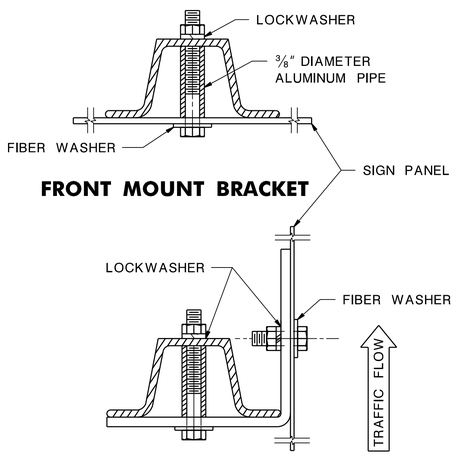


SIGN POST EXTENSION SPLICE DETAILS

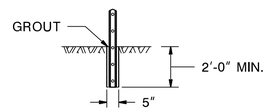


CONCRETE INSTALLATION

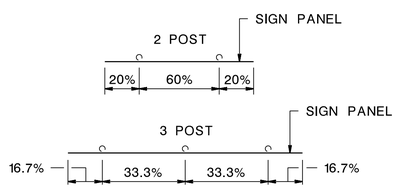


FRONT MOUNT BRACKET

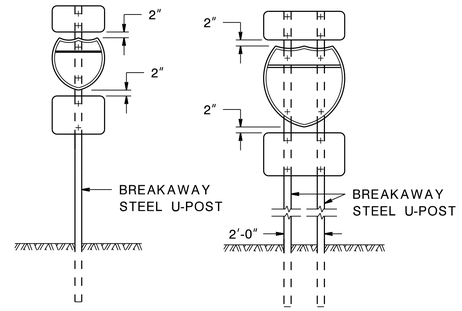
SIDE MOUNT BRACKET



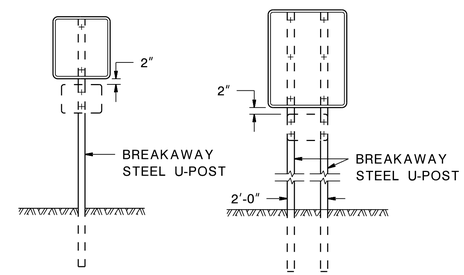
ROCK INSTALLATION



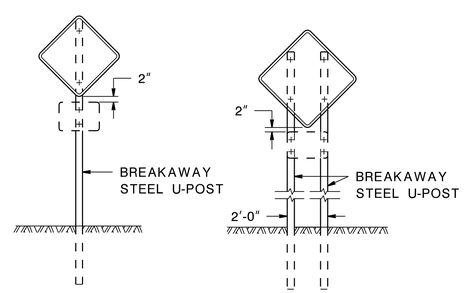
STEEL U-POST SPACING



24" x 24" SHIELD **36" x 36" SHIELD**



30" x 30" OR SMALLER **36" x 36" OR LARGER**



30" x 30" OR SMALLER **36" x 36" OR LARGER**

GENERAL NOTES:

1. ALL POSTS TO BE OF ADEQUATE LENGTH TO MEET THE REQUIREMENTS FOR ERECTION AS STATED IN THE CURRENT "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" AND AS INDICATED BELOW.
2. ALL SMALL SIGN SUPPORTS TO BE OF THE BREAKAWAY TYPE WITH EXCEPTION OF THOSE INSTALLED BEHIND GUIDE RAIL OR OTHER ROADSIDE BARRIER.
3. ALL STEEL POSTS AND BRACKETS TO BE CUT, BENT, AND HOLES PUNCHED AND DRILLED BEFORE GALVANIZING. GALVINIZING TO BE ACCORDING TO ASTM A123.
4. ALL STEEL U-POST SIGN SUPPORTS MUST BE INSTALLED FACING THE PREDOMINANT TRAFFIC FLOW. USE A MOUNTING BRACKET ON SIDE MOUNTED SIGNS SUCH AS "ONE WAY" SIGNS INSTALLED IN MEDIANS.
5. SIGN PANEL SIZES ARE TO DETERMINE POST TYPE AND NUMBER AS SHOWN ON THIS DETAIL.
6. BOLTS ARE NOT TO PROTRUDE MORE THAN ¾" BEYOND THE NUT WHEN TIGHT, BUT ARE TO ENGAGE ALL THREADS IN THE NUT.
7. WHEN SIGNS ARE INSTALLED ON SLOPES 10H:1V OR FLATTER, THE MINIMUM VERTICAL CLEARANCE REQUIREMENTS FOR SIGNS ARE:

   FOR SINGLE POST INSTALLATIONS - THE MINIMUM DISTANCE BETWEEN THE EDGE OF THE PAVEMENT AND THE BOTTOM OF ANY PANEL MUST BE 7 FEET, AND THE MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO THE TOP OF ANY SIGN PANEL MUST BE 9 FEET.

   FOR MULTI-POST INSTALLATIONS - THE MINIMUM DISTANCE BETWEEN THE EDGE OF PAVEMENT AND THE BOTTOM OF A MAIN SIGN PANEL MUST BE 7 FEET.

   SECONDARY SIGN PANELS (LAND SERVICE HIGHWAYS) - THE MINIMUM DISTANCE BETWEEN THE EDGE OF PAVEMENT AND THE BOTTOM OF A SECONDARY SIGN PANEL IS 6 FEET.

   SECONDARY SIGN PANELS (INTERSTATE AND FREEWAYS) - THE BOTTOM OF THE MAIN SIGN TO BE A MINIMUM OF 8 FEET AND THE SECONDARY SIGN PANEL A MINIMUM OF 5 FEET ABOVE THE EDGE OF PAVEMENT.

   WHERE GRADING OF 10H:1V OR FLATTER CANNOT BE OBTAINED, OR WHERE CURB OR BERM IS GREATER THAN 4 INCHES, THE MINIMUM VERTICAL CLEARANCE WILL BE MEASURED FROM THE GROUND LINE TO THE BOTTOM OF THE SIGN.
8. THE HORIZONTAL OFFSET FROM EDGE OF PAVEMENT TO EDGE OF SIGN IS DERIVED FROM SECTION 2A.19 OF THE MUTCD AS FOLLOWS:

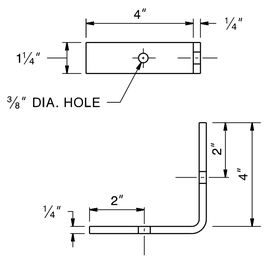
   FOR URBAN INSTALLATIONS - IN AREAS WHERE LATERAL OFFSETS ARE LIMITED, A MINIMUM LATERAL OFFSET OF 2 FEET IS DESIRABLE. A MINIMUM OFFSET OF 1 FOOT FROM THE FACE OF THE CURB MAY BE USED IN AREAS WHERE THE SIDEWALK WIDTH IS LIMITED OR WHERE EXISTING POLES ARE CLOSE TO THE CURB.

   FOR RURAL INSTALLATIONS - 6 FEET MINIMUM DESIRABLE FROM EDGE OF SHOULDER, BUT 12 FEET MINIMUM DESIRABLE FROM EDGE OF TRAFFIC OR AUXILIARY LANE.

   FOR INTERSTATE AND FREEWAY INSTALLATIONS - 6 FEET MINIMUM DESIRABLE FROM EDGE OF SHOULDER, BUT NOT LESS THAN 12 FEET FROM THE EDGE OF TRAFFIC OR AUXILIARY LANE.

   FOR RAMP INSTALLATIONS - 6 FEET MINIMUM FROM EDGE OF ROAD.

   WHERE BEHIND GUIDE RAIL - 4 FEET MINIMUM FROM BACK OF BEAM GUIDE RAIL ELEMENT TO SIGN POST.
9. DO NOT INSTALL PERMANENT SIGN SUPPORTS ON SLOPES GREATER THAN 10H:1V, EXCEPT WHERE GRADING OF 10H:1V CANNOT BE OBTAINED OR THE SIGN SUPPORTS WILL BE BEHIND A TRAFFIC BARRIER. THE SLOPE IS TO EXTEND A MINIMUM OF 3 FEET BEYOND THE OUTSIDE EDGE OF SIGN (SEE GRADING DETAIL FOR SLOPE TREATMENT).
10. EXTRUDED ALUMINUM SIGN PANELS ARE NOT PERMITTED FOR USE WITH STEEL U-POST SIGN SUPPORTS.
11. DO NOT PLACE STEEL U-POST SIGN SUPPORTS IN FRONT OF GUIDE RAIL AND THE POSTS MUST NOT STRADDLE GUIDE RAIL.
12. TO EXTEND THE HEIGHT OF A SIGN POST, A MAXIMUM OF ONE SPLICE MAY BE MADE AND MUST BE A MINIMUM OF 9 FEET FROM THE GROUNDLINE TO CENTER LINE OF SPLICE.



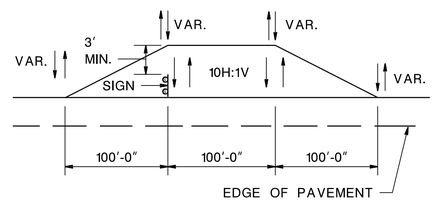
DETAIL OF BRACKET FOR SIDE MOUNTED SIGNS

PANEL SIZE (W x H)	# OF POSTS	POST SIZE (LB/FT)
18" x 18"	1	2.5
18" x 24"	1	2.5
24" x 24"	1	2.5
24" x 30"	1	2.5
24" x 36"	1	2.5
30" x 24"	1	2.5
30" x 30"	1	2.5
36" x 12"	2	2.5
36" x 36" x 36"	2	2.5
30" x 36"	1	4.0

PANEL SIZE (W x H)	# OF POSTS	POST SIZE (LB/FT)
36" x 36"	2	2.5
36" x 48"	2	2.5
45" x 36"	2	2.5
48" x 24"	2	2.5
48" x 36"	2	2.5
48" x 48"	2	4.0
48" x 64" x 64"	2	2.5
60" x 36"	2	4.0
48" x 60"	2	4.0
60" x 30"	2	4.0

U-POST SELECTION TABLE

BREAKAWAY SIGN SUPPORT



STEEL U-POST GRADING DETAIL

STEEL U-POST SIGN SUPPORTS

N.T.S.

CD-612-4

NEW JERSEY DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS

CD-612-4.1

CD11 / CD15

BD-C16D-01-ORIGINAL SHEET

GENERAL NOTES

1. CHEEK WALLS

   - CHEEK WALLS ARE PERMITTED WHEN ADJACENT TO NON-WALK AREAS OR ELEVATION DIFFERENCES CANNOT BE ACCOMMODATED BY FLARES OR GRADING.

2. CROSSWALKS

   - FOR CURB RAMPS THAT LEAD TO A SINGLE CROSSWALK, THE RAMP (EXCLUDING FLARES) TO BE FULLY INSIDE OF MARKED CROSSWALK LINES.
   - SHOULD BE PLACED A MINIMUM DISTANCE OF 4'-0" FROM STOP AND YIELD LINES.
   - FOR UN-SIGNALIZED AREAS, CROSSWALKS SHOULD BE PLACED A MINIMUM DISTANCE OF 20'-0" AWAY FROM ON ROAD PARKING ZONES. FOR SIGNALIZED AREAS, CROSSWALKS SHOULD BE PLACED A MINIMUM DISTANCE OF 30'-0" FROM ON ROAD PARKING ZONES.
   - PEDESTRIAN CROSSWALK IS 6'-0" MINIMUM.
   - USE LONGITUDINAL LINES AT 6'-0" LONG AND 2' WIDE WITH A SPACING OF 2' APART. SPACING SHOULD BE DESIGNED SO THE PAINTED AREAS AVOID THE WHEEL PATHS.

3. CURB RAMPS

   - CONSTRUCT CURB RAMPS WITH A MINIMUM 4'-0" X 4'-0" CLEAR SPACE BEFORE THE CURB FACE, WITHIN THE WIDTH OF THE CROSSWALK.
   - SLOPES THAT EXCEED 8.00%, OR CONTRACT DOCUMENTS AS APPLICABLE, WILL NOT BE ACCEPTED AND WILL BE RECONSTRUCTED.
   - PROVIDE SLIP RESISTANT TEXTURE ON CURB RAMP BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP. EXTEND TEXTURE THE FULL WIDTH AND LENGTH OF THE CURB RAMP INCLUDING FLARED SIDE RAMPS.
   - DO NOT EXCEED 2.00% CROSS SLOPE ON THE CURB RAMP OR PEDESTRIAN ACCESSIBLE ROUTE (MEASURED PERPENDICULAR TO THE DIRECTION OF TRAVEL).
   - CURB RAMP WIDTH IS 4'-0" MINIMUM.
   - ALL SLOPES ARE MEASURED WITH RESPECT TO A LEVEL PLANE. THEREFORE, THE LENGTH OF RAMP IS NOT SOLELY DEPENDANT ON THE HEIGHT OF CURB. (FOR EXAMPLE, A 6" CURB DOES NOT NECESSARILY MEAN A RAMP LENGTH OF 6'-0" FOR A 12:1 SLOPE.
   - THE CHANGE IN GRADE AT THE BOTTOM OF THE CURB RAMP AND ADJOINING ROAD SURFACE IS NOT TO EXCEED AN ALGEBRAIC DIFFERENCE OF 11.00%. THE COUNTER SLOPE OF THE GUTTER OR ROAD AT THE FOOT OF A CURB RAMP, TURNING SPACE OR BLENDED TRANSITION IS NOT TO EXCEED 8.00% AND IT IS NOT NECESSARY TO HAVE THE LENGTH GREATER THAN 15'-0".
   - FOR BLENDED TRANSITION CURB RAMPS (TYPE 3), THE MAXIMUM RUNNING SLOPE IS 5% WITH A MAXIMUM 2% CROSS SLOPE.

4. FLUSH CURB

   - CONSTRUCT TOP OF PLAIN CEMENT CONCRETE FLUSH CURB TO BE FLUSH WITH ADJACENT SURFACES (RAMPS, SIDEWALKS, FLARES).
   - CONSTRUCT FLUSH CURB FOR CURB RAMPS FLUSH TO ADJACENT ROADWAY. GRADE EDGE OF ROAD ELEVATIONS AT THE FLOW LINE TO ENSURE POSITIVE DRAINAGE AND PREVENT PONDING. THE VERTICAL ALIGNMENT OF A CURB RAMP, EXCLUDING FLARES, SHALL BE PLANAR. GRADE BREAKS SHALL BE FLUSH AND PERPENDICULAR TO THE DIRECTION OF THE RAMP RUN. RAMP TRANSITIONS BETWEEN WALKS, GUTTERS, TURNING SPACES, OR STREETS SHALL BE FLUSH AND FREE OF ABRUPT VERTICAL CHANGES (1/4" MAXIMUM).
   - FOR TYPE 3 RAMPS AND BLENDED TRANSITIONS, THE FLUSH CURB MUST EXTEND FROM THE OUTER MOST EDGE OF EACH CROSSWALK.

5. DETECTABLE WARNING SURFACES

   - NO SEPARATION BETWEEN DETECTABLE WARNING SURFACES FOR MEDIANS LESS THAN 4'-0" BETWEEN BACK OF CURBS.
   - PROVIDE DETECTABLE WARNING SURFACES (DWS) 24" MINIMUM (IN THE DIRECTION OF PEDESTRIAN TRAVEL) ACROSS FULL WIDTH OF RAMP AT THE GRADE BREAK NEAR STREET EDGE. PROVIDE DWS THAT CONTRAST VISUALLY WITH ADJACENT WALKWAY SURFACES, EITHER LIGHT-ON-DARK OR DARK-ON-LIGHT FOR THE FULL WIDTH OF RAMP.
   - ALIGN DETECTABLE WARNING SURFACE TRUNCATED DOMES ON A SQUARE GRID IN THE PREDOMINANT DIRECTION OF THE RAMP AND PERPENDICULAR TO CURB WHEN APPROPRIATE.
   - THE PUBLIC SIDEWALK CURB RAMP, DETECTABLE WARNING SURFACES (SHADED AREA) SHALL BE SAFETY RED COLOR, EXCEPT IF THE MUNICIPALITIES HAVE ESTABLISHED AN ALTERNATIVE COLOR SCHEME.
   - FOR TYPE 3 RAMPS AND BLENDED TRANSITIONS, THE DETECTABLE WARNING SURFACE MUST BE PLACED ALONG THE ENTIRE FLUSH CURB AND THE DOMES MUST BE PLACED IN SUCH A WAY THAT THE DIRECTION OF TRAVEL IS ORIENTED INTO THE CROSSWALK.

6. DRIVEWAYS

   - 6.00 % MAXIMUM SLOPE FOR THE DRIVEWAY APRON.
   - 1 1/2" MAXIMUM VERTICAL CHANGE IN HEIGHT BETWEEN THE ROAD SURFACE AND THE DEPRESSED CURB AT THE DRIVEWAY APRON.

7. JOINTS

   - PROVIDE EXPANSION JOINT MATERIAL 1/2" THICK WHERE CURB RAMP ADJOINS ANY RIGID PAVEMENT, SIDEWALK OR STRUCTURE WITH THE TOP OF JOINT FILLER FLUSH WITH ADJACENT CONCRETE SURFACE.
   - SEAL JOINTS WITH AN APPROVED SEALING MATERIAL.

8. TURNING SPACES

   - TURNING SPACE, APPROACH SIDEWALK TRANSITIONS, AND CURB RAMP SHALL BE KEPT CLEAR OF OBSTRUCTIONS, UNLESS AN EXCEPTION IS GRANTED.
   - DO NOT EXCEED 2.00% FOR RUNNING AND CROSS SLOPES.
   - TURNING SPACE SHOULD BE 4'-0" X 4'-0" MINIMUM CLEAR SPACE. IF THE TURNING SPACE IS CONSTRAINED ON 2 OR MORE SIDES, IT MUST BE 4'x5' WITH THE 5' LENGTH ALONG THE UNOBSTRUCTED SIDE. FOR TYPE 3 RAMPS AND BLENDED TRANSITIONS, THE TURNING SPACE MUST HAVE A MINIMUM OF A 5'-0" DEPTH (INCLUDING THE DETECTABLE WARNING SURFACE) ALONG THE FLUSH CURB/ROADWAY.
   - 4'x4' TURNING SPACES ARE REQUIRED AT EVERY ACCESSIBLE PEDESTRIAN SIGNAL/PUSH BUTTON LOCATION.
   - TURNING SPACES SHALL BE CONSTRUCTED AT THE TOP OF EVERY PERPENDICULAR RAMP AND AT THE BOTTOM OF EVERY PARALLEL RAMP.

9. NON-WALK SURFACES

   - NON-WALK AREA IS AN OBSTRUCTION OR GRASS/NON-PAVED AREA ADJACENT TO THE PEDESTRIAN ACCESS ROUTE THAT IS NOT USED BY THE PEDESTRIAN FOR ACCESS.

10. PEDESTRIAN PUSH BUTTONS

    - THE DETAILS DEPICT PEDESTRIAN PUSH BUTTON POLES TO ILLUSTRATE THE RECOMMENDED PLACEMENT OF PEDESTRIAN PUSH BUTTONS. FOR ALTERATION PROJECTS, PROVIDE ACCESS TO EXISTING PEDESTRIAN PUSH BUTTONS TO THE MAXIMUM EXTENT FEASIBLE. INSTALL PEDESTRIAN PUSH BUTTON STUB POLES, WHERE APPLICABLE, SO AS NOT TO CREATE PEDESTRIAN OBSTRUCTIONS.
    - PUSH BUTTON LOCATION SHALL BE COMPLIANT WITH THE CURRENT MUTCD.
    - ADJACENT TO A LEVEL NON-SLIP SURFACE TO PROVIDE ACCESS FROM A WHEELCHAIR, AND WHERE THERE IS A NON-SLIP WHEELCHAIR ACCESSIBLE ROUTE TO THE RAMP
    - WITHIN 5'-0" OF THE CROSSWALK EXTENDED
    - BETWEEN 1'-6" AND 10'-0" OF THE EDGE OF CURB, SHOULDER OR PAVEMENT
    - PARALLEL TO THE CROSSWALK TO BE USED
    - MOUNT PEDESTRIAN PUSH BUTTON 42" ABOVE THE SIDEWALK OR FINISHED GRADE TO THE CENTER OF THE PUSH BUTTON AND 10" MAXIMUM LATERALLY FROM TURNING SPACE. IN ADDITION, THE PEDESTRIAN PUSH BUTTON MUST ALSO BE 5'-0" MINIMUM DISTANCE FROM THE CURB RAMP AND 2"-0" MINIMUM DISTANCE BETWEEN A CATCH BASIN AND THE PEDESTRIAN PUSH BUTTON.

11. SIDE FLARES

    - ALL SLOPES ARE MEASURED WITH RESPECT TO A LEVEL PLANE. THEREFORE, THE LENGTH OF RAMP IS NOT SOLELY DEPENDANT ON THE HEIGHT OF CURB. (FOR EXAMPLE, A 6" CURB DOES NOT NECESSARILY MEAN A RAMP LENGTH OF 6'-0" FOR A 12:1 SLOPE.
    - SIDE FLARES 10.00% MAXIMUM SLOPE WHERE THE PEDESTRIAN PATH CROSSES THE CURB RAMP.
    - SIDE FLARES MUST BE PARALLEL TO THE CURB LINE.
    - GRADE GRASS AREAS OR OTHER NON-WALK AREAS AT 3:1 (1:3) MAXIMUM. DO NOT INSTALL CHEEK WALLS THAT INTERSECT THE PEDESTRIAN ACCESS ROUTE.
    - SIDE FLARE WIDTH IS TYPICALLY 24" AND A MINIMUM OF 12".

12. SIDEWALKS

    - NOTES THE AREA CONSIDERED TO BE THE "PEDESTRIAN ACCESSIBLE ROUTE"
    - THE MAXIMUM SIDEWALK CROSS SLOPE IS 2.00% (MEASURED PERPENDICULAR TO THE DIRECTION OF TRAVEL). THE MAXIMUM GRADE IS 5.00%. FOR SIDEWALKS ALONG STREETS; HOWEVER, THE LONGITUDINAL GRADE OF THE SIDEWALK SHOULD BE CONSISTENT WITH THE GRADE OF THE ADJACENT ROADWAY. IF THE 5.00% GRADE IS NOT FEASIBLE DUE TO TOPOGRAPHY AND OTHER PHYSICAL CONSTRAINTS, THE LOWEST PRACTICAL GRADE GREATER THAN 5.00% SHOULD BE USED.
    - SIDEWALK WIDTH MAY BE REDUCED TO 4'-0", WHEN PASSING AREAS 5'-0" X 5'-0" ARE PROVIDED EVERY 200'.

13. TRAVEL LANES

    - THE TRAVEL LANE IS DEFINED BY THE OUTSIDE EDGE OF THE WHITE PAVEMENT MARKING LINE. IF A WHITE PAVEMENT MARKING LINE DOES NOT EXIST, THE TRAVEL LANE IS DEFINED BY THE CONTRACT DOCUMENTS.

14. CONSTRUCTION MUST MEET THE STANDARDS CONTAINED HEREIN UNLESS OTHERWISE NOTED OR DIRECTED.

15. ALL HANDICAP RAMPS CONSTRUCTED IN THIS CONTRACT SHALL MEET ACCESSIBILITY REQUIREMENTS OF THE AMERICANS WITH DISABILITIES ACT AND THE PUBLIC RIGHTS-OF-WAY ACCESSIBILITY GUIDELINES.

16. GRADE BREAKS

    - GRADE BREAKS AT THE TOP AND BOTTOM OF THE CURB RAMP SHALL BE PERPENDICULAR TO THE DIRECTION OF THE RAMP RUN.
    - GRADE BREAKS ARE NOT PERMITTED ON THE SURFACE OF RAMP RUNS OR TURNING SPACES.
    - SURFACE SLOPES THAT MEET AT THE GRADE BREAKS SHALL BE FLUSH.

17. ALL VERTICAL SURFACE DISCONTINUITIES SHALL NOT EXCEED ¼" IN HEIGHT. ANY VERTICAL SURFACE DISCONTINUITY BETWEEN ¼" AND ½" SHALL BE BEVELED AT A SLOPE NO GREATER THAN 50% ACROSS THE ENTIRE DISCONTINUITY.

18. HORIZONTAL OPENINGS IN GRATES AND JOINTS SHALL NOT EXCEED ½" IN DIAMETER AND THE GRATES SHALL BE PLACED SO THE LONG DIMENSION IS PERPENDICULAR TO THE DIRECTION OF TRAVEL.

19. THE CROSS SLOPE FOR CURB RAMPS, BLENDED TRANSITIONS, AND TURNING SPACES SHALL BE 2% MAXIMUM. AT PEDESTRIAN STREET CROSSINGS WITHOUT YEILD OR STOP CONTROL, THE CROSS SLOPE SHALL BE PERMITTED TO MATCH THE STREET GRADE. PEDESTRIAN STREET CROSSINGS WITHOUT YEILD OR STOP CONTROL ARE CROSSINGS WHERE THERE IS NO YEILD OR STOP SIGN, OR WHERE THERE IS A TRAFFIC SIGNAL THAT IS DESIGNED FOR THE GREEN PHASE.

DETECTABLE WARNING SURFACE (DWS) INSTALLATION INSTRUCTIONS

1. LAY OUT AND PROPERLY FIT EACH UNIT PRIOR TO SETTING IN WET CONCRETE.
2. CUT UNITS AS NECESSARY ALONG PERIMETER OF DETECTABLE WARNING SURFACE.
3. PLACE UNITS ACROSS THE ENTIRE WIDTH OF THE CURB RAMP SURFACE AND/OR WHERE THE CURB IS FLUSH.
4. PRESS UNITS INTO FULL CONTACT WITH THE FRESH CONCRETE.
5. ADJUST HEIGHT OF EACH UNIT EDGE TO BE LEVEL WITH ADJACENT RAMP SURFACES.
6. ONLY TRUNCATED DOMES SHOULD BE ABOVE THE ADJACENT FINISHED CONCRETE.
7. FILL ANY SAW CUT GAPS WITH APPROVED JOINT SEALANT MATERIAL.

DWS NOTES:

- THE DETAILS PROVIDED ARE NOT DRAWN TO SCALE. THE QUANTITY OF DOMES DEPICTED ON THE DETECTABLE WARNING UNIT (THE DOMES AND THE ENTIRE 24" LEVEL SURFACE) IS FOR ILLUSTRATION ONLY.

DETECTABLE WARNING UNIT DIMENSIONS:

- THE SIZE OF THE DETECTABLE WARNING FIELD SHALL BE 24" IN THE DIRECTION OF TRAVEL AND SHALL EXTEND THE FULL WIDTH OF THE CURB RAMP OR FLUSH SURFACE, EXCLUSIVE OF SIDE FLARES.

DOME ALIGNMENT:

- THE ROWS OF DOMES SHALL BE ALIGNED TO BE PERPENDICULAR OR RADIAL TO THE GRADE BREAK BETWEEN THE RAMP TURNING SPACE OR CURB RAMP AND THE STREET.
- WHERE DOMES ARE ARRAYED RADIALLY THEY MAY DIFFER IN DOME DIAMETER AND CENTER-TO-CENTER SPACING WITHIN THE RANGES SPECIFIED ON THIS SHEET.

COLOR REQUIREMENTS:

- THE DETECTABLE WARNING FIELD SHALL BE THE COLOR "SAFETY RED" UNLESS NOTED OTHERWISE IN THE CONTRACT DOCUMENTS AND MUST MEET THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS.

DETECTABLE WARNINGS LOCATIONS:

- DETECTABLE WARNINGS SHALL BE LOCATED SO THAT THE EDGE OR CORNER OF THE WARNING FIELD NEAREST TO THE ROADWAY IS 5" TO 9" FROM THE FRONT OF THE CURB OR THE ROADWAY EDGE (12" WHERE TRAVERSABLE CURB IS USED).
- THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 6'-0" MINIMUM AND 15'-0" MAXIMUM FROM THE CENTERLINE OF THE NEAREST RAIL.

EQUIVALENT SLOPES

PERCENT SLOPE	EQUIVALENT SLOPE
10.00%	10:1
8.33%	12:1
7.14%	14:1
5.00%	20:1
2.00%	50:1
1.00%	100:1

30' 20' 10' 0 30' 60'

SCALE 1" = 30'

DATE	REVISIONS	BY	CHK.

STATE OF NEW JERSEY
BETH-ANN M. GRASSO
No. GE 43121
LICENSED
PROFESSIONAL ENGINEER

ADA1
ADA2

Pennoni

NJ COA NO. GA28033300
BETH-ANN M. GRASSO, P.E.
4/28/2026
PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF
NORTH COUNTY LINE RD
FROM JACKSON MILLS RD TO BENNETTS MILLS RD
JACKSON TOWNSHIP

ADA CURB RAMP DETAILS

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE NOT TO SCALE	DRAWN C.D.	APPROVED B.M.G.
FILE NO.	12 - 009 - 0128		SHEET 79 OF 84

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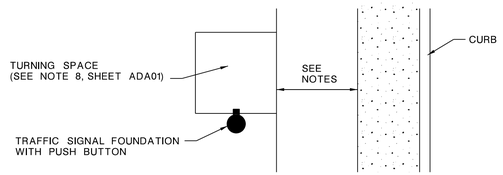
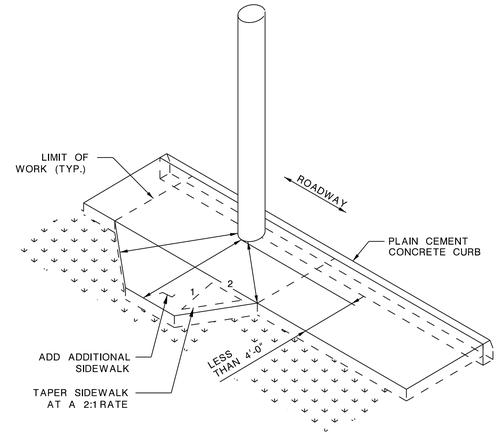


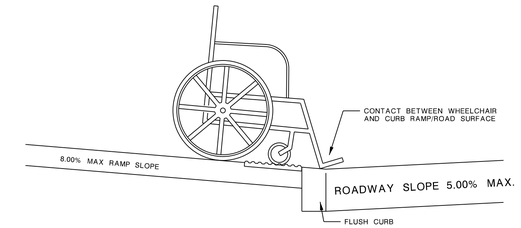
NOTES:

1. TURNING SPACE CENTERED BY PUSH BUTTON & CONNECTED TO PEDESTRIAN PATH/ FACILITIES.
2. LOCATION OF PUSH BUTTON IN REFERENCE TO THE CROSSWALK MUST BE M.U.T.C.D. & A.D.A. COMPLIANT.

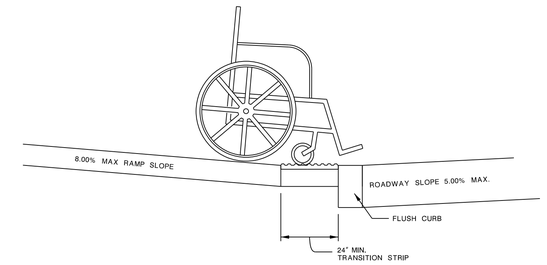
PEDESTRIAN ACCESS TO PUSH BUTTONS



SIDEWALK ADDITION DUE TO OBSTRUCTIONS (REQUIRES OCEAN COUNTY ENGINEERING DEPARTMENT APPROVAL)



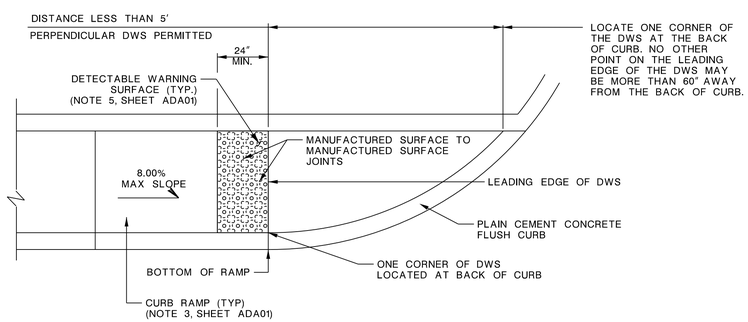
ALGEBRAIC DIFFERENCES BETWEEN ROADWAY SLOPE AND CURB RAMP SLOPE GREATER THAN 11.00% NOT PERMITTED.

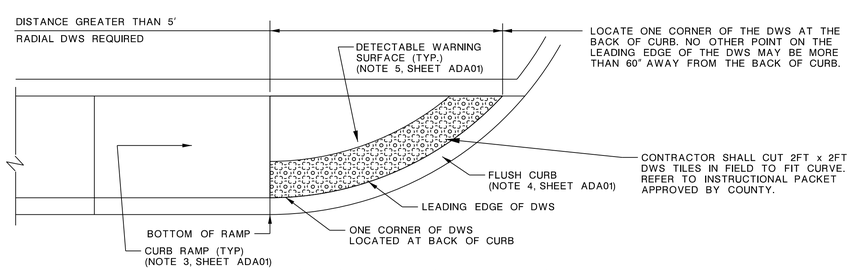


PROVIDE A 24" MIN. TRANSITION STRIP IF ALGEBRAIC DIFFERENCES BETWEEN ROADWAY SLOPE AND CURB RAMP SLOPE ARE GREATER THAN 11.00%.

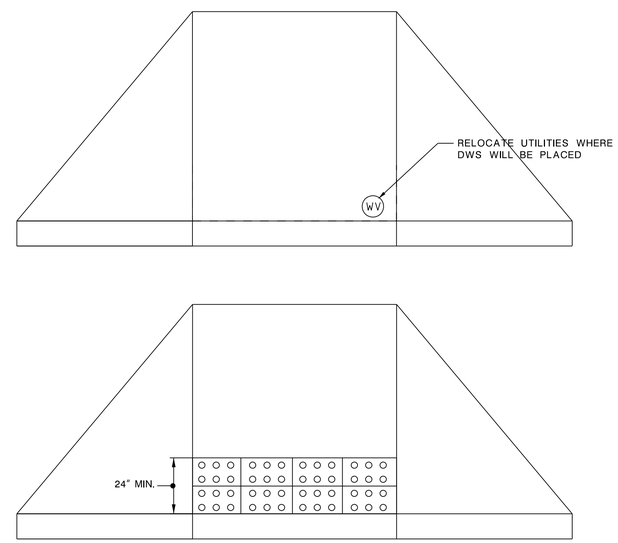
TRANSITION STRIP SLOPE NOT TO EXCEED 2.00%

CHANGE OF GRADE LIMITATIONS





DETECTABLE WARNING SURFACE (DWS) ON CURVED SURFACES



DETECTABLE WARNING SURFACE (DWS) INSTALLATION DETAIL

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

STATE OF NEW JERSEY — BETH-ANN M. GRASSO — No. GE 43121 — LICENSED PROFESSIONAL ENGINEER

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

FROM JACKSON MILLS RD
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ADA CURB RAMP DETAILS

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE NOT TO SCALE	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 80 OF 84

ADA2

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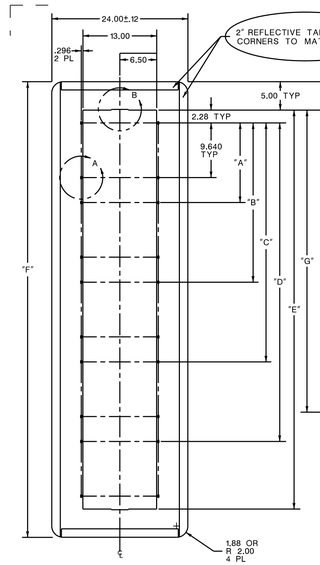
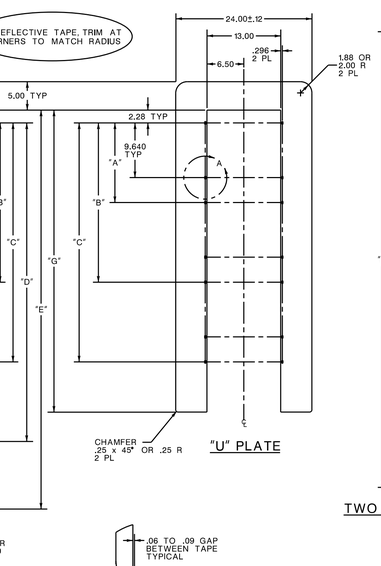
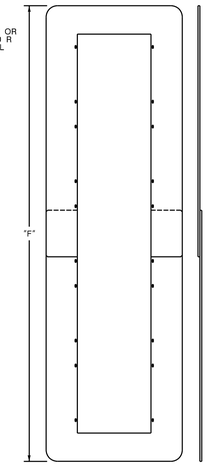


ONE PIECE BACKPLATE W/TAPE



"U" PLATE



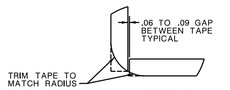
TWO PIECE "U" PLATE ASSEMBLY
SCALE: NONE

ONE PIECE BACKPLATE							
NO. OF SIG SEC	DIMENSIONS						NO. OF SLOTS "H"
	"A"	"B"	"C"	"D"	"E"	"F"	
1	- - - - -	- - - - -	- - - - -	- - - - -	14.22	24.22	4
2	- - - - -	- - - - -	- - - - -	14.04	28.26	38.26	8
3	- - - - -	- - - - -	14.04	28.08	42.30	52.30	12
4	- - - - -	14.04	28.08	42.12	56.34	66.34	16
5	14.04	28.08	42.12	56.16	70.38	80.38	20

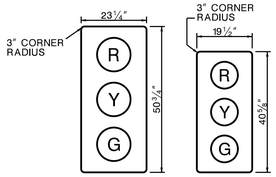
"U" PLATE					
NO. OF SIG SEC	DIMENSIONS				NO. OF SLOTS "H"
	"A"	"B"	"C"	"G"	
1	- - - - -	- - - - -	- - - - -	12.30	4
2	- - - - -	- - - - -	14.04	25.30	6
3	- - - - -	14.04	28.08	39.30	10
4	14.04	28.08	42.12	53.30	14

TWO PIECE ASSY	
NO. OF SIG SEC	DIM "F"
1	24.22
2	38.26
3	52.30
4	66.34
5	80.38
6	94.42

TOLERANCES UNLESS OTHERWISE SPECIFIED	
DECIMALS	ANGLES
± .03 .XX ± .015 .XXX	± 30'
DO NOT SCALE DRAWING	
DRILLED HOLE	
.013 - .125	± .004 .001
.126 - .250	± .005 .001
.251 - .500	± .006 .001
.501 - .750	± .008 .001
.751 - 1.000	± .010 .001
1.001 - 2.000	± .012 .001

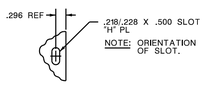


DETAIL C
SCALE 2:1



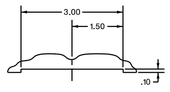
BACKPLATE
FOR 12"
SIGNAL HEAD

BACKPLATE
FOR 8"
SIGNAL HEAD



NOTE: ORIENTATION OF SLOT.

DETAIL A
SCALE 1:1

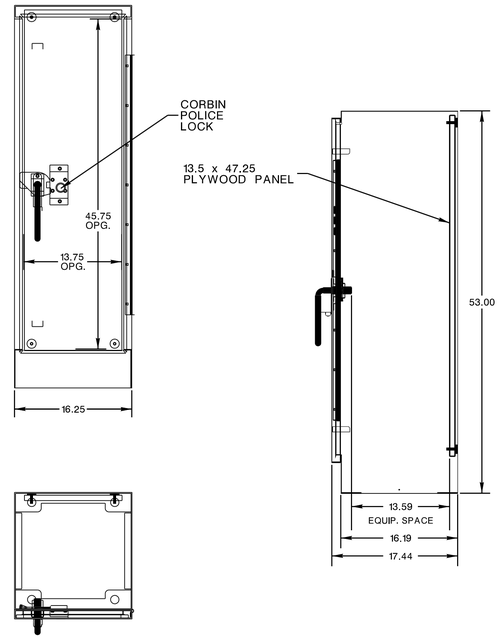


DETAIL B
SCALE 1:1

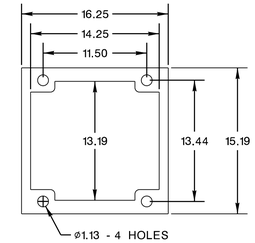
NOTE: USED ON ONE PIECE BACKPLATE, TOP & BOTTOM

REFLECTIVE BACKPLATE WITH 2" REFLECTIVE BORDER

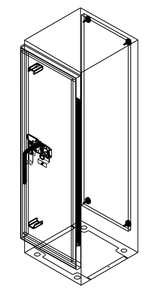
NOTES:
- MATERIAL: .125 BLACK ALUMINUM WITH HAIRCELL FINISH FRONT SIDE.
- FABRICATION: VACUUM FORMED.
- BACKPLATES SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- IF A BACKPLATE IS USED, THE CLEARANCE SHALL BE 15'-6" MINIMUM FROM THE BOTTOM OF THE BACKPLATE TO THE FINISHED GRADE OF ROADWAY.
- BREAK ALL SHARP EDGES .010.
- 2" WIDE REFLECTIVE TAPE TO BORDER OF BACKPLATE. TRIM TAPE SO IT DOES NOT OVERLAP THE PIECES. FOR 2 PIECE BACKPLATES, TRIM THE TAPE SO IT DOES NOT OVERLAP THE 2 PIECES.



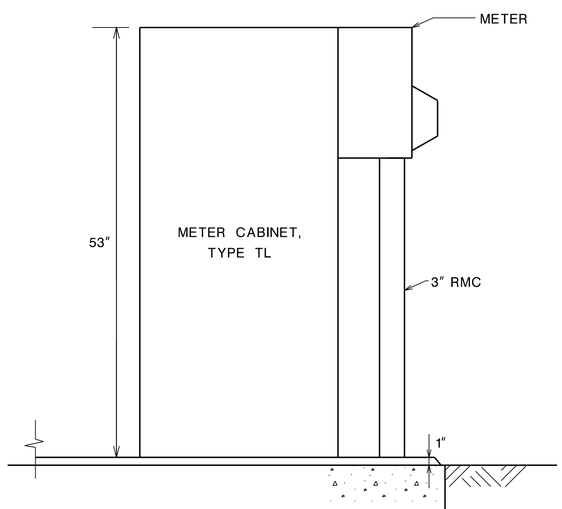
METER CABINET TYPE "TL"



PAD MOUNTING PATTERN



MATERIAL: .125 5052-H32 ALUMINUM
FINISH: NATURAL MILL FINISH



METHOD OF MOUNTING JCP&L AND PSE&G
METER TO CABINET, TYPE TL

FOUNDATION, TYPE MODIFIED P-MC
OR
FOUNDATION, TYPE MODIFIED P-MC-GAC

NOTES:

1. ALL CONDUIT SHALL BE INSTALLED SO THAT COUPLINGS ARE EMBEDDED PLUMB AND FLUSH WITH TOP OF CONCRETE FOUNDATION.
2. J-BOLT MUST BE INSERTED 1½" ⅛" INTO 3" COUPLING.
3. PROPOSED FOUNDATIONS AND JUNCTION BOXES TO BE CONSTRUCTED OR RESET SHALL BE SET UTILIZING THE PROPOSED GRADES AS SHOWN IN THE CONSTRUCTION/GRADING PLANS FOR THE SURROUNDING AREA OF THE PROPOSED EQUIPMENT. IF THE FOUNDATIONS OR JUNCTION BOXES ARE TO BE CONSTRUCTED OR RESET NEAR THE SIDEWALK, USE THE PROPOSED SIDEWALK GRADES TO BASE THE FOUNDATION OR JUNCTION BOX GRADES. ANY FOUNDATION OR JUNCTION BOX THAT IS CONSTRUCTED OR RESET ABOVE OR BELOW THE PROPOSED GRADES WILL BE RESET OR RECONSTRUCTED TO THE PROPER GRADES AT THE CONTRACTOR'S EXPENSE.

DATE	REVISIONS	BY	CHK.

Pennoni
NJ COA NO. GA28033300

STATE OF NEW JERSEY
BETH-ANN M. GRASSO
No. GE 43121
LICENSED PROFESSIONAL ENGINEER

BETH-ANN M. GRASSO, P.E.
4/28/2026
PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF
NORTH COUNTY LINE RD
FROM JACKSON MILLS RD
TO BENNETTS MILLS RD
JACKSON TOWNSHIP

ELECTRICAL DETAILS

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE NOT TO SCALE	DRAWN C.D.	APPROVED B.M.G.
FILE NO.	12 - 009 - 0128		SHEET 81 OF 84

ED4

J:\Accounts\OCEAN COUNTY\12-009-0128 Rebuild & Reconstruct Roads\CR 49\CR49\PLANS\SHEETS\81-ED-Det.dgn
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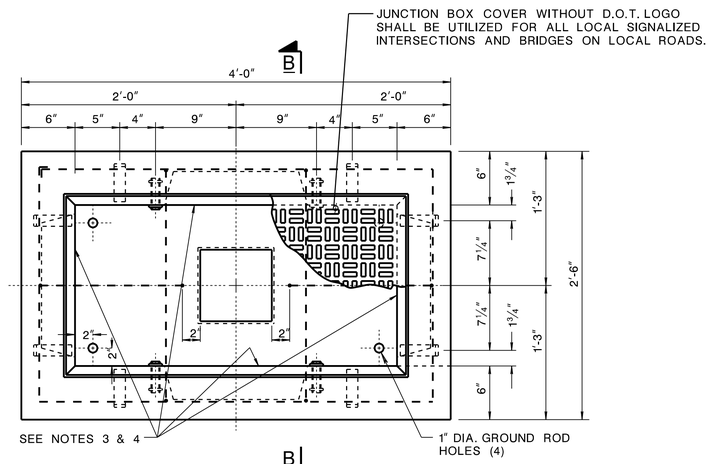
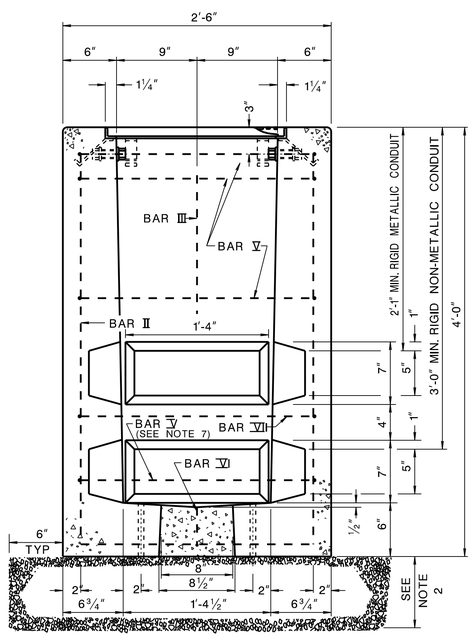


PLAN



SECTION B-B

18" X 36" JUNCTION BOX "JB"

PRECAST

ITEM

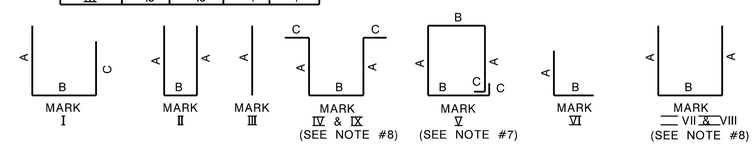
JUNCTION BOX FOUNDATION "JBF"

MARK	SIZE NO. 4 RODS			NO. RODS REQ.
	A	B	C	
I	41"	40"	40"	2
II	40"	24"	—	2
III	19"	—	—	2
IV	15"	16"	12"	4
V	44"	26"	4"	4
VI	1"	15"	—	2
VII	15"	26"	—	2
VIII	40"	59"	—	2
IX	15"	16"	4"	4

ITEM

18" X 36" JUNCTION BOX "JB"

MARK	SIZE NO. 4 RODS			NO. RODS REQ.
	A	B	C	
II	42"	25"	—	4
III	21"	—	—	2
V	44"	26"	4"	3
VI	2"	16"	—	2
VII	15"	26"	—	2



NOTES:

1.) THE CONSTRUCTION AND INSTALLATION OF PRECAST JUNCTION BOX FOUNDATION AND 18" X 36" JUNCTION BOX SHALL CONFORM TO SPECIFICATIONS FOR THE CONTRACT AND TO DETAILS AND NOTES SHOWN ON THIS DRAWING AND IN ADDITION SHALL INCLUDE APPLICABLE NOTES AND DETAILS SHOWN ON DRAWING FOR CAST-IN-PLACE TYPE.

2.) COMPACTED ¾" GRAVEL OR BROKEN STONE BASE REQUIRED FOR ALL PRECAST UNITS. MINIMUM DEPTH 8".

3.) A 16" X 7" X 4" RECESS TO BE PROVIDED IN SIDE AND END WALLS AS SHOWN.

4.) AFTER THE INSTALLATION OF CONDUIT, ALL OPEN RECESSES ARE TO BE COMPLETELY BRICKED AND FINISHED.

5.) AN ALTERNATE RECESS ARRANGEMENT MAY BE SUBSTITUTED AS SHOWN BELOW IN DETAIL "A" & "B". IF THIS ARRANGEMENT IS USED AND THE CONDUIT

6.) PRECAST JUNCTION BOX FOUNDATION MAY BE INSTALLED BEHIND THE GUIDE RAIL OR IN AREAS WHERE THE SLOPE IS LESS THAN 1:22.

7.) INSTALL THIS BAR WHEN USING DETAIL "A" ONLY.

8.) FOR ALTERNATE LOCATION FOR THE FOUNDATION, USE MARK 8 AND 9 IN PLACE OF MARK 1 AND 4.

9.) ALIGN CONDUITS.

10.) PROPOSED FOUNDATIONS AND JUNCTION BOXES TO BE CONSTRUCTED OR RESET SHALL BE SET UTILIZING THE PROPOSED GRADES AS SHOWN IN THE CONSTRUCTION/GRADING PLANS FOR THE SURROUNDING AREA OF THE PROPOSED EQUIPMENT. IF THE FOUNDATIONS OR JUNCTION BOXES ARE TO BE CONSTRUCTED OR RESET NEAR THE SIDEWALK, USE THE PROPOSED SIDEWALK GRADES TO BASE THE FOUNDATION OR JUNCTION BOX GRADES. ANY FOUNDATION OR JUNCTION BOX THAT IS CONSTRUCTED OR RESET ABOVE OR BELOW THE PROPOSED GRADES WILL BE RESET OR RECONSTRUCTED TO THE PROPER GRADES AT THE CONTRACTOR'S EXPENSE.

TYPICAL DETAIL FOR 18" X 36" JUNCTION BOX

NOTES:

1. ALL CONDUIT SHALL BE INSTALLED SO THAT COUPLINGS ARE EMBEDDED PLUMB AND FLUSH WITH TOP OF CONCRETE FOUNDATION.
2. J-BOLT MUST BE INSERTED 1½" ⅛" INTO 3" COUPLING.
3. PROPOSED FOUNDATIONS AND JUNCTION BOXES TO BE CONSTRUCTED OR RESET SHALL BE SET UTILIZING THE PROPOSED GRADES AS SHOWN IN THE CONSTRUCTION/GRADING PLANS FOR THE SURROUNDING AREA OF THE PROPOSED EQUIPMENT. IF THE FOUNDATIONS OR JUNCTION BOXES ARE TO BE CONSTRUCTED OR RESET NEAR THE SIDEWALK, USE THE PROPOSED SIDEWALK GRADES TO BASE THE FOUNDATION OR JUNCTION BOX GRADES. ANY FOUNDATION OR JUNCTION BOX THAT IS CONSTRUCTED OR RESET ABOVE OR BELOW THE PROPOSED GRADES WILL BE RESET OR RECONSTRUCTED TO THE PROPER GRADES AT THE CONTRACTOR'S EXPENSE.

FOUNDATION SHALL BE POURED MONOLITHIC.

STATE OF NEW JERSEY
BETH-ANN M. GRASSO
No. GE 43121
LICENSED PROFESSIONAL ENGINEER
ED4

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

DATE	REVISIONS	BY	CHK.

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

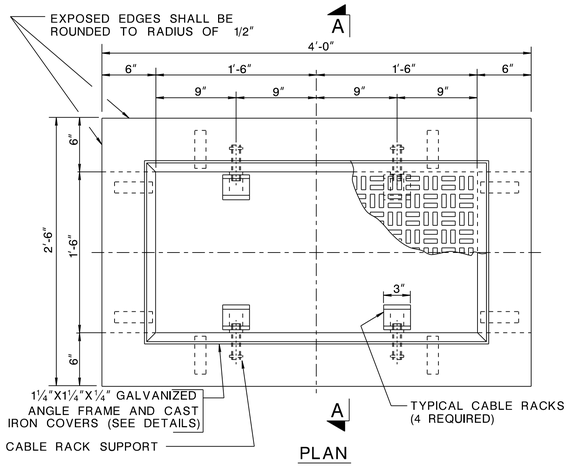
FROM JACKSON MILLS RD TO BENNETTS MILLS RD

JACKSON TOWNSHIP

ELECTRICAL DETAILS

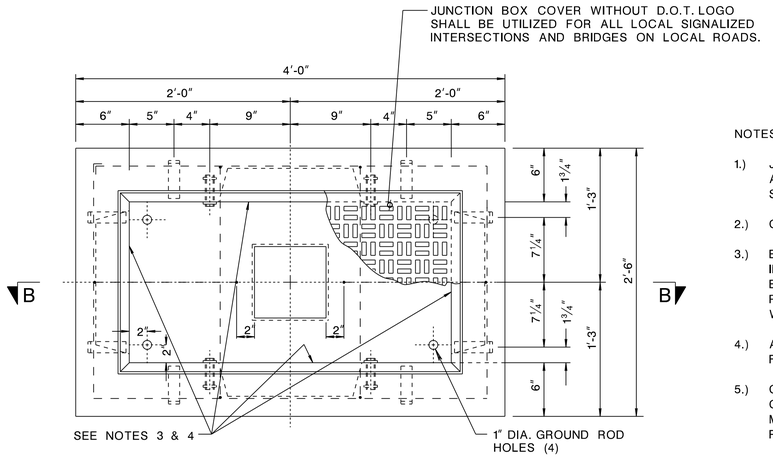
FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE NOT TO SCALE	DRAWN C.D.	APPROVED B.M.G.
FILE NO.	12 - 009 - 0128		SHEET 82 OF 84

J:\Accounts\C2SS\C2SS0001 - C2SS0-NJ Round 3 Bundle 3\C2SS3_PRESTRESSED_EDI-1.dgn
IS\NJ\Ocean\Ocean County\Bundle 3\Bundle 3\C2SS3_PRESTRESSED_EDI-1.dgn
Plotted: 5/28/26



PLAN

18" X 36" JUNCTION BOX - CLASS "JB"
CONCRETE CLASS "B"
USING APPROVED ⅝" AGGREGATE
0.8 CU. YD.



PLAN

NOTES:

1.) JUNCTION BOX FOUNDATION SHALL BE SET PARALLEL TO THE CURB AND TOP OF JUNCTION BOX SHALL BE SET AT GRADE IN SIDEWALK, PAVED AREA, IN GRASS OR DIRT AREAS.

2.) CABLE RACKS FURNISHED AND INSTALLED AS INDICATED.

3.) BONDING AND GROUNDING INSULATED BUSHINGS SHALL BE INSTALLED ON METALLIC CONDUITS TERMINATING IN JUNCTION BOXES AND/OR FOUNDATIONS AND SHALL HAVE A FITTING TO PREVENT ENTRY OF FOREIGN MATTER PRIOR TO INSTALLATION OF WIRING.

4.) A NYLON CORD, 125 POUND MINIMUM TEST STRENGTH, SHALL BE FURNISHED AND INSTALLED IN ALL CONDUITS. SEE SPECIFICATIONS.

5.) CONDUITS SHALL ENTER JUNCTION BOX PERPENDICULAR TO WALLS OR AS APPROVED BY THE ENGINEER. A 2" SEPARATION SHALL BE MAINTAINED BETWEEN ADJACENT WALLS, CONDUITS, AND CABLE RACK LOCATIONS.

6.) TERMINAL ENDS OF ALL METALLIC CONDUIT SHALL BE THREADED.

7.) ALL NON-METALLIC CONDUITS SHALL TERMINATE WITH BELL END CONSTRUCTION IN JUNCTION BOX.

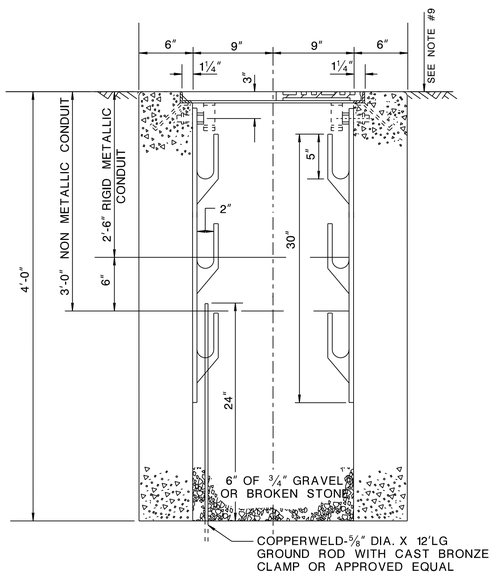
8.) ALL UNUSED CONDUITS SHALL BE PLUGGED OR CAPPED.

9.) ENGINEER MAY REQUIRE TOP OF JUNCTION BOX TO BE INCLINED IN ORDER TO CONFORM WITH FIELD CONDITIONS. JUNCTION BOX SHALL BE SET TO GRADE IN SIDEWALK AREA AND IN ALL OTHER AREAS.

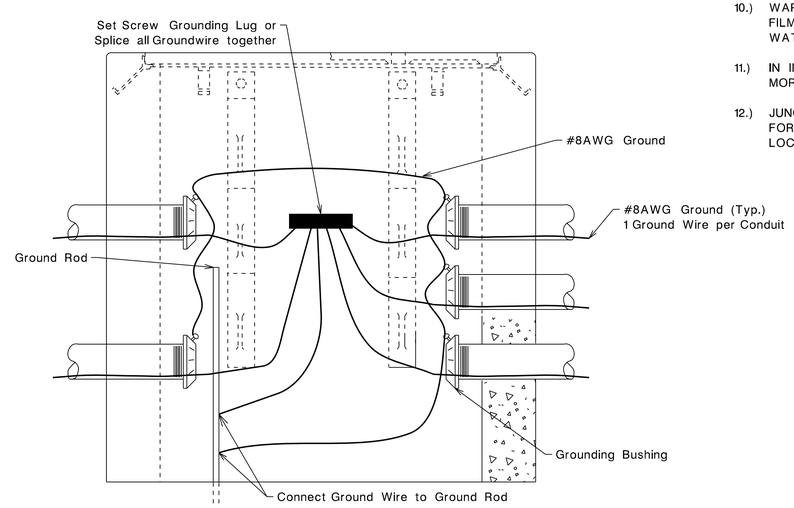
10.) WARNING TAPE SHALL BE A RED 4 MIL. FLEXIBLE POLYETHYLENE FILM WHICH IS RESISTANT TO ACIDS, BASES, HYDROCARBONS AND WATER.

11.) IN INCLINE AREA, NO PART OF THE JUNCTION BOX SHALL EXTEND MORE THAN 4" ABOVE THE FINISH GRADE.

12.) JUNCTION BOX COVER WITHOUT D.O.T. LOGO SHALL BE UTILIZED FOR ALL LOCAL SIGNALIZED INTERSECTIONS AND BRIDGES ON LOCAL ROADS.



SECTION A-A



SECTION B-B

TYPICAL DETAILS FOR GROUND WIRE IN JUNCTION BOXES

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

STATE OF NEW JERSEY — BETH-ANN M. GRASSO — No. GE 43121 — LICENSED PROFESSIONAL ENGINEER

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

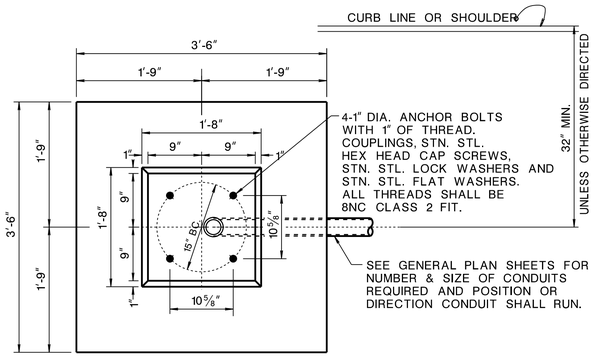
FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

ELECTRICAL DETAILS

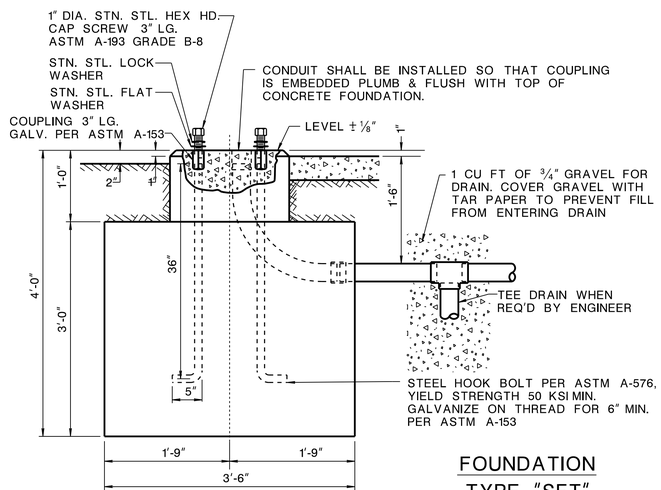
FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE NOT TO SCALE	DRAWN C.D.	APPROVED B.M.G.
FILE NO. 12 - 009 - 0128			SHEET 83 OF 84

ED4



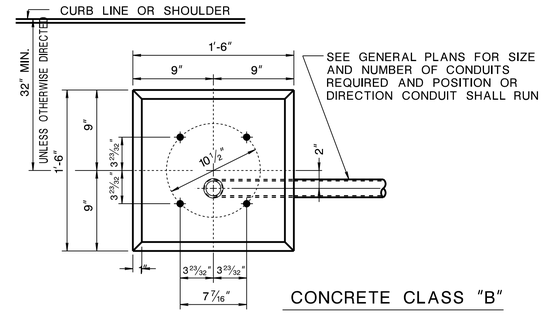
PLAN

CONCRETE CLASS "B"
1.5 CU. YDS.
FOUNDATION SHALL BE POURED
MONOLITHIC



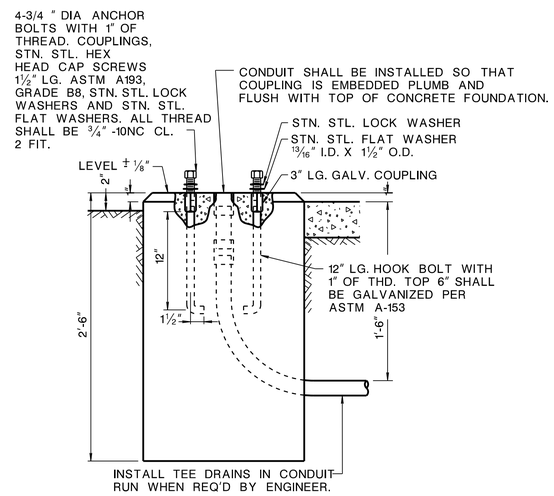
ELEVATION

FOUNDATION
TYPE "SFT"



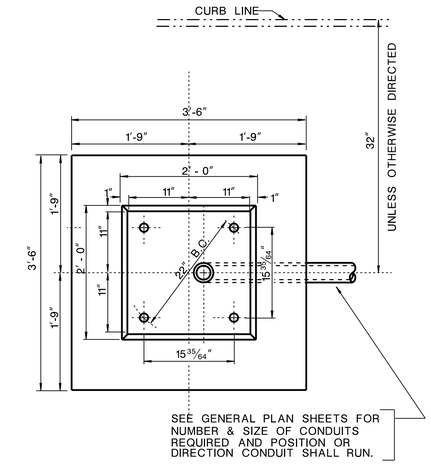
PLAN

CONCRETE CLASS "B"
6 CU. FT.

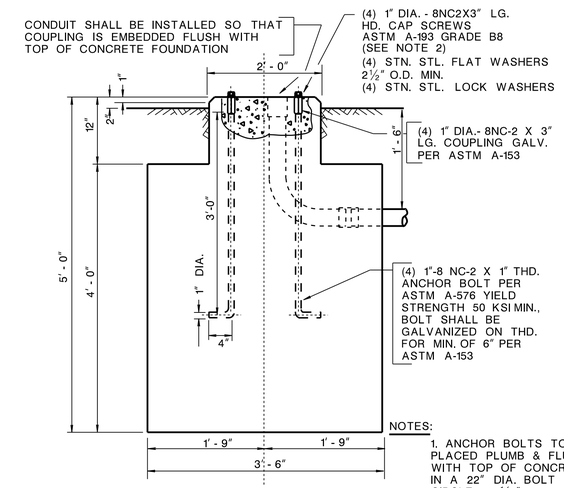


ELEVATION

FOUNDATION
TYPE "SPF"



PLAN



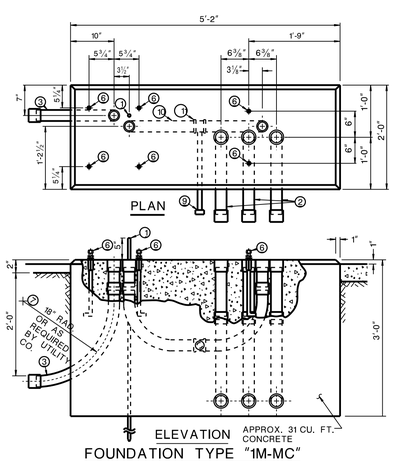
NOTES:

1. ANCHOR BOLTS TO BE PLACED PLUMB & FLUSH WITH TOP OF CONCRETE IN A 22" DIA. BOLT CIRCLE + 1/32".
2. CAP SCREW SHALL PROVIDE A MINIMUM OF ONE INCH OF THREAD IN THE COUPLING WITHOUT BUTTING THE ANCHOR BOLT.

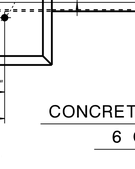
ELEVATION

FOUNDATION TYPE "SFK"

CONCRETE CLASS "B"
2 CU. YDS.
FOUNDATION SHALL BE POURED MONOLITHIC



FOUNDATION TYPE "1M-MC"



GALVANIZED
COUPLING

GALV. PER ASTM A-153
CONTINUOUS THREADS

TYPICAL DETAILS FOR FOUNDATIONS SFT, SFK, SPF AND 1M-MC

NOTES:

1. ALL CONDUIT SHALL BE INSTALLED SO THAT COUPLINGS ARE EMBEDDED PLUMB AND FLUSH WITH TOP OF CONCRETE FOUNDATION.
2. J-BOLT MUST BE INSERTED 1½" ± 1/8" INTO 3" COUPLING.
3. PROPOSED FOUNDATIONS AND JUNCTION BOXES TO BE CONSTRUCTED OR RESET SHALL BE SET UTILIZING THE PROPOSED GRADES AS SHOWN IN THE CONSTRUCTION/GRADING PLANS FOR THE SURROUNDING AREA OF THE PROPOSED EQUIPMENT. IF THE FOUNDATIONS OR JUNCTION BOXES ARE TO BE CONSTRUCTED OR RESET NEAR THE SIDEWALK, USE THE PROPOSED SIDEWALK GRADES TO BASE THE FOUNDATION OR JUNCTION BOX GRADES. ANY FOUNDATION OR JUNCTION BOX THAT IS CONSTRUCTED OR RESET ABOVE OR BELOW THE PROPOSED GRADES WILL BE RESET OR RECONSTRUCTED TO THE PROPER GRADES AT THE CONTRACTOR'S EXPENSE.
4. FOUNDATION SHALL BE POURED MONOLITHIC.

DATE	REVISIONS	BY	CHK.

Pennoni

NJ COA NO. GA28033300

BETH-ANN M. GRASSO, P.E.

STATE OF NEW JERSEY
BETH-ANN M. GRASSO
No. GE 43121
LICENSED PROFESSIONAL ENGINEER

4/28/2026

PROFESSIONAL ENGINEER
NJ LICENSE NO. 24GE04312100

OCEAN COUNTY NEW JERSEY

RECONSTRUCTION OF

NORTH COUNTY LINE RD

FROM JACKSON MILLS RD
TO BENNETTS MILLS RD

JACKSON TOWNSHIP

ELECTRICAL DETAILS

FLD.BK.	DATE APRIL 27, 2026	DESIGNED J.M.M.	CHECKED R.M.E.
	SCALE NOT TO SCALE	DRAWN C.D.	APPROVED B.M.G.
FILE NO.	12 - 009 - 0128		SHEET 84 OF 84